



Placentia Planning Commission Agenda

Regular Meeting

June 13, 2017

6:30 p.m.

City Hall Council Chambers
401 E. Chapman Avenue

Christine Schaefer
Chair

Frank Perez
Vice Chair

Heather Francine
Commissioner

Dennis Lee
Commissioner

James Schenck
Commissioner

Vic Tomazic
Commissioner

Claudia Keller
Commissioner

City of Placentia
401 E Chapman Avenue
Placentia, CA 92870

Phone: (714) 993-8124
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Website: www.placentia.org

Procedures for Addressing the Commission

Any person who wishes to speak regarding an item on the agenda or on a subject within the Planning Commission's jurisdiction during the "Oral Communications" portion of the agenda should fill out a "Speaker Request Form" and give it to the Commission Secretary BEFORE that portion of the agenda is called. Testimony for Public Hearings will only be taken at the time of the hearing. Any person who wishes to speak on a Public Hearing item should fill out a "Speaker Request Form" and give it to the Commission Secretary BEFORE the item is called.

The Commission encourages free expression of all points of view. To allow all persons the opportunity to speak, please keep your remarks brief. If others have already expressed your position, you may simply indicate that you agree with a previous speaker. If appropriate, a spokesperson may present the views of an entire group. To encourage all views, the Commission discourages clapping, booing or shouts of approval or disagreement from the audience.

PLEASE SILENCE CELL PHONES AND OTHER ELECTRONIC EQUIPMENT WHILE THE COMMISSION IS IN SESSION.

Special Accommodations

In compliance with the Americans with Disabilities Act, if you need special assistance to participate in this meeting, please contact the City Clerk's Office at (714) 993-8231. Notification 48 hours prior to the meeting will generally enable City staff to make reasonable arrangements to ensure accessibility.
(28 CFR 35.102.35.104 ADA Title II)

Copies of all agenda materials are available for public review in the Office of the City Clerk, City Planning Division Counter, Placentia Library Reference Desk and the internet at www.placentia.org under the Planning Commission page. Persons who have questions concerning any agenda item may call the City Planning Division at (714) 993-8124 to make inquiry concerning the nature of the item described on the agenda.

In compliance California Government Code Section 54957.5, any writings or documents provided to a majority of the Planning Commission regarding any item on this agenda that are not exempt from disclosure under the Public Records Act will be made available for public inspection at the City Clerk's Office at City Hall, 401 East Chapman Avenue, Placentia, during normal business hours.

Study Sessions are open to the public and held in the City Council Chambers or City Hall Community Room.

REGULAR MEETING
6:30 p.m. – City Council Chambers

CALL TO ORDER:

ROLL CALL: Commissioner Keller
Commissioner Francine
Commissioner Lee
Commissioner Schenck
Commissioner Tomazic
Vice Chair Perez
Chair Schaefer

PLEDGE OF ALLEGIANCE:

ORAL COMMUNICATIONS:

At this time the public may address the Planning Commission concerning any agenda item, which is not a public hearing item, or on matters within the jurisdiction of the Planning Commission. There is a five (5) minute time limit for each individual addressing the Planning Commission.

CONSENT CALENDAR:

REGULAR AGENDA:

1. **Applicant:** City of Placentia

Project Location: Citywide

**General Plan Conformance Finding for Fiscal Year 2017-18 and 2019-2024
Seven Year Capital Improvement Program**

Recommended Actions: It is recommended that the Planning Commission:

- a. Adopt Resolution No. PC-2017-10, A Resolution of the Planning Commission of the City of Placentia, California finding that the Fiscal Year 2017-18 and 2019-24 Capital Improvement Program conforms to the adopted General Plan and recommending the City Council approve the Capital Improvement Program.

PUBLIC HEARINGS:

1. Applicant: City of Placentia

Project Location: The Old Town Placentia Revitalization Plan and Development Project applies to an L-shaped Planning Area that is located in the southwestern portion of the City of Placentia. The Planning Area is centered along Bradford Avenue south of Chapman Avenue and Santa Fe Avenue from Murray Street on the west to Alta Street on the east. The portion of the Planning Area centered on Bradford Avenue extends east to Alta Street and west to Main Street south of Chapman Avenue. The portion of the Planning Area centered on Santa Fe Avenue extends from Murray Street on the west, Aguirre Lane (and extension of Aguirre Lane) on the north, the Burlington Northern-Santa Fe (BNSF) railroad tracks to the south, and east to Alta Street. The Planning Area is just over 32 acres in size and is located north of the Burlington Northern-Santa Fe (BNSF) railroad tracks and south of Chapman Avenue in the City of Placentia.

General Plan Amendment (GPA) 2017-02 and Zone Change (ZC) 2017-02:

Review of the Old Town Revitalization Project which includes the Adoption of the Old Town Revitalization Plan, the Old Town Streetscape Master Plan, General Plan Amendment 2017-02 and Zone Change 2017-02 to establish the Old Town Zoning District and New Development Standards.

Recommended Actions: It is recommended that the Planning Commission:

- a. Open the Public Hearing concerning the Old Town Revitalization Plan, the Old Town Streetscape Master Plan, the Initial Study and Negative Declaration, Zoning Code Amendment 2017-02 and General Plan Amendment 2017-02; and
- b. Receive the Staff Report and consider all public testimony;
- c. Close the Public Hearing; and
- d. Adopt Resolution PC-2017-11, A Resolution of the Planning Commission of the City of Placentia, recommending that City Council approve General Plan Amendment 2017-02 to change the current land use designation from Commercial, Medium Density Residential and High Density Residential to the new Old Town designation for the project area; and, to approve Zone Change 2017-02, changing the existing zoning from SF-C, C-1, C-2, and R-2 and creating a new zoning district "Old Town" and development standards for the "Old Town Placentia" district for the project area; recommending adoption of

the Old Town Streetscape Master Plan; recommending adoption of the Old Town Placentia Revitalization Plan; and recommending the adoption of a Negative Declaration pursuant to the California Environmental Quality Act Guidelines (CEQA) set forth in Title 14 CCR § 15074 and the City of Placentia Environmental Guidelines.

OLD BUSINESS: None

NEW BUSINESS: None

DEVELOPMENT REPORT:

DIRECTOR'S REPORT:

PLANNING COMMISSION REQUESTS

Commission members may make requests or ask questions of Staff. If a Commission member would like to have formal action taken on a requested matter, it will be placed on a future Commission Agenda.

ADJOURNMENT

The Planning Commissioners ADJOURN to the next regular meeting on Tuesday, July 11, 2017 at 6:30 p.m. in the Placentia City Hall Front Community Meeting Room located at 401 East Chapman Avenue, Placentia CA, 92870.

CERTIFICATION OF POSTING

I, Joseph M. Lambert, Secretary to the Planning Commission of the City of Placentia, hereby certify that the Agenda for the June 13, 2017 Regular Meeting of the Planning Commission of the City of Placentia was posted on June 8, 2017.



Joseph M. Lambert, Secretary



Placentia Planning Commission

AGENDA STAFF REPORT

TO: PLANNING COMMISSION
VIA: DIRECTOR OF DEVELOPMENT SERVICES
FROM: LUIS ESTEVEZ, DIRECTOR OF PUBLIC WORKS
DATE: JUNE 13, 2017
SUBJECT: **GENERAL PLAN CONFORMANCE FINDING FOR FISCAL YEAR 2017-18 AND 2019-2024 SEVEN YEAR CAPITAL IMPROVEMENT PROGRAM**

RECOMMENDATION:

It is recommended that the Planning Commission take the following actions:

1. Adopt Resolution No. PC-2017-10, A Resolution of the Planning Commission of the City of Placentia, California finding that the Fiscal Year 2017-18 and 2019-24 Capital Improvement Program conforms to the adopted General Plan and recommending the City Council approve the Capital Improvement Program.

DISCUSSION:

California Government Code § 65401 requires the City to prepare a coordinated program (i.e., Capital Improvement Program (CIP) identifying projects recommended for planning, initiation or construction during the ensuing fiscal year. In accordance with state law, the Planning Commission must review and report on the conformity of the CIP with the adopted General Plan.

The (CIP) serves as a single comprehensive plan of proposed capital improvement projects for the budget year FY 2017-18 and the six (6) years thereafter. The CIP is a budgeting and strategic planning tool used to determine the extent of future needs and sources of funding. The CIP also serves as a guide for the efficient evaluation, prioritization and implementation of public improvements and facilities. The CIP identifies 98 funded and unfunded projects to undertake over the next seven (7) years. For FY 2017-18, the City has identified 37 projects in which it expects to continue, undertake and/or complete.

As part of the CIP process, capital improvement projects are evaluated to determine the estimated cost and prioritized to take full advantage of Federal, State, Orange County and other funding from outside agencies. The CIP is reviewed yearly, during which time the City's needs may be re-prioritized and the City's financial status may be re-evaluated.

The first CIP priority is safety within the City's right-of-way, parks, and facilities. This priority is addressed throughout each section of the CIP document in numerous ways, including streets, traffic control and operations, parks, public buildings, bridges, sewer systems and storm drain systems. Additionally, a primary focus of the CIP is to preserve and reinvest in the City's physical assets, as

reflected in each of the ten CIP sections. This includes pavement rehabilitation projects for residential and arterial streets as well as improvements at City facilities for accessibility and ADA improvements. The proposed CIP recommends several projects which address facilities or equipment that are no longer adequate to meet demand or serve our citizens. These projects include the replacement or upgrade of facilities or equipment, expansion or renovation of an existing facility, or construction of a new facility. Another important component of the CIP is to implement projects identified within a master plan or other planning document that are needed to maintain or meet adequate levels of service. These projects may include new facilities or improvements toward existing facilities. Finally, the CIP addresses several future planned projects through funding of studies or project construction plans.

Conformity Findings

The basis for making a conformity finding is determining that each project fits into a goal or implementation policy as outlined in the adopted General Plan. For example, if a project is proposed to pave over several acres of open space, and that open space is called out in the Open Space Element as being protected, then the project does not conform to that General Plan Element. Another example is, if a project is proposed to widen a street or add bicycle or pedestrian paths to increase mobility, such a project would conform to the Circulation Element goals to increase accessibility and mobility for alternative modes of transportation.

As was the case with last year, the limiting factor in making conformity findings is that the City has not yet concluded its comprehensive General Plan Update. City Staff has reviewed each of the proposed projects to be undertaken during Fiscal Year 2017 -18 in relation to the adopted General Plan. While many of the individual elements do not have goals, policies or implementation programs as updated elements would now contain, there is still the ability to compare the projects identified in the CIP matrix against the narrative of the adopted elements and make a determination that they are each in conformity with the adopted General Plan for the City of Placentia including improvements to traffic circulation, parks, recreation and open space as well as land uses within the City. As a few examples, some of the Capital Improvement Projects identified in the matrix provides for the Kraemer Boulevard Street resurfacing, numerous traffic signal improvements, improvements to the Old Town Metrolink station as well as park improvements. The Commission's finding that a proposed CIP item conforms to the General Plan does not necessarily mean the City will have the ability to implement or construct a certain CIP item. All CIP projects and plans must go through separate individual environmental and Council review. Accordingly, Staff is recommending that the Planning Commission find that the proposed CIP projects are in conformity with the adopted General Plan.

CEQA:

The proposed Finding of General Plan Conformity is not a project as defined by the California Environmental Quality Act (CEQA). The finding of conformity does not commit the City to any of the identified projects in the CIP. Therefore, approval of the CIP qualifies for an exemption from the California Environmental Quality Act (CEQA) per Guideline 15061 (b)(3) for activities that have no potential for causing a significant effect on the environment. Furthermore, individual projects identified within the CIP will be reviewed in accordance with CEQA requirements prior to their implementation and when specific project details are available for analysis.

Prepared and submitted by



Luis Estevez
Director of Public Works

Reviewed and approved by:



Joseph M. Lambert
Director of Development Services

Attachments:

1. Resolution No. PC-2017-10
2. General Plan Conformity Matrix

RESOLUTION NO. PC-2017-10

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF PLACENTIA, CALIFORNIA FINDING THAT THE FISCAL YEAR 2017-18 TO 2019-24 CAPITAL IMPROVEMENT PROGRAM CONFORMS TO THE ADOPTED GENERAL PLAN AND RECOMMENDING THE CITY COUNCIL APPROVE THE CAPITAL IMPROVEMENT PROGRAM

A. Recitals.

(i). On June 13, 2017, this Planning Commission considered the conformity of the proposed Fiscal Year 2017-18 to 2019-24 Capital Improvement Program to the adopted General Plan.

(ii). This Planning Commission has reviewed and considered all elements of the proposed Capital Improvement Program, including written staff reports and verbal testimony presented.

(iii). All legal prerequisites to the adoption of this Resolution have occurred.

B. Resolution.

NOW, THEREFORE, the Planning Commission of the City of Placentia does hereby find, determine, and resolve as follows:

1. This Commission hereby specifically finds that all the facts as set forth in the Recitals, Part A., of this Resolution are true and correct.

2. This Commission finds, and recommends the City Council find, that the Fiscal Year 2017-18 to 2019-24 Capital Improvement Program is in conformance with the Placentia General Plan.

3. The proposed Fiscal Year 2017-18 to 2019-24 Capital Improvement Program promotes implementation of the goals and policies set forth in the adopted General Plan.

4. The Planning Commission hereby makes, and recommends the City Council make, the following finding: There is no possibility that the proposed Fiscal Year 2017-

18 to 2019-24 Capital Improvement Program may have a significant effect on the environment and said amendments are therefore not subject to the requirements of the California Environmental Quality Act pursuant to the provisions of §15061(b)(3) of Division 6 of Title 14 of the California Code of Regulations, and the City of Placentia Environmental Guidelines.

5. The Planning Commission hereby recommends that, upon adoption of the Fiscal Year 2017-18 to 2019-24 Capital Improvement Program, a Notice of Exemption be filed with the Orange County Clerk-Recorder, as required by law.

6. This Commission finds, and recommends the City Council find, that the facts supporting the above specified findings are contained in the staff report and exhibits, and information provided to this Planning Commission with respect to the proposed Fiscal Year 2017-18 to 2019-24 Capital Improvement Program.

7. This Commission hereby recommends that the City Council of the City of Placentia approve the proposed Fiscal Year 2017-18 to 2019-24 Capital Improvement Program.

8. The Secretary of this Commission shall:

a. Certify to the adoption of this Resolution; and

b. Forthwith transmit a certified copy of this Resolution to the City Council of the City of Placentia together with all documents prepared with respect to these considerations and transcripts of any and all hearings conducted with respect to the action recommended for approval herein.

PASSED, ADOPTED, AND APPROVED this 13th day of June, 2017

Christine J. Schaefer, Chair

I, Joseph L. Lambert, Secretary to the Planning Commission of the City of Placentia, do hereby certify that the foregoing Resolution was adopted at a regular meeting

of the Planning Commission of the City of Placentia held on
June 13, 2017, by the following vote:

AYES: HILL, SCHENCK, SOLOMONSON, PEREZ, SCHAEFER
NOES: NONE
ABSENT: LEE, TOMAZIC
ABSTAINED: NONE

ATTEST:

Joseph L Lambert,
Secretary to the Planning Commission

APPROVED AS TO FORM

CHRISTIAN BETTENHAUSEN,
CITY ATTORNEY

City of Placentia - General Plan Conformity		
Capital Improvement Projects		
Project #	Project Name	General Plan Element
PW 2017-02	Golden Avenue Bridge Design and Rehabilitation	Circulation
PW 2016-33	Placentia Avenue Rehabilitation	Circulation
PW 2017-03	Pedestrian Accessibility Project Phases VI-X	Circulation
PW 2017-05	Residential Slurry Seal Project	Circulation
PW 2017-04	Kraemer Boulevard Street Resurfacing Project	Circulation
PW 2017-06	Turf Grass Median Renovation Project	Circulation
PW 2017-07	Chapman/Placentia Avenue Intersection Project	Circulation
PW 2017-08	Placentia Avenue Traffic Signalization Coordination Project	Circulation/Land Use
PW 2017-09	Safe Routes 2 School Project Phase III	Circulation
PW 2017-10	Chapman/Malvern Traffic Signal Synchronization Project	Circulation/Land Use
PW 2017-11	Miraloma/Richfield Traffic Signal Improvement Project	Circulation/Land Use
PW 2017-12	Citywide Traffic Signal Repair Project	Circulation
PW 2017-13	Systemic Safety Analysis Report	Circulation
PW 2017-14	Kraemer/Alta Vista Traffic Signal Improvement Project	Circulation
PW 2017-15	Bastanchury/Valencia Traffic Signal Improvement Project	Circulation
PW 2017-16	Imperial Highway Traffic Signal Synchronization Project	Circulation
PW 2017-17	Old Town Sewer Rehabilitation Project	Land Use/Open Space/Housing
PW 2017-18	Catch Basin Screens/Full Capture Systems Citywide Phase IV	Land Use/Open Space/Housing
PW 2017-19	Old Town Metrolink Station and Parking Structure	Land Use/Circulation
PW 2017-20	Veterans Monument Expansion Project	Land Use
PW 2017-21	City Council Chambers Renovation Project	Land Use
PW 2017-22	City Yard Secured Access Gate Project	Land Use
PW 2017-23	Police Department Men's Locker Room Improvemetrn Project	Land Use/Seismic Safety
PW 2017-24	City Hall Flooring Improvement Project	Land Use
PW 2017-25	Champion Sports Complex Improvements	Parks and Recreation
PW 2017-26	Parque Del Arroyo Verde Playground Improvement Project	Parks and Recreation
PW 2017-27	GIS System Upgrades	N/A
PW 2017-28	Bitech Finance System Upgrade Project	N/A
PW 2017-29	EOC Technology Upgrade Project	N/A
PW 2017-30	Police CAD System Upgrade	N/A
N/A	Sewer System Master Plan	Land Use/Housing
N/A	Old Town Parking Management Plan	Land Use/Housing
N/A	Citywide Park Master Plan	Parks and Recreation/Open Space
N/A	Community Circulator/Shuttle Planning Document	Land Use/Circulation
N/A	General Plan Update & Environmental Clearance	Land Use/Housing



Placentia Planning Commission

AGENDA STAFF REPORT

TO: PLANNING COMMISSION

VIA: DIRECTOR OF DEVELOPMENT SERVICES

FROM: JENNIFER DAVIS, CONTRACT PLANNER

DATE: JUNE 13, 2017

SUBJECT: **REVIEW OF THE OLD TOWN REVITALIZATION PROJECT WHICH INCLUDES THE ADOPTION OF THE OLD TOWN REVITALIZATION PLAN, THE OLD TOWN STREETScape MASTER PLAN, GENERAL PLAN AMENDMENT 2017-02 AND ZONE CHANGE 2017-02 TO ESTABLISH THE OLD TOWN ZONING DISTRICT AND NEW DEVELOPMENT STANDARDS**

RECOMMENDATION:

It is recommended that the Planning Commission take the following actions:

1. Open the Public Hearing concerning the Old Town Revitalization Plan, the Old Town Streetscape Master Plan, the Initial Study and Negative Declaration, General Plan Amendment 2017-02 and Zone Change 2017-02; and
2. Receive the staff report and consider all public testimony; and
3. Close the public hearing; and
4. Adopt Resolution PC-2017-11, A Resolution of the Planning Commission of the City of Placentia, recommending that City Council approve General Plan Amendment 2017-02 to change the current land use designation from Commercial, Medium Density Residential and High Density Residential to the new Old Town designation for the project area; and, to approve Zone Change 2017-02, changing the existing zoning from SF-C, C-1, C-2, and R-2 and creating a new zoning district "Old Town" and development standards for the "Old Town Placentia" district for the project area; recommending adoption of the Old Town Streetscape Master Plan; recommending adoption of the Old Town Placentia Revitalization Plan; and recommending the adoption of a Negative Declaration pursuant to the California Environmental Quality Act Guidelines (CEQA) set forth in Title 14 CCR § 15074 and the City of Placentia Environmental Guidelines.

PROJECT SUMMARY

The City of Placentia has been given a golden opportunity in the form of a new Metrolink station right near the City's town center in Old Town Placentia. The station will be located near Melrose Street and Crowther Avenue just east of the Orange Freeway (SR-57). This new station will be the 13th station on the Metrolink 91/Perris Valley Line which runs from Los Angeles' Union Station to Downtown Riverside and then further east to Perris. In order to capitalize on this opportunity the

City recently adopted a Transit Oriented Development zoning district in the area south of the BNSF railroad tracks. To further this positive transition, the City is now considering creating a new Zoning and General Plan district for the Old Town Placentia area north of the railroad tracks. The positive effect of Metrolink passengers and the convenience of living near a train station provides a foundation for revitalization of the City's historic core. Leveraging this significant asset to transform this area is at the heart of the vision for both the Transit Oriented Development (TOD) zoning district to the south and now for the Old Town Placentia zoning. These two new zoning districts, with accompanying new development standards, seek to provide a tool to revitalize the Old Town district that will be served by the transit station.



The Old Town is located in Placentia's southwestern quadrant and is a part of the north Orange County area. The Plan Area is about 32 acres in size and consists of 147 parcels made up of a diverse mix of commercial land uses, surrounded by residential neighborhoods to the west, east, and north. There are a few residential uses. Four community churches are within and surrounding the Old Town. Community facilities include the senior center and the fire station. Many parcels are less than 5,000 square feet in size, which contributes to the small scale of the Old Town.

The proposed zone change would create the Old Town Zoning District in an area along Bradford and Santa Fe Avenues (see map below). The new zoning district would include all new development standards that would be used to guide the orderly development of remodeling, new construction, and other revitalization efforts to help create a vibrant, thriving downtown area.

The revitalization efforts encompass three separate components: 1) the Old Town Revitalization Plan; 2) Zoning and Development Standards; and 3) the Master Streetscape Plan. Each are described below.

Old Town Revitalization Plan Component

This document contains the overall vision for the area. It seeks to promote an active Old Town and to create a central gathering space inclusive of all population groups in the City and the region: a place where residents can walk to the corner store, go to dinner, congregate, shop, and relax in one place without parking more than once. The Plan strives to guide improvements in the built environment of the Old Town while also enhancing the human environment, creating more opportunities for people to interact. It is not prescriptive but rather seeks to provide guidance and recommendations in the following topic areas, with the goal of activating and revitalizing the Old Town area:

1. Mobility – including transportation circulation, transit, bicycle network, pedestrian network, and “flex streets.”
2. Infrastructure – including utility systems.
3. Streetscapes- the public realm, including open space and plazas, street furniture, landscaping, signage and lighting.
4. Parking – for businesses and users of the transit station.
5. Land Use – uses, zoning, and architectural styles.
6. Historic Resources – promoting conservation where appropriate.
7. Business Support – programs to help the businesses.
8. Programming – special events such as festivals.
9. Maintenance – ongoing street improvement maintenance.

The Plan also contains brief sections on funding and implementation. The complete Old Town Revitalization Plan document is attached to this report.

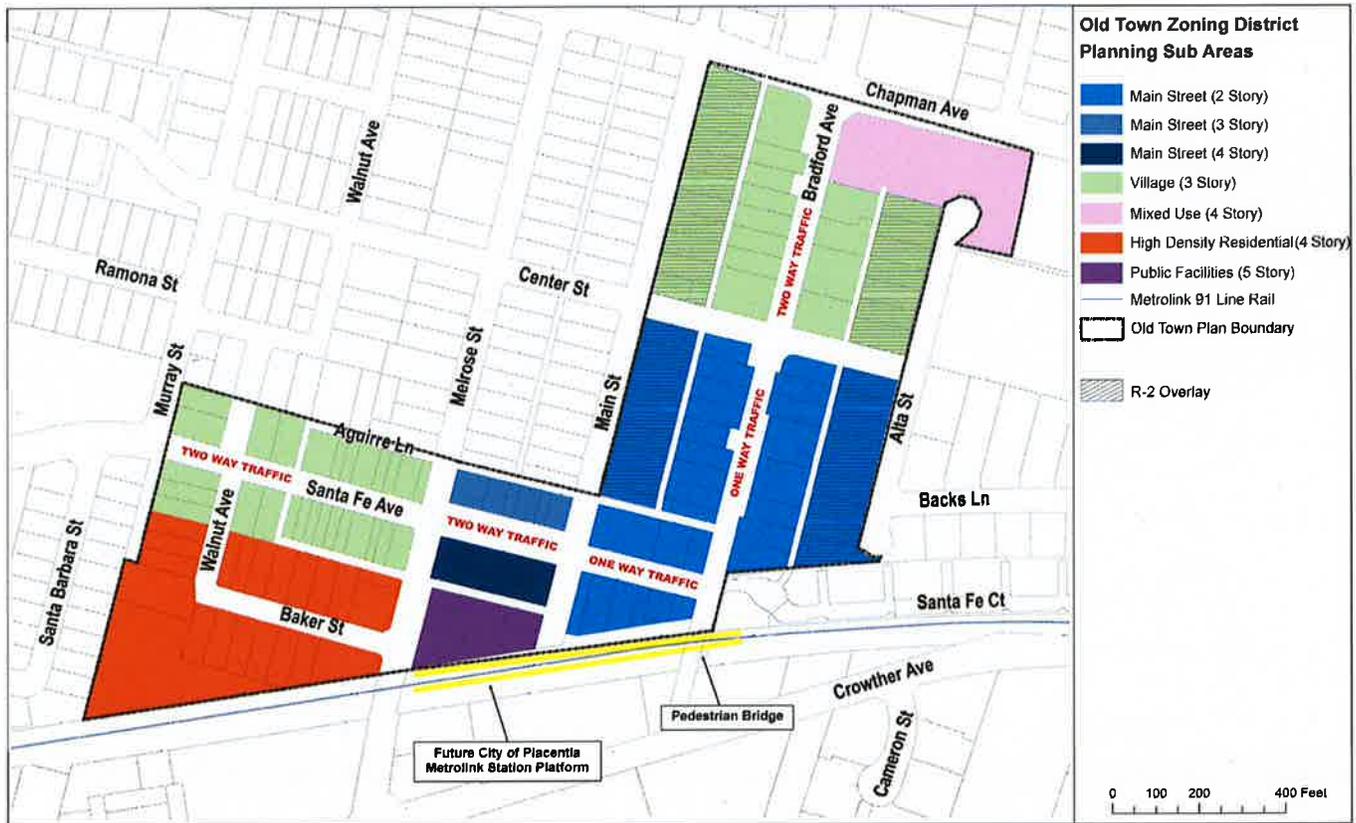
New Zoning and Development Standards Component

Objectives. As one of the three revitalization elements, the new zoning and development standards play the biggest part in furthering the vision for this area. The purpose of these standards is to facilitate the enhancement of the City’s historic core, characterized by its small-scaled village atmosphere, shopfront architecture, mix of uses and inviting streetscapes. Specifically, the new regulations are aimed at accomplishing the following objectives, complementing the objectives of the TOD Packing House District to the south:

- A. Meet the goals, objectives and follow the recommendations for Old Town as detailed in the Old Town Revitalization Plan;
- B. Preserve and enhance the distinguishable “village” look and feel of Old Town Placentia;
- C. Preserve the original Old Town grid pattern comprised of short, walkable blocks;
- D. Preserve the diverse mix of land use types while attracting a more diversified commercial base that creates a shopping destination and contributes to a more pedestrian-friendly environment;
- E. Encourage infill development that contributes to creating a compact walkable environment important to a vital town center;
- F. Support businesses and attract new investment through enhanced safety and security measures, development incentives, a high level of amenities and adaptive reuse standards;
- G. Create fine-grained detail in architectural and urban form that provides interest and complexity at the level of the pedestrian and bicyclist;
- H. Enhance public safety by improving street lighting, public services, streetscapes and alleys, and police surveillance;
- I. Support multimodal transportation that improves ease of access to a variety of transportation methods such as walking, cycling and public transit;
- J. Create a sustainable environment through standards that result in “green” buildings and infrastructure, as well as a safe, healthy and attractive environment;
- K. Preserve and highlight historic features that make Old Town Placentia a unique destination;
- L. Provide for public gathering spaces and small passive recreation places;

- M. Improve connections between Old Town and the region through increased transit ridership and the provision of hubs for various transportation ;
- N. Maintain an adequate level of parking and access for automobiles;
- O. Continue to brand Old Town Placentia through wayfinding, welcome signs, banners, public art, creative public spaces, a focal point (i.e., gazebo), etc.;
- P. Create a physical and visual connection with the TOD Packing House District to the south through interconnected plazas on both sides of the rail line; and
- Q. Ensure high quality architecture (360 degree design) that embraces the character of Old Town and its architecture and urban design.

Zoning Districts. The Old Town currently has four different zoning districts. The Plan Area includes all of the current “SF-C” Santa Fe Commercial District zone (Chapter 23.37 of the Code) and other parcels in the “C-1”, “C-2”, and “R-2” zones (see Existing Zoning Map below). The proposed zoning will create an Old Town designation and within this designation there will be seven subareas. These subareas help to refine the height, uses and architecture of different areas of Old Town and were drawn around distinctive areas or locations in the Plan area, with distinctive characters. In addition, along Main and Alta Street, there is an R-2 overlay zone that will allow those existing residential uses to remain without acquiring a non-conforming status. The subareas are briefly described below:



Main Street (Two, Three and Four Story Height Subareas). This subarea is found along both Bradford Avenue and Santa Fe Avenue from Center Street to Main Street and will be small in scale with frontages that blend with the streetscapes. The height limit varies from two to four stories, with the higher four story height limit being permitted along Santa Fe Avenue. The focus is on historic conservation, Main Street architecture and infill development to create a concentrated, active area of Old Town. Uses would be limited to commercial uses that occupy smaller spaces and attract a large number of visitors. The Development Standards for this subarea call out several parking reductions.

The Main Street (MS) subarea provides for the preservation and enhancement of the unique character of the historic Old Town commercial area with commercial block building forms and shopfront architecture. The north and south sides of Santa Fe Avenue from its intersection with Bradford Avenue to the first block west of Melrose Avenue is characterized by commercial block style buildings at the back of the sidewalk. This pattern would be repeated along the Bradford Avenue segment. The historic character of existing buildings is to be preserved where sufficient architectural integrity remains, and new buildings are to be designed to be compatible with the historic character of the area in the commercial block style.

Allowable land uses emphasize ground floor retail, restaurant, and related uses, with opportunities for offices above. The restoration and renovation of existing façades is encouraged. Parking is on-street, in existing and proposed surface lots, and in park-once surface lots/garages. Buildings are to be built to the front property line along both Bradford Avenue and Santa Fe Avenue. A small rear, alley set back is required for vehicle loading, amenities, and for each business and building.

Village (3 Story Height Subarea). This subarea is found in two separate places. One at the north end at the southwest corner of Chapman Avenue and Bradford Avenue extending south along both sides of Bradford Avenue and the other at the west end along both sides of Santa Fe Avenue from Melrose Street on the east to Murray Street on the west. The intent is to create new and infill development opportunities while recognizing existing historically significant properties such as the old City Hall and the Library.

The Village (V) subarea provides for a new walkable retail and office center as an extension of the Main Street district. The Village subarea would provide for the same building form and setbacks as the Main Street subarea but would allow three stories. In this zone, the primary frontages along Bradford and Santa Fe Avenues would orient to the street with zero to five foot front setbacks and a zero to fifteen foot rear setback for secondary frontages along the alleys. Ground floor spaces would be occupied by retail and restaurants, with offices and residential above. Building types are intended primarily to be a cohesive design with well-defined frontages in the shopfront style similar to the Main Street subarea.

High Density Residential (Four Story Height Subarea). There is one subarea in the Old Town designated for high density residential development. This can be four stories in

height and is located along Baker St and Walnut Avenue at the south west corner of the Plan area, just north of the railroad tracks.

Mixed Use (Four Story Height Subarea). This subarea is found at the southeastern corner of Bradford and Chapman. The Mixed Use subarea provides for a mix of concentrated urban housing types with some commercial uses, within convenient walking distance of the Main Street and Village subareas.

Public Facilities (Five Story Height Subarea). A public parking structure will be constructed in conjunction with the new Metrolink Station and it will serve both train riders and those visiting the Old Town.

R-2 Overlay. Along Alta and Main Streets, in the Main Street and Village subareas, from Chapman to Aguirre Lane extension, there is an R-2 overlay which allows existing residential uses to remain and limits the commercial uses fronting on these residential streets.

Zoning Development Standards. This part of the Zoning Code amendment focuses on specific standards for each subarea in the plan. The standards for this area are much more prescriptive in terms of land uses, parking, setbacks, etc. than the current Zoning Code. Many of these include minimum and maximum standards, unlike many existing zoning codes. These minimum/maximum standards help further the tenets of TOD but not allowing developers to “over park” their projects. In this way, these standards help guide new development in a thoughtful manner, helping to achieve the City’s objectives for revitalizing Old Town. The full development standards document is attached to this report but highlights include:

- Mixed-Use development (residential and non- residential uses in same building)
- Higher Density residential development (max 65 du/ac)
- Varying building heights (from 30’ up to 65’, depending on the subarea) and minimum ground floor height (15’)
- Refined list of land uses: active commercial and multi-family (commercial uses that would complement an active pedestrian oriented environment)
- Fewer parking spaces required, including a “maximum number” and parking reduction allowances (see below)
- Shopfront standards (see below)
- Public art and plazas encouraged
- Improved signage regulations
- Amortization (see below)
- Additional definitions

Shopfronts. One section of the standards provides detailed guidelines for actual shopfronts, thereby preserving the small-scale commercial shopfront that is characteristic in Old Town. These guidelines are as follows:

1. 12 feet to 16 feet tall, as measured from the adjacent sidewalk.

2. The corresponding storefront(s) opening(s) along the primary frontage shall comprise 65% of the 1st floor wall area facing the street and not have opaque, stucco or reflective glazing.
3. Storefronts may be recessed from the frontage line by up to 10 feet.
4. "Bulkheads," which are a segment of wall which transitions between the window opening(s) and the adjacent grade, are also encouraged. The bulkhead shall be between 24 inches and 36 inches tall (aluminum storefront or spandrel panel may not substitute for a bulkhead).

Architecture. Another standard that is specific to Old Town is that the architecture of new development is required to be one of three styles: Main Street Commercial; Mission Revival; or Spanish Colonial. The table below shows which styles are permitted in which subareas:

Architectural Styles		
<p>Main Street Commercial Multi-story facades are typically divided into base, body and top with the ground floor taller than the shorter upper floor which is finished by a significant parapet. The ground floor has recessed entries and any expansive glass is interrupted by structural columns with transoms to allow light to penetrate deep into the interior. Upper floor windows are smaller with vertical windows directly relating to the ground floor openings.</p>		<p>Style of architecture required in Main Street Subareas</p>
<p>Mission Revival Prominent features of the style include red clay tile roofs, use of balconies, smooth-stuccoed exterior walls usually painted white, arched openings, colorful tile work and elaborate landscaping. The buildings frequently have courtyards.</p>		<p>Choices of architecture for the following:</p> <ul style="list-style-type: none"> • Village Subarea • Mixed Use Subarea • High Density Residential Subarea

<p>Spanish Colonial Typical features include a low-pitched roof with little or no eave overhang, a red-tiled roof, perhaps a prominent rounded arch over a door, window or porch, a stucco wall surface, and usually an asymmetrical façade. The features elaborate molded ornament around doors and windows, polychrome tile at ingresses and wrought iron grilles and balconies.</p>		<p>This style of architecture permitted only in High Density Residential Subarea</p>
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Parking Standards. The parking standards identify the parking requirements for new land uses in new or modified buildings in Old Town Placentia. In some instances, applicants may also elect to pay an in-lieu fee or may be eligible for a parking exception as outlined below:

- 1. In-Lieu Parking Fee.** Applicants may elect to pay an in-lieu fee in exchange for meeting the parking requirements in the Main Street Subarea or in the Village Subarea.
- 2. Main Street Subarea Parking Exception.** Providing parking in this subarea is optional. However, in the 3 and 4 story areas of Main Street Subarea, parking shall be required for third and fourth floor uses, according to this section, or by payment of an in-lieu parking fee.
- 3. Village Subarea Parking Exception.** Parking only required for the second and third stories, or by payment of an in-lieu parking fee. Parking for the first (ground) floor of existing and future buildings shall not be required.
- 4. Parking Reduction.** Applicants may apply for a parking reduction before the Planning Commission for Village, High Density Residential and Mixed Use Subareas up to a maximum reduction of 25% through a parking demand study or shared parking analysis. One such incentive could include a Zip car or shared car plan. Parking demand studies shall be reviewed and approved at a noticed public hearing before the Planning Commission, or by the Director if a non-discretionary review.

Amortization for Existing Uses. The proposed amortization clause in the development standards seeks to preserve private property rights in existence at the time of this zone change. Immediately, all legal uses, buildings or structures may be continued to operate as a legal nonconforming use, building or structure. Additionally, for the five-year period after the zone change is adopted, these same legal nonconforming uses may be transferred or assigned. After this five-year period has elapsed, legal nonconforming uses can continue if the business is transferred to a child or parent of the owner or to the owner's employees. The businesses may not be modified or expanded, however secondary uses are permitted to change. Due to its historic nature, the existing church building at 102 S. Bradford is exempt from the amortization clause altogether.

Streetscape Master Plan Component

Streetscapes are the common connectors between plazas, spaces, parking, and buildings. During public review of the Old Town revitalization project, the public requested a consistent aesthetic that would include curb appeal, earth tones, welcome signs, and street lights with hanging plants and other amenities such as bike racks, benches, and planters. Street art and public plazas were also desired. An important way to ensure the coordinated approach to improving the public streets and sidewalks is by adopting streetscape master plan that guides improvements in the public realm. Staff coordinated the creation of a Streetscape Master Plan for the Old Town area, emphasizing these components. In addition, photo simulations of typical cross sections of the Old Town streets were drawn to help envision just how much the improvements in the public realm can uplift the Old Town area. The complete Streetscape Master Plan and photo simulations are attachments to this report.



The Streetscape Master Plan dictates the specific amenities for the Old Town area, so that these elements will be consistent throughout and give the area a sense of place and a feeling of cohesiveness. These include bike racks, lighting, trash receptacles, bollards

and seating. In addition the Master Plan identifies the locations of bollards that can be removed or added as needed for public events. These public events will help to enliven the area.

GENERAL PLAN AMENDMENT

The creation of the new Old Town zone requires that both the text and the map of the General Plan be amended. The proposal is to amend Land Use Element Section 5.5 LAND USE DESIGNATIONS, adding the following designation and definition:

Old Town General Plan Designation - This designation permits a mixture of housing, retail, office, and/or other similar uses integrated into a walkable neighborhood and located within a half-mile of quality public transportation. Typically, this type of development is characterized by a mixture of medium-high to high density residential development (30 to 65 dwelling units/acre), and neighborhood-supporting mixed-use areas designed to be contextually appropriate in and compatible with the identified neighborhood or historic area. On a single site, a combination of non-residential and residential uses can occur in the same structure or on the same site, where the residential component is located either above (vertical mixed-use) or behind or next to (horizontal mixed-use) the non-residential component. Commercial retail is encouraged to be the primary use on the ground floor. Professional office and housing uses are also encouraged, particularly as adaptive reuse

opportunities within existing structures. This designation encourages lower parking standards, enhanced pedestrian environment, active streetscape, and enhanced amenities for bikes and buses. Similar to the TOD area, the pedestrian and transit rider are given priority over the motorist. Transit orientation, walkability, and pedestrian access are key considerations. This land use designation shall feature sites reserved solely for residential development (30 to 65 dwelling units/acre) and for mixed-use developments.

GENERAL PLAN MAP AMENDMENT

This project requires the General Plan Map to be revised to change the current land use designation from the Commercial, Medium Density Residential and High Density Residential to the new Old Town designation for the project area. This new designation helps to consolidate the project area, making it easier to attract and guide development.

COMMUNITY INPUT

The proposed development standards and zone change has been presented to the public at various meetings and through various media outlets. Staff has received a majority of positive feedback from the community. The table below outlines the community outreach to date:

Date	Meeting	Location
January 26, 2016	Economic Development Committee	City Hall
February 17, 2016	Community Meeting	Senior Center
February 23, 2016	Economic Development Committee	City Hall
April 12, 2016	Placita Santa Fe Merchants Meeting	Tlaquepaque
May 4, 2016	Community Meeting	Bachs Community Center
May 5, 2016	Chamber of Commerce Board Meeting	Chamber Office
June 14, 2016	Placita Santa Fe Merchants Meeting	Tlaquepaque
October 11, 2016	Placita Santa Fe Merchants Meeting	Tlaquepaque
November 28, 2016	Planning and Development Ad Hoc Committee	City Hall
November 29, 2016	Economic Development Committee	City Hall
December 6, 2016	City Council Study Session (1 of 2)	Chambers
January 5, 2017	Chamber of Commerce Board Meeting	Chamber Office
January 10, 2017	Placita Santa Fe Merchants Meeting	Tlaquepaque
January 25, 2017	Community Meeting	Senior Center

Date	Meeting	Location
February 14, 2017	Placita Santa Fe Merchants Meeting	Tlaquepaque
March 14, 2017	Placita Santa Fe Merchants Meeting	Tlaquepaque
March 16, 2017	Chamber of Commerce Board Meeting	Chamber Office
April 10, 2017	City Council Study Session #2	City Hall
May 18, 2017	Planning and Development Ad Hoc Committee	City Hall

OTHER CITY ACTIONS TO IMPLEMENT

In order to wholly implement the vision of the TOD zone, several other actions by the City are required. These include the following:

1. Complete a Wayfinding Program;
2. Adopt a Public Art Policy and Procedure;
3. Adopt an Old Town Impact Fee Program and Update the current Fee Schedule;
4. Create additional permit applications and procedures for: creative sign; master sign program; and outdoor dining;
5. Create a “peer review” architectural and urban design review process;
6. Establish an in-lieu parking fee procedures; and
7. Ensure OCTA Bus Routes service the TOD and Old Town Placentia areas.

CEQA:

An initial study was conducted by Blodgett Baylois Environmental Planning, a professional environmental firm. This document evaluated all CEQA issues contained in the latest Initial Study Checklist form. Because this is principally an infill plan for revitalization, the Initial Study found no impacts in any of the environmental areas analyzed and therefore no mitigation was required. Therefore the environmental document to be adopted is a Negative Declaration.

A Notice of Intent to Adopt a Negative Declaration (NOI) was issued for this project by the City of Placentia. The Initial Study and Notice of Intent to Adopt a Negative Declaration was circulated for 20 days for public comment, running from Tuesday, May 23, 2017 to Monday, June 12, 2017.

Prepared and submitted by:



 Jennifer Davis
 Contract Planner

Reviewed and approved by:



 Joseph M. Lambert
 Director of Development Services

Attachments:

1. Resolution No. PC-2017-11, with Exhibits:
 - Exhibit A: Map of Old Town Zone
 - Exhibit B: Amendment to the City of Placentia General Plan Land Use Element Section 5.5
 - Exhibit C: Old Town Placentia Revitalization Plan
 - Exhibit D: Old Town Streetscape Master Plan, with cross sections and photo simulations of cross sections
 - Exhibit E: Old Town District Development Standards, adding Chapter 23.112 to the Placentia Municipal Code and amending Chapter 23.08.010 of the Placentia Municipal Code
2. Initial Study, Draft Negative Declaration and Initial Study Appendices

RESOLUTION NO. PC-2017-11

AN RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF PLACENTIA, CALIFORNIA RECOMMENDING TO THE CITY COUNCIL 1.) AN AMENDMENT TO THE a.) CITY OF PLACENTIA GENERAL PLAN, b.) CITY OF PLACENTIA MUNICIPAL CODE THAT ADDS CHAPTER 23.112 ENTITLED “OLD TOWN PLACENTIA DEVELOPMENT STANDARDS”, AND c.) OFFICAL ZONING MAP OF THE CITY OF PLACENTIA; AND 2.) ADOPTING a.) A STREETScape MASTER PLAN AND b.) THE OLD TOWN PLACENTIA REVITALIZATION PLAN AND c.) A NEGATIVE DECLARATION FOR THE PROPOSED PROJECT

A. Recitals.

(i). On June 13, 2017, the Planning Commission of the City of Placentia conducted, and concluded, a duly noticed public hearing, as required by law, to amend the General Plan and amend Chapter 23.08.010 of the Placentia Municipal Code and the add Chapter 23.112 entitled “Old Town Placentia Development Standards” to the City of Placentia Municipal Code creating development standards for properties within the Old Town boundaries and amending the Official Zoning Map of the City of Placentia.

(ii). An Initial Study was prepared to review and consider the environmental impacts of the Old Town District Development Standards in accordance with the California Environmental Quality Act and the City of Placentia Environmental Guidelines as codified in Title 14 of the California Code of Regulations (“CCR”) Section 15000 et. seq., Public Resources Code Section 21000 et. seq. and the City of Placentia Environmental Guidelines.

(iii). On or about May 23, 2017, based upon the Initial Study, the City of Placentia, as lead agency, published a Notice of Intent to Adopt a Negative Declaration (“ND”) in accordance with CCR Section 15072. Thereafter, on or about May 23, 2017, the City of Placentia published the ND which found that the project will not have a potentially significant effect on the environment in accordance with CCR Section 15070.

(iv). On or about May 23, 2017, the City published the draft ND for a period of 20 days for public comment in accordance with CCR Section 15073.

(v.) The City of Placentia provided notice of public hearing in accordance with California Government Code Section 65090 and the City of Placentia Municipal Code Section 23.96.030 by publication in a local newspaper at least 10 days prior to the public hearing and by direct U.S. mail to all property owners within 300-feet of the proposes project boundaries.

(vi.) All other legal prerequisites to the adoption of this Ordinance have occurred.

B. Resolution.

NOW, THEREFORE, the Planning Commission of the City of Placentia makings the following findings and recommendations to the City Council:

SECTION 1. The recitals set forth above are true and correct and adopts those recitals as though fully set forth herein.

SECTION 2. Find that the Negative Declaration circulated for public review contains all contents as required in CCR Section 15071. Moreover, the City has complied with all requirements of the California Environmental Quality Act and the City of Placentia Environmental Guidelines as codified in Title 14 of the California Code of Regulations (“CCR”) Section 15000 et. seq., Public Resources Code Section 21000 et. seq. and the City of Placentia Environmental Guidelines.

SECTION 3. Certify the Negative Declaration for the “project” including the general plan amendment, zoning code amendment, zoning map amendment and the streetscape master plan standards and find that the project will not have a potentially significant effect on the environment in accordance with CCR Section 15070.

SECTION 4. Approve an amendment to the City of Placentia General Plan Land Use Element Section 5.5 entitled “Land Use Designations” to allow the “Old Town” land use designation and definition as set forth in Exhibit “B.”

SECTION 5. Approve an amendment to the City of Placentia Municipal Code by amending Chapter 23.08.010 of the Placentia Municipal Code and adding Chapter 23.112 entitled “Old Town Placentia Development Standards” to the City of Placentia Municipal Code creating development standards for properties within the Old Town boundaries as set forth in Exhibit “E” and find, in accordance with Chapter 23.96 (“Amendments”) of the City of Placentia Municipal Code that said amendment to the municipal code will not be detrimental to the health, safety or general welfare of the persons residing or working within the neighborhood of the proposed amendment or within the city; injurious to property or improvements within the neighborhood or within the city; nor inconsistent with the latest adopted General Plan.

SECTION 6. Amend the Official Zoning Map of the City of Placentia, maintained in accordance with the provisions of § 23.08.020 of the Placentia Municipal Code, as set forth in Exhibit “A” attached hereto and by this reference made a part hereof.

SECTION 7. Adopt the Streetscape Master Plan as set forth in Exhibit “D” which provides direction for public right of way and streetscape improvements including street furniture, street/traffic lights, landscaping, pavement and other standards in order to provide unifying, consistent and aesthetically pleasing features of the public areas within the Old Town project area.

SECTION 8. Adopt the Old Town Placentia Revitalization Plan as set forth in Exhibit "C" which provides direction and contains the overall vision for the Old Town project area.

SECTION 9. Prior to taking this action, the Planning Commission reviewed, considered and has exercised its independent judgment based on substantial evidence on the Negative Declaration and all of the information and data in the administrative record, all oral and written testimony received and finds that the Negative Declaration was prepared in full compliance with the California Environmental Quality Act.

SECTION 10. If any section, subsection, sentence, clause, or phrase of this resolution and/or the documents in support of this resolution is/are for any reason held to be invalid or unconstitutional by the decision of any court of competent jurisdiction, such decision shall not affect the validity of the remaining portions of this resolution.

Section 11. The Secretary to the Planning Commission shall certify to the adoption of this Resolution.

PASSED and ADOPTED this 13th day of June, 2017.

CHRISTINE SCHAEFER, CHAIR

I, Joseph M. Lambert, Secretary to the Planning Commission of the City of Placentia, do hereby certify that the foregoing Resolution was introduced at a regular meeting of the Planning Commission of the City of Placentia held on the 13th day of June, 2017, and was passed at this regular meeting of the Planning Commission of the City of Placentia held on the 13th day of June, 2017, by the following vote:

AYES:

NOES:

ABSENT:

ABSTAINED:

ATTEST:

Joseph M. Lambert,
Secretary to the Planning Commission

APPROVED AS TO FORM:

YOLANDA M. SUMMERHILL
ASSISTANT CITY ATTORNEY

Exhibit A: Map of Old Town Zone

Exhibit B: Amendment to the City of Placentia General Plan Land Use Element Section 5.5

Exhibit C: Old Town Placentia Revitalization Plan

Exhibit D: Old Town Streetscape Master Plan, with cross sections and photo simulations of cross sections

Exhibit E: Old Town District Development Standards adding Chapter 23.112 to the Placentia Municipal Code and amending Chapter 23.08.010 of the Placentia Municipal Code

EXHIBIT A

OLD TOWN ZONE BOUNDARIES

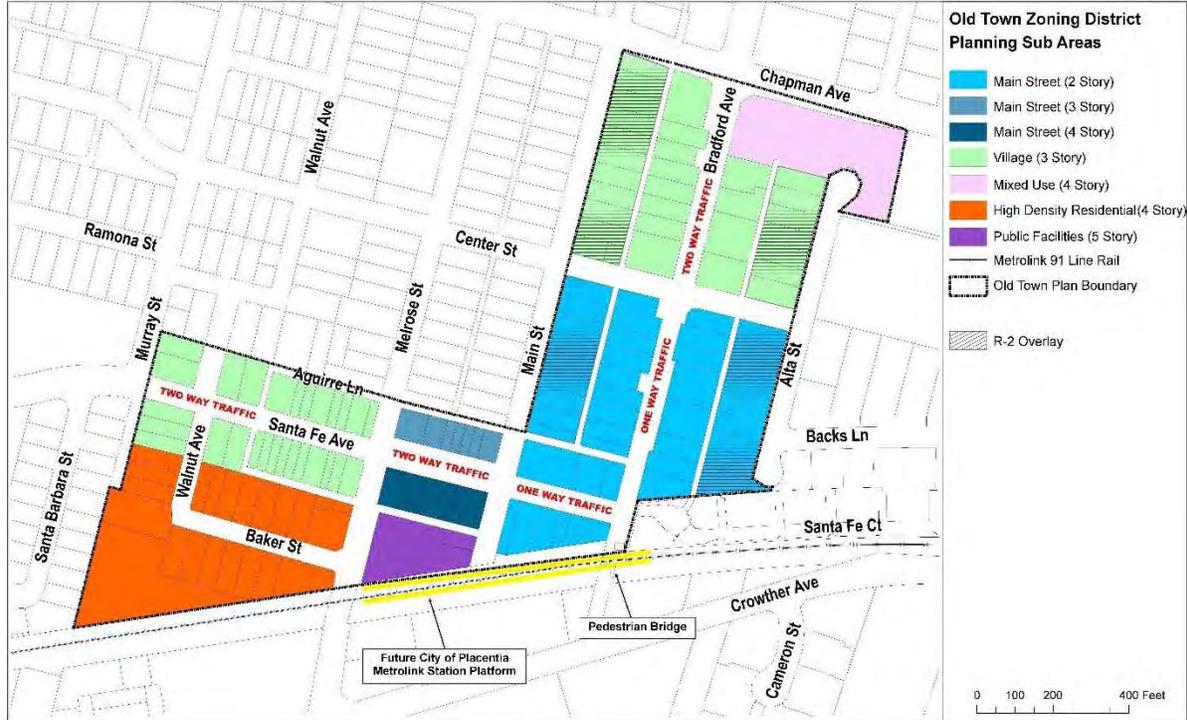


EXHIBIT B

Amendment to the City of Placentia General Plan Land Use Element Section 5.5

Old Town General Plan Designation - This designation permits a mixture of housing, retail, office, and/or other similar uses integrated into a walkable neighborhood and located within a half-mile of quality public transportation. Typically, this type of development is characterized by a mixture of medium-high to high density residential development (30 to 65 dwelling units/acre), and neighborhood-supporting mixed-use areas designed to be contextually appropriate in and compatible with the identified neighborhood or historic area. On a single site, a combination of non-residential and residential uses can occur in the same structure or on the same site, where the residential component is located either above (vertical mixed-use) or behind or next to (horizontal mixed-use) the non-residential component. Commercial retail is encouraged to be the primary use on the ground floor. Professional office and housing uses are also encouraged, particularly as adaptive reuse opportunities within existing structures. This designation encourages lower parking standards, enhanced pedestrian environment, active streetscape, and enhanced amenities for bikes and buses. Similar to the TOD area, the pedestrian and transit rider are given priority over the motorist. Transit orientation, walkability, and pedestrian access are key considerations. This land use designation shall feature sites reserved solely for residential development (30 to 65 dwelling units/acre) and for mixed-use developments.

EXHIBIT C

Old Town Placentia Revitalization Plan

June 13, 2017

Draft

Old Town  *Placentia*

Revitalization Plan

Prepared by

*High Peaks Planning
for the City of Placentia*



OLD TOWN PLACENTIA REVITALIZATION PLAN

1. Introduction

This Old Town Placentia Revitalization Plan (“Plan”) is a comprehensive plan for the revitalization, activation, and growth of the City’s original central business district founded in 1910 near the site of the original Placentia train depot and citrus packing houses. The area suffered from disinvestment and economic decline with the widespread growth of Orange County’s suburbanization starting in the 1960s.

However, existing property owners, residents, and businesses have kept the area active with a unique and diverse mix of small businesses, restaurants, churches, and other uses. This activity has helped this area retain some of its small town charm. Among Orange County’s old towns, Placentia has retained its unique small scale environment with a rich variety of land uses.

Today, population groups of all types are looking for small scale, walkable urban centers located around transit to live, work, and play. This is occurring in Orange County with new, more compact, and transit-oriented developments and historic reuse districts emerging in Anaheim, Orange, Fullerton, and Santa Ana. Coupled with the recently adopted Transit Oriented Development (TOD) Packing House District south of the railroad along Crowther Avenue, there is a unique opportunity for Old Town to see significant revitalization.

This Plan focuses on an 18-year period, from 2017 to 2035 and is expected to be implemented in three phases under a partnership between Elected Officials, City Staff regional agencies, local businesses, residents, community organizations, and other stakeholders.

PURPOSE

The purpose of this Plan is to enhance the physical and economic environment in the City’s Old Town aimed at creating a lively destination to support the current economic base, enhance the town center of Placentia, and better connect to adjacent neighborhoods and surrounding cities. The area’s assets include a walkable layout with its short blocks across a linear street pattern. The Old Town, also known as “Placita Santa Fe”, has a good mix of commercial uses including popular restaurants, shops, and neighborhood serving stores.

In 2016, the City of Placentia and the Orange County Transportation Authority (OCTA) approved a new Metrolink Station in the Old Town area. This Station, to be completed by 2019, is a primary element in the planned changes to the Old Town. The Station will have two

OLD TOWN PLACENTIA REVITALIZATION PLAN

platforms, one north of the railroad and one south of the railroad. A pedestrian overcrossing bridge and a grade separated road at Melrose Street have been completed in anticipation of the station. The City has also adopted a zone change with corresponding development standards for a mixed use, transit-oriented development area to the immediate south of the Old Town, known as the “Packing House District”. Both plans will meld a cohesive vision for the area around the new transit station and will be tied together by a shared public realm plan.



Metrolink Station. The new Placentia Metrolink Station will have two platforms, a north and south similar to this rendering.



Pedestrian Friendly. The intersection of Santa Fe Avenue and Bradford Avenue will be the focal point of the Plan in look and function.

The Plan covers both private property and public spaces, city and other government owned land, utility and service providers and addresses both public and private realms. It also provides direction on implementation and funding once this Plan is adopted. Responsibility for implementation includes the City of Placentia, regional governmental agencies, land owners, developers, business operators, and other stakeholders.

This Plan is intended to represent a shared vision for the Old Town and will allow other, more specific planning and development to occur, as well as enable direction for seeking grant funding and other funding sources for construction of public improvements and infrastructure. An amendment to the City’s General Plan and Zoning Code is being processed concurrently with the adoption of this Plan to provide a regulatory framework for private development within the Old Town.

OLD TOWN PLACENTIA REVITALIZATION PLAN

BACKGROUND

Placentia's Old Town constitutes the original town of Placentia, founded in 1910 and incorporated in 1926. The City has grown to a population of over 52,000 today. The Old Town developed around the original Placentia train depot and the citrus packing houses. Residential homes were typically Craftsman or California Bungalow style homes and duplexes built around the commercial and industrial uses. Afterwards, the Old Town transitioned to house more industrial and auto-related uses as the citrus industry declined. When the City incorporated, the Old Town was the location of City buildings, services, as well as commercial banking, shops and commerce.

The City of Placentia considers investment in its Old Town one of the top priorities and has prepared this Plan as a comprehensive action plan to guide the area's development over the coming years. Prior planning and revitalization efforts included formal City redevelopment of blighted areas around the Old Town, using redevelopment funds, creating a specific zoning designation for the "Santa Fe Commercial zoning district", and branding "Old Town Placentia" as an attraction. With the Placentia Metrolink Station nearing construction and past projects like the railroad grade separation projects, new transit oriented opportunities have presented themselves. The annual Los Posadas Tamale Festival hosted by the Placita Santa Fe Merchants Association has brought City-wide recognition to Old Town. The previously prepared Draft Westgate Specific Plan in 2006 (not adopted) was a more recent attempt to bring a broader vision to the Old Town. In that plan, revitalization of the City's historic core was included as a top priority.



OLD TOWN PLACENTIA REVITALIZATION PLAN

COMMUNITY OUTREACH

In preparation for the City of Placentia's Revitalization Plan for the Old Town area, a series of community meetings and workshops were conducted to gather the community's input and insight. This identified the community's goals and desires, and helped create a working glossary of terms related to the Plan. Data collected from the community workshops and public outreach was used to inform the vision, values, policies, and design elements of the Plan.

Outreach efforts focused on meetings, community events, public workshops, and stakeholder comments. Discussions covered a range of topics, including vision, mobility, land use/density, business uses, streetscapes and landscapes, public spaces, wayfinding, programming, implementation, and funding.

Outreach began with a collection discussion amongst a group of identified stakeholders, comprised of agencies, business owners, city-wide organizations, regional and local chambers of commerce, local merchant and business organizations, schools, and individuals from the community. Many of the identified stakeholders were similarly engaged during the 2006 Placentia-Westgate Specific Plan that was not formally adopted.

Many participants were especially concerned with Old Town's perceived lack of safety. Stakeholders expressed an overwhelming need for a plan that implemented safety improvements, including a more consistent sanitation schedule, better lighting, and improved police surveillance. Stakeholders were additionally excited about plans that would offer enhanced parking opportunities, outdoor dining, additional retail and restaurants, and an improved connectivity between the new Metrolink station and its surrounding community.

The first community workshop was held on February 19, 2016 to discuss the purpose of the plan as well as further opportunities to foster and encourage public involvement. The public was given a clear description of the project boundaries and was presented with a review of past recommendations made regarding the Westgate Specific Plan for context. Additionally, they received visual examples of design elements. Participants undertook a design exercise ("charrette") intended to gather comments, ideas, and prioritize design elements. This activity took place amongst small groups, which presented their findings to the entire audience at the end of the activity. Common concerns included: poor landscaping, lack of street lighting, challenging parking, and physical deterioration. Common priorities included: improvement of private and public spaces, enhanced streetscapes, landscapes, improved lighting, sidewalks,

OLD TOWN PLACENTIA REVITALIZATION PLAN

and alleys. The community desired an enhanced environment, that would be safer, more inviting, creating a better sense of place, while drawing from the historic importance in the area.

The second community workshop was held on May 4th, 2016, where the public was presented with both a summary of findings drawn from the previous meeting, as well as visual renderings of concepts, and alternatives for consideration. Participants discussed their impressions in small groups and emphasized the stated concerns regarding “Circulation and Infrastructure,” the public recommended better lighting and enhanced security, as well as two to four hour parking accommodations for downtown patrons. Regarding “Density and Land Use”, the public emphasized their desire for a more “open market” concept, as well as a premium coffee retailer. Regarding “Streetscape and Landscape,” the public requested a consistent aesthetic that would include curb appeal, earth tones, welcome signs, and street lights with hanging plants. They requested that bike racks, benches, planters, and welcome signs be prioritized, but they were also interested in more decorative focal points such as street art and a public gazebo in a small public plaza area. Regarding “Identity and Programming”, the public has a high interest in establishing funding streams to support improvements—examples include the establishment of a lighting district, a business improvement district, and a special assessment. Lastly, there was an almost unanimous vote to change the district’s name to “Old Town Placentia”, as well as the desire to “phase out” auto shops and “phase in” minor public/private improvements that would add to the vitality of the district.

SETTING AND BOUNDARY

DEMOGRAPHICS

According to the Southern California Association of Government (SCAG) profile published in 2014, the City has a population of over 52,000 persons, representing 1.7 percent of Orange County’s 3,113,991. Placentia experienced a growth rate that was higher than average for its area, 12.1 percent between 2000 and 2014. The population includes Hispanic (37.6 percent), Non-Hispanic White (42.9 percent), and Non-Hispanic Asian (15.0 percent), with Non-Hispanic Blacks, American Indians, and others making up the remaining 4.5 percent. There are an estimated 16,554 households within this population, and the median household income is \$76,688 as of 2014. The homeownership rate is high at 64.6 percent. According to SCAG, the most common housing type is single family detached dwelling; 70.3 percent of the homes. As

OLD TOWN PLACENTIA REVITALIZATION PLAN

of 2014, 84 percent of the population drives alone to work and 94 percent of Placentia's residents commute to other cities for their place of employment, Anaheim and Los Angeles ranking in first and second place. Placentia statistics show the City to have 19,581 jobs out of Orange County's 1,570,830 jobs. Between 2000 and 2014, Placentia saw a 3.7 percent reduction in the number of residents carpooling to work, which reflects the biggest change in transportation modes that were surveyed. There was a minimally detected increase in the number of residents who used public transportation. These demographics point to a stable, mostly single family residence, community with the potential for more sustainable transportation patterns.

EXISTING ZONING, LAND USES, AND BUSINESS TYPES

The Old Town is located in Placentia's southwestern quadrant and is a part of the north Orange County area. The Plan Area is about 32 acres in size and consists of 147 parcels made up of a diverse mix of commercial land uses, surrounded by residential neighborhoods to the west, east, and north. There are a few residential uses. Four community churches are within and surrounding the Old Town. Community facilities include the senior center and the fire station. Many parcels are less than 5,000 square feet in size, which contributes to the small scale of the Old Town.

The Plan Area is centered along Bradford Avenue and Santa Fe Avenue and is bounded by Alta Street in the east, the Burlington Northern and Santa Fe Railway train tracks in the south, Chapman Avenue and Aguirre Lane on the north and Main and Murray Street on the west. To the north along Chapman Avenue is Kraemer Memorial Park. The immediately adjacent residential neighborhood to the north and west of the Plan Area, bounded by Chapman Avenue on the north and Murray Street on the west, is made up of primarily single family and duplex homes built during the Old Town's initial development period, many designed in the Craftsman or California Bungalow architectural style.

The Plan Area includes all of the current "SF-C" Santa Fe Commercial District zone (Chapter 23.37 of the Code) and other parcels in the "C-1", "C-2", and "R-2" zones (see Existing Zoning Map below). The Plan Area does not include any schools, however, Valencia High School is located just to the north of Chapman Avenue. California State University Fullerton is located approximately two miles to the west, in the City of Fullerton. Surrounding cities include Anaheim to the south, with large industrial tracts, and Fullerton to the west with large residential and commercial zones. The 57 (Orange Freeway) and the 91 Riverside Freeway provide the most direct freeway access. Business organizations include the Placita Santa Fe

OLD TOWN PLACENTIA REVITALIZATION PLAN

Merchants Association and the Placentia Chamber of Commerce. Long time establishments in the Old Town include the American Legion Post No. 277 and the Tlaquepaque Restaurant among others.

CONSISTENCY WITH CITY POLICIES, CODES, AND PLANS

Subsequent to the proposed General Plan and Zoning Code Amendments, this Plan is intended to be consistent with the City's General Plan, Zoning Code, Municipal Codes, and local and regional planning initiatives particularly for climate action, sustainability, circulation, complete streets, and livability. The City is currently in the process of updating the City's General Plan that was adopted in the 1980s. This Plan is intended to expand and update the purpose and reach of the "Santa Fe Commercial District" zoning code provisions, creating a new zone called "Old Town," with revised development standards to accommodate the goals of the community and this Plan. This Plan has also been prepared to be complementary to the Transit Oriented Development Packing House District and development code for the area to the immediate south of the Old Town, separated by the Burlington Northern-Santa Fe (BNSF) railroad tracks.



OLD TOWN PLACENTIA REVITALIZATION PLAN

Map 1: Old Town Placentia Existing Zoning Map

2. Vision

The vision for revitalization of Old Town Placentia includes a combination of historic preservation, infill complementary development, creation of unique public spaces, and designating it transportation hub, all to provide this downtown core with a special identity to call its own. Placentia is generally considered a bedroom community, with a large portion of the City's land uses devoted to single family homes and apartment. While this character is important to the City's identity, it is also important to present a different part of the City, unique and tied to the past.

AN ACTIVE OLD TOWN

The vision for Old Town Placentia is to create a central gathering space inclusive of all population groups in the City and the region. A place where residents can walk to the corner store, go to dinner, congregate, shop, and relax in one place without parking more than once. This Plan strives to improve the built environment of the Old Town while also enhancing the human environment, creating more opportunities for people to interact. The Plan is intended to attract customers, visitors, nearby residents, and travelers to destinations and to encourage people to stay longer by highlighting the aesthetics, views, scenes, and businesses of Old Town.



PHOTO SIMULATION. Streetscape design considered first the private and public realms and is a way to bring the two together for creating an inviting atmosphere for working, living, visiting, and shopping as well as helping to define the multimodal roadway for people of all ages and abilities. Pedestrians and cyclists have space to

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interact with the environment. Landscaping, hardscapes, and street furniture provide a pleasant atmosphere.

Central to the Plan is the creation of a “Pedestrian Priority Zone” around the intersection of Bradford and Santa Fe Avenue and a pedestrian network with primary paths along streets and secondary paths along alleys, behind buildings. The short blocks that are the original layout of the Old Town are very walkable and are preserved in the Plan.



WALKABLE DESTINATIONS. By designating a “Pedestrian Priority Zone” in the Plan, circulation, landscape, safety improvements, as well as land use development decisions, will work better together. Destinations within and surrounding the Old Town will feel closer together and easily reached on foot, especially for children and the elderly.

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Preserving the current diverse mix of land use types is important as well as supporting small businesses, public buildings, and housing. Although the area has developed with auto-related and industrial uses over the years, the goal is to phase out those types of uses to enhance more intense commercial development that contributes to a more pedestrian friendly environment.

LOOK AND FEEL

The vision is to create a unique and distinguishable look and feel to Old Town Placentia that is unique and slightly different from other old towns in Southern California by retaining the small scale buildings and shopfronts with smaller parcels, walkable blocks, and building frontages that are complementary to the public, pedestrian-scale spaces. The Old Town currently has a “village” character and the goal is to preserve and enhance that image.

Specifically, the objective is to preserve the commercial block style of building characteristic of Old Town Placentia. Buildings are built side by side and are placed at, or close to the public right of way line, with very little interruptions for driveways and parking. Uses typically include shops, restaurants, and other attractions on the ground floor and offices and residential uses on the top floors. Building façades are well articulated and focus on openings, proportions, entries, and embellishments that add to the attractiveness of the area. Infill development is encouraged to help create the compact walkable environment important to a vital town center.

The Plan defines a look and feel for Bradford Avenue that is complementary but distinguishable from Santa Fe Avenue through streetscape and landscape features. Current building heights at two stories could be preserved around the Pedestrians Priority Area with increased allowances to three and four stories on the outer edges, but still retaining the commercial block style of buildings.



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BEFORE. Intersection at Santa Fe Avenue and Main Street looking east showing wide lanes with street parking, narrow sidewalks, a lack of landscaping, and no designated areas for cyclists.

AFTER. The Plan calls for a “complete streets” approach which makes public spaces important for all users. The Plan calls for narrowing roadways for vehicles and widen sidewalks to create an attractive and inviting pedestrian realm.

A SUSTAINABLE FUTURE

The Plan also includes provisions for sustainable development by creating a multimodal transportation hub around the Metrolink Station. The objective is to increase active transportation modes such as walking and bicycling alone or in combination with transit. A multimodal future is built into the plan by placing less emphasis on development around the automobile and creating a multimodal place providing more options for pedestrians and cyclists.

Cities across the nation are implementing policies to enhance public health, public safety, and the environment. This includes the creation of a sustainable environment that encourages community activity and addresses climate change as well. There are multiple strategies that can be used to improve air quality and conserve water. The Green Cities program plays a role in reducing waste by having recycling, reuse and composting programs. Its mission is to accelerate the adoption of innovative policies and practices that further sustainability at the local, State, and national levels and can be used as a model to guide the policies of this Plan. Green Cities support multimodal transportation that improves ease of access to a variety of transportation methods such as walking, cycling, and public transit. Green infrastructure includes innovative water drainage systems, water retention, and sustainable landscaping to ensure water quality and conservation.

PLAN GOALS AND POLICIES

The goals and objectives of the Plan are to support an enhanced the environment which will encourage more capital improvements and serve as a catalyst for stimulating private investment.

1. Goal: Make Old Town Placentia a Unique Destination

Policies:

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- A. Celebrate the City's heritage
- B. Make connections to the past
- C. Create a walkable village atmosphere
- D. Build public gathering spots
- E. Enhance the Old Town's identity overall
- F. Highlight historic features
- G. Revitalize historic resources
- H. Improve public and private spaces
- I. Create a diversified commercial base
- J. Create a shopping and dining destination

2. Goal: Make Old Town Placentia a Pedestrian, People Priority Zone

Policies:

- A. Encourage compact and integrated development
- B. Encourage outdoor dining
- C. Provide for small passive recreation spots
- D. Support inclusivity across all walks of life
- E. Enhance community facilities and services
- F. Build lively streetscapes
- G. Create a small-scale village environment
- H. Get people to stay longer
- I. Create a safe, inviting, pleasant, attractive and appealing sense of place
- J. Tuck away parking where feasible
- K. Avoid conflicts with motor vehicles
- L. Provide opportunities to create intimate walkways
- M. Make the Old town walk-friendly for all ages and abilities
- N. Limit vehicular trips

3. Goal: Promote Multi Modal and Sustainable Transportation

Policies:

- A. Integrate Old Town with surrounding neighborhoods and commercial districts
- B. Encourage use and increased ridership of the Metrolink System
- C. Move away from the auto-orientation in the layout and land uses

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D. Provide hubs for various transportation modes

4. Goal: Support Businesses and Provide for Development Opportunities

Policies:

- A. Enhance safety and security
- B. Develop incentive programs
- C. Support community events that bring different communities together
- D. Provide business start-up opportunities
- E. Promote the Old Town through media and outreach
- F. Provide for infill development, reuse, and preservation
- G. Develop a promotional and branding campaign to encourage visits to Old Town Placentia

3. Mobility

The development of the Old Town Placentia around the railway network was a key factor to the local citrus industry in the early 1900s. As a result, today the area is not well integrated into the arterial street network of the City and the north Orange County region. The area is somewhat by-passed and tucked away from view. Contributing to the Old Town's isolated feel is the blocking of local streets with dead-ends and limited access to major thoroughfares. The train tracks are an additional feature that contributes to the area's inaccessibility.

With the coming of the Placentia Metrolink Station and the completion of grade separations, the area's local and regional connectivity will improve substantially, particularly for travelers using all types of transportation modes. The Metrolink Station provides the opportunity to reimagine getting to and from and around the Old Town. Recent circulation improvements include the pedestrian bridge over the BNSF tracks and the underpass at Melrose Avenue.

Safety will be a driving factor in improving connectivity and will play a part in redesigning streets, sidewalks, and common public spaces, particularly where modes come together in a shared environment. The goal of this Plan is to make streets within and surrounding the Plan Area more accommodating and inviting for all users with varying abilities, a concept known as "complete streets."

LOCAL AND REGIONAL CONNECTIVITY

It is important that the Old Town be better connected to the larger north Orange County economic, commerce, and population centers including major destinations like California State University Fullerton to the west, Placentia City Hall to the east, the TOD Packing House District to the south, and schools and neighborhoods adjacent to and surrounding the Old Town.

COMPLETE STREETS

Complete streets means that streets are designed for all users of the public roadways including bicyclists, transit users, pedestrians, vehicles and for all ages and abilities. To achieve this balance, a redesign of current roadway configurations may be necessary since much of the urban landscape between the 1920s and 1990s was centered around the automobile for movement and parking.

Streets have traditionally been designed to move automobile traffic as fast and efficiently as possible with pedestrians given second priority. Increasing traffic congestion has made

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streets less welcoming to pedestrians and cyclists. Streets are important for the livability of a community, and more and more people want to be able to walk and ride bicycles in their neighborhoods. More people are focused on healthy living, with includes better food choices, increased physical activity, reduced stress and the opportunity to relax and enjoy nature. To improve a more active lifestyle, many communities are taking steps to modify street designs to optimize their usability by a variety of users.

With a complete streets model, pedestrians of all ages and abilities can be given safe spaces with continuous unobstructed sidewalks. Cyclists desire a safe, clearly designated and cohesive network of cycling lanes. Transit riders want convenient and reliable transportation that reduces traffic congestion. Children and adults want safe and inviting routes to schools and transit stations. Motorists need roadways that facilitate safe movement while still providing safe spaces for pedestrians and cyclists. Freight operators and service providers require safe accommodations for unloading and loading supplies via dedicated curb access. People doing business need adequate space to provide active and rich street environments.



BRADFORD/SANTA FE INTERSECTION. The Plan proposes a pedestrian priority intersection where Bradford Avenue and Santa Fe Avenue meet.



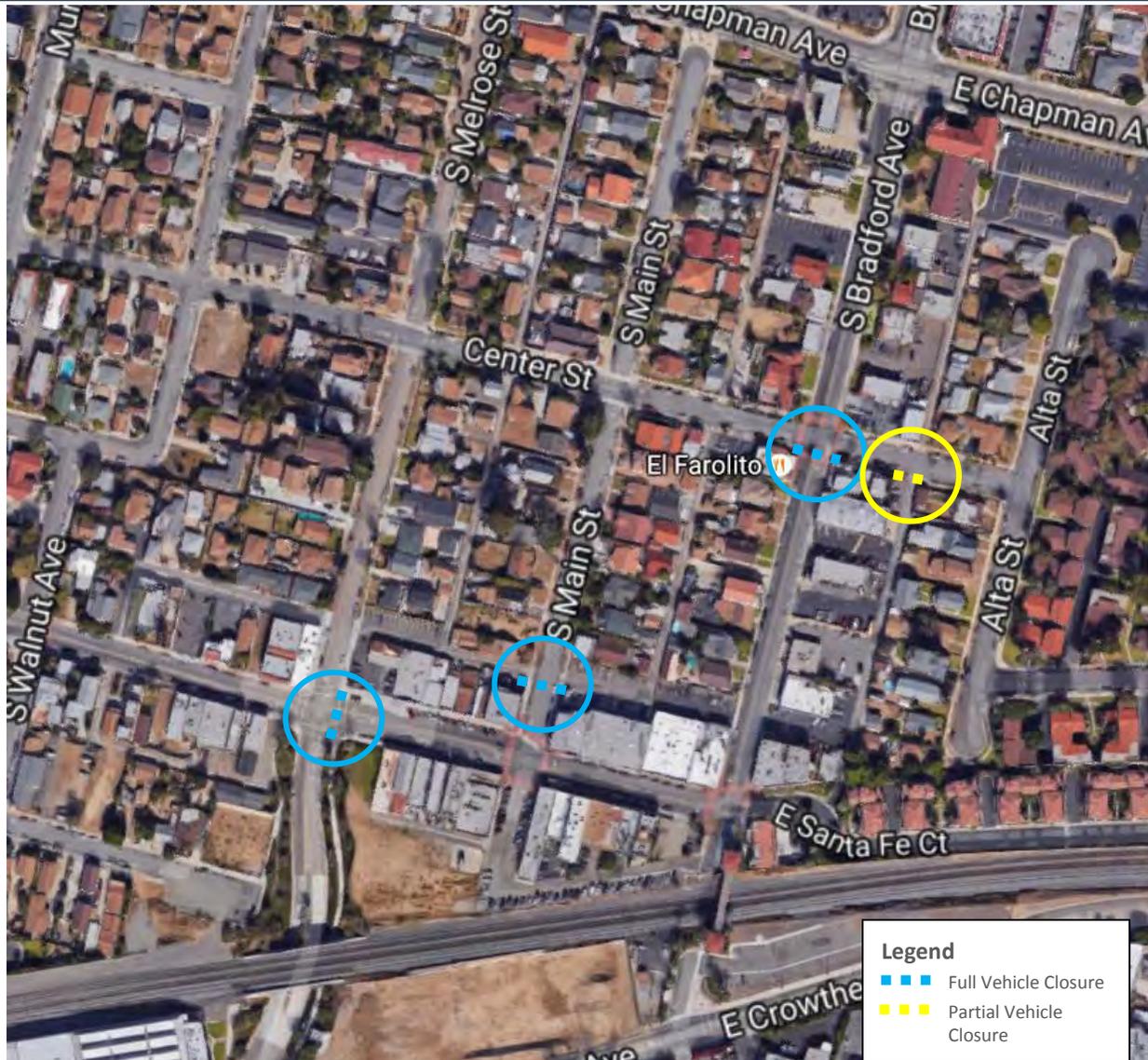
PEDESTRIAN OVERCROSSING BRIDGE. The existing pedestrian bridge over the BNSF railroad tracks provides a safe route between the two future Metrolink Platforms and will allow easy access to the Old Town from areas to the south.

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FLEX STREETS

This Plan considers that at various times of the year, certain streets could be easily closed for special events and festivals, day and night through provisions for “flex streets”. Flex streets provide communities a means to transform roadways into safe public areas for city supported functions. A flex street is a roadway that can be changed from vehicle-use to pedestrian use by strategic placement of removable bollards or other vehicular barriers. Vehicles can be prohibited from road use during pedestrian priority events to ensure a safe walking area. The Plan proposes that Bradford and Santa Fe Avenues from Center Street to Main Street be designated as flex streets for programming activities and special events to be coordinated with City departments, including Police, Public Works, and Orange County Fire. Flex streets in the Old Town would be sectioned off by removable bollards at Center Street and Main Street. Other flex streets can be created with removable bollards throughout the Old Town, providing options for temporary street closures for community events.

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SANTA FE AVENUE & BRADFORD AVENUE (from Center Street to Main Street). This street segment (s) can be easily converted into a “Flex Street” using physical temporary barriers such as rails and bollards that close the street in part or in full to vehicle traffic. This frees up public roadway space for community activities such as fairs and cultural festivals.

TRAFFIC SAFETY

This Plan considers that traffic safety and the reduction of collisions is a top priority. Although the Old Town does not suffer from many traffic collisions, the introduction of more pedestrians, bicyclists, and vehicles could increase the need for safety awareness and improvements to reduce any conflicts between the different types of users.

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CIRCULATION

This Plan recognizes that enhanced vehicle circulation will be important to moving through the Old Town, particularly in cases where drop off and pick up of passengers and goods occur. The proposed circulation for the plan area includes creating a one way circulation pattern starting at Center Street on Bradford Avenue, heading south to Santa Fe Avenue. This circulation plan

includes bike lanes and parallel parking where right of way space allows.

There are, like any active area, points where modes intersect and conflicts arise where traffic and pedestrians meet. Grade separated crossing with the Metrolink pedestrian bridge and the Melrose Street underpass have been completed reducing conflicts with the BNSF railroad. Throughout the Old Town, intersections and crosswalks are to be enhanced giving pedestrians more visibility and priority.

TRANSIT SERVICE

Important to creating a multimodal hub, is introducing various type of transit options for the Plan Area to include rail (Metrolink), bus (OCTA), and transit circulator (OCTA) routes, stations, and stops. Through enhancing the pedestrian realm within the Old Town, walking routes to transit stations and stops will be enhanced, creating an environment that promotes use of transit. The City is also reviewing the feasibility of a trolley service, which would stop near the Old Town area.



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BRADFORD/SANTA FE INTERSECTION. The Plan proposes a pedestrian priority intersection where Bradford Avenue and Santa Fe Avenue meet.



PEDESTRIAN OVERCROSSING BRIDGE. The existing pedestrian bridge over the BNSF railroad tracks provides a safe route between the two future Metrolink Platforms

BICYCLE NETWORK

Bicycle facilities increase mobility options for residents and guests. Bike lanes are important to mobility in the Old Town and are proposed for both Bradford and Santa Fe Avenues. To ensure that bike travelers will have places to keep their bicycles, short term and long term bicycle parking spaces should be considered with each new development and with the development of the streetscape plan. Short term bicycle parking spaces are bicycle racks while long term bicycle parking spaces are provided via paid bicycle lockers. Long term storage is particularly important for users of the Metrolink Station. Furthermore, a bicycle share program could be implemented for those without bicycles. This would enable bicycle commuters to connect with bicycle routes to Fullerton on the northwest and Anaheim in the south and allowing bicycle commuters to connect to residences on the eastern areas of the City. Where feasible, bike lanes will be installed in the Old Town district. Where the roadway cannot accommodate Class IV lanes, Class II or III can be installed. Bike lanes can be either one way or two, depending on the right of way width available.

PEDESTRIANS NETWORK

An overall goal of the Plan is to improve the walking environment for all visitors to Old Town. The Old Town Placentia Plan includes improvements to walkways for all users. Sidewalks along Bradford Avenue and Santa Fe Avenue can serve as the primary pedestrian network. These sidewalks widths are proposed to be widened from their current 5-feet up to 16-feet, with an average of 10-feet, depending on location and feasibility. Wider sidewalks will be achieved by

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narrowing or redesigning roadways to provide more public space for pedestrian travel, landscaping, outdoor cafes, and streetscape amenities. This increases the priority for the pedestrian and can create a more active streetscape.

The goal is to create more pedestrian routes that will help to bring destinations with the Old Town closer by allowing shortcuts and easier access between spaces. A secondary pedestrian network can be provided by activating existing alleys behind buildings. Alleys can be lined with green infrastructure to create more intimate and personal city spaces. Store owners can be encouraged to create secondary frontages along alleys and to maintain the rear entrances to their businesses. Alleys can be beautified in a way that they will also be utilized as off-street spaces for festivals and cultural activities as well. Naming of alleys as “courts”, “lanes”, etc. will help to create a sense of place with the Old Town.



PRIMARY. Example of a small commercial block style buildings fronting a main street serves as the primary pedestrian travel route in the area.



SECONDARY. In addition to providing parking and loading behind the buildings, the above example provides a secondary pedestrian and dining opportunities.

STREET CROSS SECTIONS AND CIRCULATION PLAN

The following pages contain illustrations show the vision for Bradford and Santa Fe Streets.

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Bradford - Southbound



Santa Fe - Westbound

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RECOMMENDATIONS

CIRCULATION, SAFETY AND FLEXIBLE STREETS

1. Promote the tenets of Complete Streets model and follow the City's Complete Streets Ordinance, once adopted.
2. Plan for "Flex Streets" that allow for community events and activities but also allow for temporary closures and redirection of traffic.
3. Widen sidewalks and reduce roadway widths where feasible to slow vehicle traffic and increase activity in the pedestrian realm.
4. Reduce speed limits to 15 miles per hour in the most highly concentrated pedestrian areas, based on the results of revised speed surveys.
5. Add signage at potential conflict points.
6. Open up closed streets and alleys to bicycles and pedestrians (keep existing closed to vehicles).
7. Open up more access routes into and out of the Old Town by opening through traffic in both directions along Melrose Street.
8. Designate a passenger drop off and pick up zone for the Metrolink Station.
9. Create a pedestrian priority intersection where appropriate, which will include where Santa Fe Avenue and Bradford Avenue meet.
10. Install enhanced crosswalk designs for increased visibility of pedestrians at all intersections in the Old Town.
11. Add signage and visual reminders for vehicles in and around the Old Town.
12. Designate emergency access routes reviewed and approved by Public Safety.
13. Conduct periodic reviews of the circulation system.

TRANSIT

14. Include enhanced wayfinding for the Placentia Metrolink Station and parking structure.
15. Design and install enhanced bus shelters on Chapman Avenue.
16. Determine the feasibility of a local circulating bus or trolley that services the Old Town, the Metrolink Station, the TOD Packing House District and surrounding areas.
17. Consider a circulator stop near the proposed Metrolink parking structure.

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18. Improve the vehicular circulation pattern within Old Town for better traffic flow and pedestrian safety.

BICYCLE FACILITIES

19. Install bike lanes along both Bradford and Santa Fe Avenues.
20. Install short term and long term bicycle parking spaces at central and convenient locations, as development and streetscape improvements occur.
21. Investigate the feasibility of a bicycle share program for those without bicycles, particularly near the Metrolink Station.

PEDESTRIAN

22. Where feasible, widen sidewalks to give priority to the pedestrian and to allow space for more active streetscape activities such as walking, outdoor dining, seating, and other amenities that improve the environment for the pedestrian.
23. Activate the alleys, and consider the alleys as a possible secondary pedestrian network and encourage secondary business entries there.
24. Consider “greening” the alleys.
25. Minimize driveways and curb cuts fronting main thoroughfares.
26. Encourage the maintenance of alleys by the business or property owners.
27. Consider alleys as off-street spaces for festivals and cultural activities.
28. Consider a program of community naming of the alleys.

4. Infrastructure

To support any new infill development in the Old Town, an assessment of the capacity of the various infrastructure systems will need to be conducted. Because the Plan can allow for greater density through infill and increased height limits, the existing systems will likely have to be upgraded.

WATER

This Plan considers that increased development may require upgrades to the current water lines and facilities. The Plan Area is served by water distribution supply lines ranging in size from 8-inches to 12-inches. Water supply assessment provided by Golden State Water Company.

WASTEWATER

This Plan considers that increased development may require upgrades to the current wastewater and facilities. The Plan Area is served by the City's sanitary sewer system. This system contains sewage collection piping ranging in size from 8-inches to 15-inches.

DRAINAGE AND STORMWATER MANAGEMENT

The Plan Area is served by a storm drainage system consisting primarily of pipes with diameters ranging from 8-inches to 60-inches, which ultimately drain to Carbon Creek. These creeks have been channelized in their downstream sections and are also used in conjunction with the storm drainage system for flood protection. Drainage in the Plan Area collects at Bradford and Santa Fe Avenues, and Melrose Street.

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PAVEMENT. Permeable pavement treatments help to retain runoff and improve water quality.



BIOSWALES. Swales help to ensure proper storm water drainage and water retention.

UTILITIES

The Plan area includes policies creating attractive public realm environment in all corners. To the extent feasible, overhead utility lines should be undergrounded or otherwise upgraded.

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RECOMMENDATIONS

WATER

1. Conduct a water supply and facilities assessment to determine the capacity of the network.
2. Center water facilities around the intersection of Bradford Avenue and Santa Fe Avenue, near the new Metrolink Station.

WASTEWATER

3. Conduct a wastewater facilities assessment to determine the capacity of the network.
4. Center wastewater facilities around the intersection of Bradford Avenue and Santa Fe Avenue, near the new Metrolink Station.

DRAINAGE AND STORMWATER

5. Assess the capacity of the current drainage system to accommodate future development
6. Encourage stormwater capture and filtration through bio swales or other innovative and best management practices for private development and City projects.

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5. Streetscapes

Streetscapes are the common connectors between plazas, spaces, parking, and buildings. Streetscapes in the Old Town are proposed to provide for a clean, uniform, unified, uncluttered, and attractive shared environment, with a color scheme, style, and design tied to the City's heritage. Landscaping, wayfinding, amenities, street furniture, and related elements are to be integrated with the public realm guidelines of the TOD Packing House District to the south.



PEDESTRIAN REALM

The key component of the Old Town Placentia streetscape plan is the emphasis on a linear and park-like, pedestrian realm branching out from the new Metrolink Station area. The pedestrian realm design has been coordinated with the proposed Old Town circulation plan and land use plan as contained here.

An enhanced pedestrian experience will occur through streetscape improvements that unify corridors with consistent tree palettes and planting. Pedestrian amenities such as benches, trash cans, bike racks, and pedestrian lighting will be added. Pedestrian amenities should be organized into clusters. Utility boxes that inhabit the roadway space should be wrapped with artistic materials that are evocative of Old Town Placentia. Shade structures can also be used to improve the pedestrian experience.

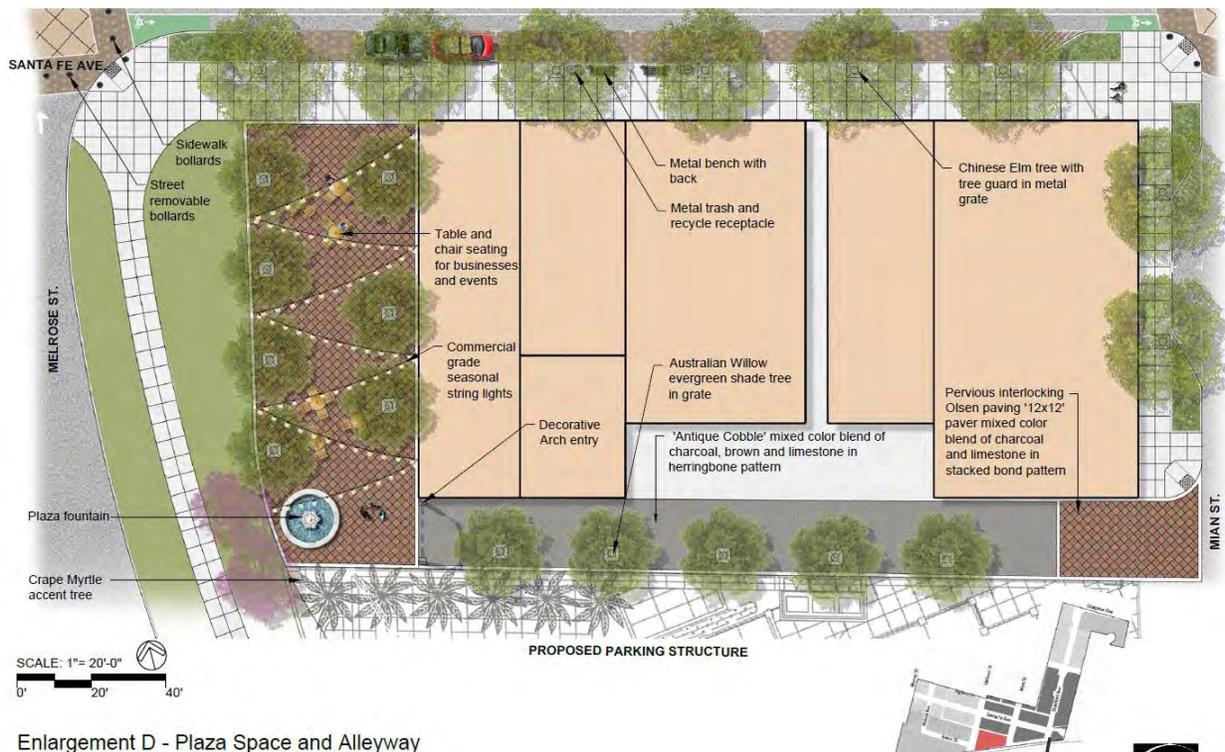


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OPEN SPACE AND PUBLIC PLAZAS

Open space is integral to the success of the Plan in creating a destination and active town center. It is an important part of a commitment to provide places that promote the physical and emotional well-being of residents. Locating a space for a central public plaza is recommended. This public plaza could include a stage area, public amenities, landscaping and seating. The plaza could play an important role in community activities and festivals through the year and would be the central location for information and signage and could provide a focus to civic life in Placentia, provide shade, establish a focal point for the Metrolink Station, and attract visitors. Secondary to a public plaza could be a series of small open spaces to contain amenities, public art, and signage combined to produce an attractive and well maintained areas of interest, continuity in the outdoor environment.

The Plaza complements commercial activity by allowing shoppers and patrons a place for respite amidst their activities. Instead of leaving the Old Town, pedestrians can retreat for a break. Public amenities contribute to an atmosphere and character, and help paint an area as a destination with a consistent planting palette and lighting scheme.



Enlargement D - Plaza Space and Alleyway

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AMENITIES AND STREET FURNITURE

Amenities in the public realm provide for shared features that make the area attractive as well as functional. Street furniture such as benches, decorative traffic signs, and trash receptacles coordinated in style and color enhances the feel of the Old Town. Options include a mixture of seating types, bike racks, etc. Furniture should sit in clusters and be organized in a way that does not feel cluttered and that provides pedestrians respite from street and shopping activity. Resting or seating areas should be organized around trees, planters, and green spots for access to shade and a more peaceful environment. Amenities can include fountains and water features.



LANDSCAPE

North Orange County's landscape is largely urbanized and populated with an eclectic palette of species. In the untouched, rural areas, the landscape is rustic, reflecting several large tree species such as eucalyptus, oaks, citrus and palms. The landscape plan includes street trees along Bradford and Santa Fe Avenues complemented by smaller planter boxes and ornamental plants.

Landscapes shall preserve and promote the aesthetic character and value of Placentia's core by defining, unifying and enhancing the public realm while providing the passive solar functions of cooling in summer while allowing filtered sunlight and warmth in winter.

Landscapes shall mitigate environmental degradation resulting from stormwater runoff. The landscape shall consist of elements consistent with the character, climate and soils of Placentia. As practical, plant materials will be indigenous to Placentia, or similar in character and habitat to indigenous materials and shall be of low water usage. Storm Water Best Management Practices will be established to improve water quality.

Streetscapes as a major component of thoroughfares shall help define the street space as a safe environment for automobiles, cyclists and pedestrians while adding beauty and shade to the street. Indigenous trees such as, Oaks and Sycamores, shall be used to provide a sustainable habitat and reinforce the existing natural aesthetics of the open space framework. These trees are tolerant of stress created during periods of drought, and are receptive to eco-friendly integrated pest management. The open space system shall be complex and usable for a variety of active and passive purposes to serve a wide cross section of ages and abilities.

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SIGNAGE

Signage is a key navigation element of the Old Town Placentia Streetscape as it becomes more pedestrian oriented. Because signage is necessary for wayfinding, adds to the ambiance of an area and promotes city branding; a system of guidelines will be implemented to create a feeling of consistency in Old Town Placentia. Signage should indicate traffic direction, restrooms, parking lots, commercial areas, public transportation stops, and where to find city information. Signage should also be clear and easy to read. Signage should not cover the façades, doors, or windows of buildings and should be in proportion to the primary building façade.



GATEWAY SIGNAGE. Marking the entrances into the Old Town will help to create the sense of place.



WAYFINDING. Clear signage shows where specific areas can be found.

LIGHTING

An array of features should be lighted around the Old Town making the area attractive, inviting, and safe at night times. Lighting is a fundamental amenity for streets, access-ways, parking lots, plazas, and other public spaces and helps to establish a more human environment for walking and driving. Well-designed lighting also fosters a safer and more viable night-life atmosphere and adds a more festive and relaxing feeling. It can create interest by illuminating special locations and architectural and landscape features, and can help convey an overall feeling of activity during the evening.

Lighting within the Plan Area would be pedestrian oriented and contribute to the overall experience of the environment. Commercial and mixed-use areas should exemplify the vitality of an active night-life while preserving the quality of the night sky. Street lighting should be pedestrian in scale.

The illumination of signs, fountains, pedestrian crossings, and special features is encouraged, and the color and quality of the light should support the architectural character and textures of the building. Lighting fixtures to be spaced close together with lower light levels than further apart with intense and varied light levels. Light fixtures should not cause glares into the upper stories of buildings.



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Architectural lighting should be encouraged and be consistent with the design vocabulary of the streetscape. Retail environments require special lighting to attract customers and promote the sale of merchandise. Interior lights should be directed towards the interior of the store, and varied light expression is encouraged. Retailers should make special effort to create interesting and dynamic storefronts while abiding by the concerns of light-wash and over-lighting. Parking structure lighting should be limited to highlighting special features such as green or living walls, stair and elevator core elements, lobbies, and signage. Pedestrian and vehicular entrances must be controlled to reduce glare and/or light wash. Residential and balcony lighting should be directed toward building walls. Light fixtures should be hidden or integrated into the architectural character of building. In general, balcony lighting should provide architectural highlights with no illumination beyond the direct boundary of the building. Security lighting should be connected to a timer and/or motion detector.



PROMENADES. Open areas for pedestrians and outdoor business bring in more visitors.



CAFES. Widened streets with landscaping and outdoor cafes provide areas that people can meet up at for a more vibrant streetscape.

OUTDOOR KIOSKS

To supplement the small-scale character, vitality, and diversity in the commercial atmosphere, Old Town may contain areas that are suitable for outdoor kiosks, which could sell retail goods and provide connectivity for cell phones and other devices. Kiosks can often help provide start-up and pop-up business opportunities and add a level of interest to the streetscape.

HARDSCAPES AND PAVEMENT TREATMENTS

Included in the streetscape pallet are hardscapes and pavement treatments incorporated to break up spaces and particularly to define node, centers, segments, and districts within the Old Town. Hardscape materials such as wood, brick, masonry, stones, can be artfully combined to create these edges. Roadway pavement is encouraged to support the pedestrian friendly environment.

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PUBLIC ART

The Old Town is also intended to be a center of arts and culture in the City. To reinforce that element, the Plan includes provisions for placement of public art. A public art program would be coordinated by City departments and community organizations and could include a mural program.

RECOMMENDATIONS

1. Prepare a streetscape master plan based on the public realm document created for both Old Town and the TOD Packing House District.
2. Conduct a study to consider the feasibility of outdoor kiosks in the Plan area and create a Master Kiosk Plan before allowing these uses.
3. Conduct study to determine where public plazas would be feasible and develop a public plaza and open space plan.
4. Develop a Master Wayfinding Plan that would include both Old Town and the TOD Packing House District.

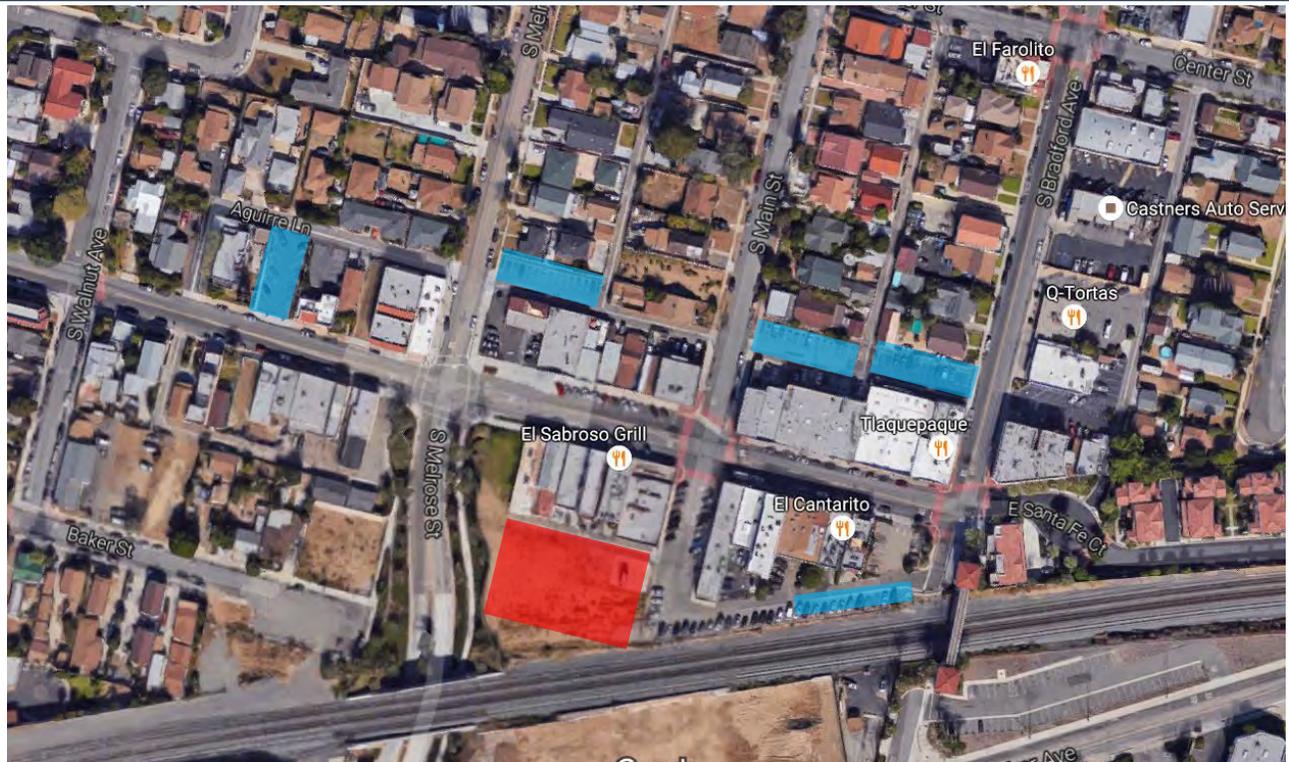
5. Parking

Parking is an essential part of the success of the revitalized Old Town, and has contributed to limited growth and investment opportunities. Less land devoted to the automobile will make land uses and destinations within the Plan Area seem closer together and more easily accessible on foot. Currently, the Plan Area has on-street parallel and angled parking, surface public parking lots, and on-site private parking in addition to driveways associated with parking.

Consistent with the objective of creating a pedestrian friendly walkable environment with a sense of place, the Plan encourages creative and reduced parking standards, similar to TOD style development. Because parcels are small, space should be restricted to buildings to help create that compact development with a large variety of uses. Some parking in the Old Town is intended to be shared public parking through on-street parking, City parking lots, and the Metrolink Parking structure to provide parking for both the Old Town businesses and Metrolink commuters.

The structure is planned to provide space for approximately 253 vehicles within a five-level structure. Also included would be designated space for electrical vehicle charging, bicycle racks, and long term bicycle parking (lockers).

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Public Parking Legend

-  Existing Surface Parking Lot
-  Future 253 Space Parking Structure

RECOMMENDATIONS

1. Consider amenities such as electrical vehicle charging, bike storage and parking for shared car programs within new parking structures and surface lots.
2. Conduct an annual parking management study (required by OCTA) to ensure that sufficient parking is provided for Metrolink riders.
3. Employ parking management strategies to ensure a cohesive and coordination approach to all parking within the Old Town, including shared parking, valet parking, paid, permitted, time-limited, and other innovative parking approaches.

6. Land Use

The vision for the land use of private and public land is to keep development compact by providing opportunities for infill that preserves the small-scale character of the area and places increasing height away from the historic core. A portion of the Plan area encompasses surface parking lots and driveways, and the objective would be to convert that space to development use for stores, restaurants, offices, and residential uses. The Plan also includes opportunities for rehabilitation, additions, and alterations to existing properties. The objective is to create a more compact commercial core district, keep parcel sizes small as they currently exist, consolidate, as necessary, in height districts 3 and 4 to create larger development sites.

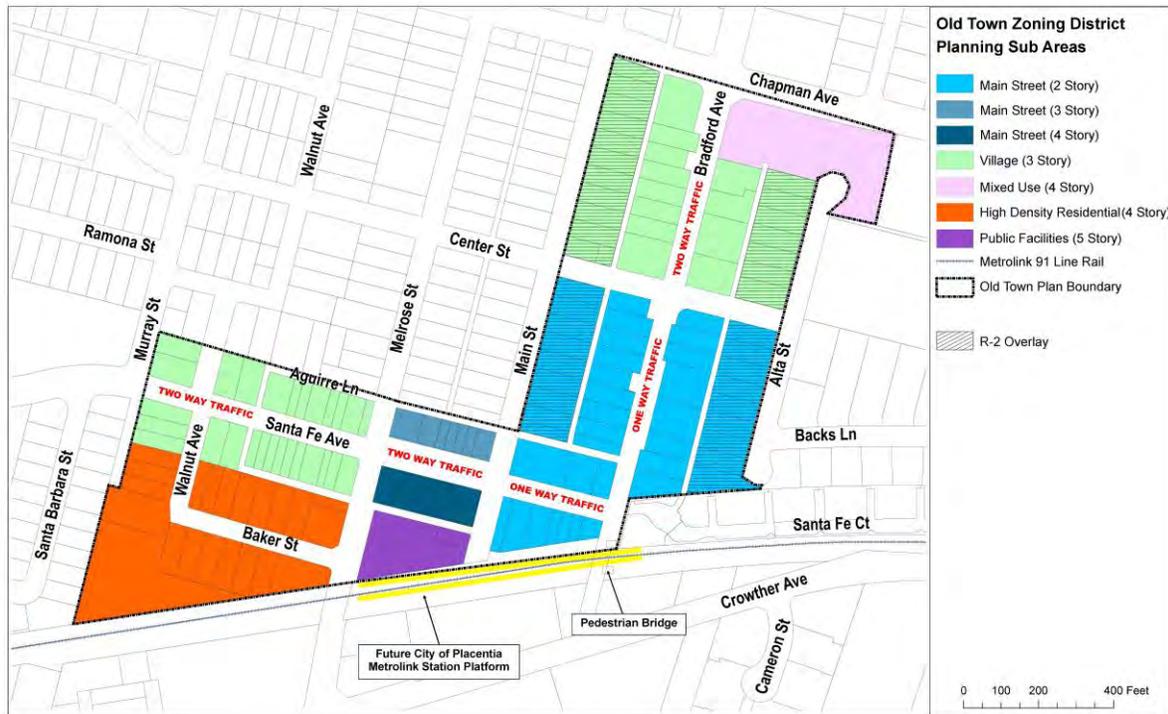
TYPES OF USES

The Plan's vision is to encourage a continuing and robust mix of uses oriented around this unique destination. The goal for commercial diversity includes a healthy mix of retail, restaurants, and office uses combined with outdoor gathering spaces such as public plazas. In addition, office and service uses would be secondary and permitted on the second floor of the Plan area buildings, providing an important element to create a more full service area. A list of permitted uses are contained in the Old Town Development Standards, approved as a Zone Change.

ZONING DISTRICTS

The Plan Area includes all of the current "SF-C" Santa Fe Commercial District zone (Chapter 23.37 of the Code) and some surrounding parcels in the "C-1", "C-2", and "R-2" zones. The Plan proposes the area to be rezoned as "Old Town" and further divides the Old Town into five main planning sub areas to guide new development, infill, preservation, and reintegration of land uses. These subareas were drawn around distinctive areas or locations in the Plan area, with distinctive standards.

OLD TOWN PLACENTIA REVITALIZATION PLAN



Main Street (Two, Three and Four Story Height Subareas). This subarea is found along both Bradford Avenue and Santa Fe Avenue from Center Street to Main Street and will be small in scale with frontages that blend with the streetscapes. This is also within the Pedestrian Priority Zone and the one-way travel segments. The height limit varies from two to four stories, with the higher four story height limit being permitted along Santa Fe Avenue. The focus is on historic rehabilitation, Main Street architecture and infill development to create a dense urban segment. Uses would be limited to commercial uses that occupy smaller spaces and attract a large number of visitors. The Development Standards for this subarea call out several parking reductions.

The Main Street (MS) subarea provides for the preservation and enhancement of the unique character of the historic Old Town commercial area with commercial block building forms and shopfront architecture. The north and south sides of Santa Fe Avenue from its intersection with Bradford Avenue to the first block west of Melrose Avenue is characterized by commercial block buildings at the back of the sidewalk. This pattern would be repeated along the Bradford Avenue segment. The historic character of existing buildings is to be preserved where sufficient architectural integrity remains, and new buildings are to be designed to be compatible with the historic character of the area in the commercial block style.

OLD TOWN PLACENTIA REVITALIZATION PLAN

Allowable land uses emphasize ground floor retail, restaurant, and related uses, with opportunities for offices above. The restoration and renovation of existing façades is encouraged. Parking is on-street, in existing and proposed surface lots, and in park-once surface lots/garages. Buildings are to be built to the front property line along both Bradford Avenue and Santa Fe Avenue. A small rear, alley set back is required for vehicle loading, amenities, and for each business and building.

Village (3 Story Height Subarea). This subarea is found in two separate places. One at the north end at the southwest corner of Chapman Avenue and Bradford Avenue extending south along both sides of Bradford Avenue and the other at the west end along both sides of Santa Fe Avenue from Melrose Street on the east to Murray Street on the west. The intent is to create new and infill development opportunities while recognizing existing historically significant properties such as the old City Hall and the Library.

The Village (V) subarea provides for a new walkable retail and office center as an extension of the Main Street district. The Village subarea would provide for the same building form and setbacks as the Main Street subarea but would allow three stories. In this zone, the primary frontages along Bradford and Santa Fe Avenues would orient to the street with zero to five foot front setbacks and a zero to fifteen foot rear setback for secondary frontages along the alleys. Ground floor spaces would be occupied by retail and restaurants, with offices and residential above. Building types are intended primarily to be a cohesive design with well-defined frontages in the shopfront style similar to the Main Street subarea.

High Density Residential (Four Story Height Subarea). There is one subarea in the Old Town designated for high density residential development. This can be four stories in height and is located along Baker St and Walnut Avenue at the south west corner of the Plan area, just north of the railroad tracks.

Mixed Use (Four Story Height Subarea). This subarea is found at the southeastern corner of Bradford and Chapman. The Mixed Use subarea provides for a mix of concentrated urban housing types with some commercial uses, within convenient walking distance of the Main Street and Village subareas.

Public Facilities (Five Story Height Subarea). A public parking structure will be constructed in conjunction with the new Metrolink Station and it will serve both train riders and those visiting the Old Town.

R-2 Overlay. Along Alta and Main Streets, in the Main Street and Village subareas, from Chapman to Aguirre Lane extension, there is an R-2 overlay which allows existing residential uses to remain and limits the commercial uses fronting on these residential streets.

ARCHITECTURAL STYLE AND GUIDELINES

OLD TOWN PLACENTIA REVITALIZATION PLAN

The Plan prioritizes the “Commercial Block” style of architecture in its historic and contemporary forms. The style can be adapted for the two, three and larger four story envelopes. This style includes facades placed at or close to the right-of-way line, with the entrance at the sidewalk. Single or multiple storefronts and entryways should be recessed. This style is also a form of building designed for occupancy by retail, service, or office use on the ground floor and secondary commercial or residential use on the upper floors.

This style is characteristic of a building designed for occupancy by retail, restaurants and other commercial uses on the first floor with offices and services on the second floor. The upper floors can also be configured for residences. The main entrance to each ground floor commercial or storefront is directly from the street. Where an alley is present, loading may be accessed through the alley. Upper floor residential uses may contain flats or lofts.

Architectural Styles	Definition	Photo	Sub Areas
Main Street Commercial	Multi-story facades are typically divided into base, body and top with the ground floor taller than the shorter upper floor which is finished by a significant parapet. The ground floor has recessed entries and any expansive glass is interrupted by structural columns with transoms to allow light to penetrate deep into the interior. Upper floor windows are smaller with vertical windows directly relating to the ground floor openings.		Main Street (preserve and enhance existing buildings)
Mission Revival	Prominent features of the style include red clay tile roofs, use of balconies, smooth-stuccoed exterior walls usually painted white, arched openings, colorful tile work and elaborate landscaping. The buildings frequently have courtyards.		Village Mixed Use High Density
Spanish Colonial	Typical features include a low-pitched roof with little or no eave overhang, a red-tiled roof, perhaps a prominent rounded arch over a door, window or porch, a stucco wall surface, and usually an asymmetrical façade. The features elaborate molded ornament around doors and windows, polychrome tile at ingresses and wrought iron grilles and balconies.		High Density, Mixed Use and High Density Residential subareas

OLD TOWN PLACENTIA REVITALIZATION PLAN

The shopfronts are an important design feature of facades in this style which are placed at or close to the right-of-way line, with the entrance at sidewalk grade. This type is conventional for retail frontage and is commonly equipped with cantilevered shed roof(s) or awning(s). Recessed storefronts are also acceptable. A great variety of shopfront designs are possible, but the following is consistent with existing Main Street shopfront configurations and is encouraged throughout the Plan area:

- a. 12 feet to 16 feet tall, as measured from the adjacent sidewalk.
- b. The corresponding storefront(s) opening(s) along the primary frontage shall comprise 65% of the 1st floor wall area facing the street and not have opaque, stucco or reflective glazing.
- c. Storefronts may be recessed from the frontage line by up to 10 feet.
- d. "Bulkheads," which are a segment of wall which transitions between the window opening(s) and the adjacent grade, are also encouraged. The bulkhead shall be between 24 inches and 36 inches tall (aluminum storefront or spandrel panel may not substitute for a bulkhead).

RECOMMENDATIONS

1. Retain and preserve the historic architecture of the core of the Plan area, principally located in the Main Street subarea and along Bradford Avenue.
2. Maintain a cohesive architectural style throughout the Old Town area.
3. Ensure that shopfronts and their entries are of pedestrian scale, relate to the street and create inviting places for shoppers.
4. Encourage land uses that activate the street and provide a mix of daytime and night-time uses.

OLD TOWN PLACENTIA REVITALIZATION PLAN

5. Historic Resources

Based on a limited field review of historic properties in the Old Town, there appears to be enough historic fabric and integrity for the area to qualify as a historic resource under local and State law and the area would likely be eligible for the National Register of Historical Places as a historic district.

HISTORIC CONTEXT

Placentia was originally part of the Rancho San Juan Cajon De Santa Ana, known today as the area comprising the cities of Placentia, Anaheim, Fullerton, and La Habra. The City of Placentia emerged when approximately 4,000 acres were purchased by two separate parties, Daniel Kraemer and William and Sarah Jane McFadden, in the late 1860s. Like many early Southern California settlements, the area was attractive for both its climate and its soil—the town quickly became known for its citrus crops, though walnuts, grapes, and avocados were also grown. The town’s development was largely dependent on early, civic-oriented residents. In 1910, a rancher by the name of A.S. Bradford convinced the Santa Fe railroad to re-route its track through the area, making the rail distance to Los Angeles notably shorter. A train depot was constructed alongside multiple packing houses to accommodate the town’s growing citrus industry, and by 1926, the town had continued to develop and saw 500 of its citizens successfully vote to incorporate.



HISTORIC STREET SCENE. Old Town Placentia today is the site of the original Placentia Central Business District.



PLACENTIA DEPOT. Placentia was founded around the depot built in 1910 serving the citrus packing industry.

A.S. Bradford was also responsible for laying out the main streets of the town, and he had the foresight to structure the town’s center around the Santa Fe depot. Between 1910 and 1912, three blocks of the designated city center on Santa Fe Avenue were subdivided multiple times to properly accommodate commercial lots; these lots would eventually house banks, markets, hardware stores, and hotels. 1910 saw Samuel Kraemer Jr. construct the town’s first packing house, called the Placentia Mutual Orange

OLD TOWN PLACENTIA REVITALIZATION PLAN

Association. In 1917, the Union Oil Company took an interest in exploratory drilling and struck oil in 1919. As a result, Placentia experienced another period of huge development, as settlers seeking fortunes, rather than agricultural prospects, arrived to the town.

RESOURCE IDENTIFICATION

The City of Placentia has not conducted a historic resource survey to determine which properties have historic significance; however, an inventory of currently designed, listed, and surveyed historic properties located in the Plan Area is included in Table below. This list does not represent all historic properties in the Old Town but is a start to help focus any historic preservation efforts.

Identified Historic Resources in Old Town Placentia

Name	Year	Address	Designation	Prior Evaluation/Date
Placentia Water Tower		Chapman Ave at Main St	Local	
Old Placentia Library		143 S Bradford Ave	Local	
Old City Hall	1939	120 S Bradford Ave	Local	(1)
Cavalry Church	1928	102 S Bradford Ave		(1)/2002
The Kraemer Building	1914	238 & 240 S Bradford Ave	Local	(2)/2002
Telephone Exchange Building	1931	125 S Bradford Ave		(2)/2002
Marjie Building		149,155, and 161 W Santa Fe Ave	Local	
The Stradley Building No. 2		167 & 173 W Santa Fe Ave	Local	

Sources: City of Placentia; California Historic Property Data File (2012); California Register of Historical Resources

Notes:

(1) Appears Eligible for National Register & California Register based on Survey Work

(2) Contributor to a local district as recognized by local government

Properties identified as potential historic resources in the Westgate Plan include over 20 identified properties that have enough integrity to at least qualify as contributors to a historic district. The properties represent buildings that could undergo review against the Secretary of the Interior's Standards and Guidelines for the Rehabilitation of Historic Properties.

The old Placentia City Hall, Library, and Telephone Exchange buildings on their own and combined represent a cluster of Spanish Colonial Revival style architecture, and a site important to early Placentia. The commercial block style architecture on Santa Fe Avenue constitutes another historic cluster with strong associations to the early citrus industry and town formation. Some properties appear to be

OLD TOWN PLACENTIA REVITALIZATION PLAN

important due to early town site associations, but need to be further evaluated due to the amount of alterations inconsistent with key historic features and styles. This Plan recommends that a historic resource survey be conducted for the entire Plan Area to closely identify properties of significance. Infill development and alterations will need to be reviewed on a case by case basis.



SENIOR CENTER. Today the old library serves as a senior center with activities and programming for all ages.



HISTORIC INTERIOR VIEW. The first Placentia Library was designed in the Spanish Colonial Revival style and has retained a significant amount of historic integrity in feeling, design, materials, and associations.

ADAPTIVE REUSE, HISTORIC PROPERTY REHABILITATION, AND INFILL DEVELOPMENT

After identifying the historic resources in an area, there are treatment options consistent with economic development objectives that go beyond regular building maintenance and repair. Adaptive reuse is converting the use of a historic property for more contemporary use that may require certain modifications to the building form and features. Such modifications can be achieved without compromising the features identified as significant in earlier work. Historic rehabilitation may involve returning previously inconsistent alterations made to conform to the style and features of the historic architecture. Infill development in a historically sensitive area requires special attention to scale, massing, rhythm, and building placement.

Guidance for alterations to historic properties are best found in the Secretary of the Interior's Standards and Guidelines for Rehabilitation of Historic Properties (see <https://www.nps.gov/tps/standards.htm>).

REGULATORY FRAMEWORK

Certain rules and laws apply to the treatments of historic properties. Local ordinances such as that in the City of Placentia's Municipal Code prescribe how to treat listed historic properties in the City. At the State level, the California Environmental Quality Act requires review of discretionary public approvals for impacts to historic resources.

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HISTORIC PRESERVATION PROGRAMS

Certain programs and activities can help with supporting a historic preservation environment such as collecting oral histories, displaying information about important historical events in the community, and providing preservation incentive programs.



HISTORIC PHOTOS. If available historic photos provide good guidance on how to address inappropriate alterations over the years during a historic rehabilitation effort.



HISTORIC PHOTOS. If available historic photos provide good guidance on how to address inappropriate alterations over the years during a historic rehabilitation effort.

RECOMMENDATIONS

1. Conduct an historic survey of the Old Town area.
2. Require special attention for infill development in terms of scale, massing, rhythm, and building placement.
3. Determine which resources should be designated, and once designated, seek to preserve or restore this historic buildings pursuant to Secretary of Interior Standards.
4. Implement the façade improvement program to support preservation efforts.

6. Business Support

Small businesses are the engines helping to drive economic development. Nearly all the businesses located in Old Town Placentia are small businesses or “mom and pop” type of stores. Conversations with small business owners during the community workshops further highlighted the need for small business support, with an emphasis on minority and women-owned businesses, and assistance through the City of Placentia, State programs, business assistance centers, and the local chambers of commerce to develop policies, procedures and practices that encourage small business growth in Placentia and Old Town. This was further supported by input from the Placita Santa Fe Merchants Association and Chamber of Commerce. Areas of need prioritized by workshop participants included:

- Business Development
- Marketing
- Identity/Branding/Name
- Maintenance
- Safety & Security
- Funding
- Parking Management Program
- Partnerships

An area of business support that was frequently mentioned included support with funding and how to create funding streams to support improvements to the Old Town as included in the Plan. Recommendations for generating funding streams included:

- Adopt an Old Town Community Facilities District (CFD)
- Establish a Lighting District in this specific area
- Establish a Business Improvement District
- Establish a Special Assessment
- Establish a parking In-Lieu Fee Program
- Adopt Old Town impact fees

The City and the business community can encourage local businesses to take a more active role in the Old Town by adopting such measures as staying open for longer hours as feasible, illuminating storefronts, arranging attractive window displays, greening the rears of shops as secondary frontages.

OLD TOWN PLACENTIA REVITALIZATION PLAN

RECOMMENDATIONS

1. Conduct appropriate studies to determine the feasibility of different funding streams and adopt the appropriate funding mechanism.
2. Continue to participate in meetings with local businesses and owners to discuss concerns and areas of improvement.

3. Programming

Programming includes promoting and hosting community events including farmer's markets, craft fairs, outdoor performances, street festivals, community classes, parades, and special celebrations in the town square fashion, such as the popular Las Posada Tamale Festival.

The programming in a community and specifically in the town's center provide encouragement for citizens to gather for civic events, as well as private functions. These include non-built environmental components of a community that support the Plan's goals for an active Old Town and add interest to attract visitors, tenants, customers, etc. Community programming is intended to:

- Strengthen community image and sense of place
- Support economic development
- Strengthen safety and security
- Promote health and wellness
- Foster human development
- Increase cultural unity
- Protect environmental resources
- Facilitate community problem solving
- Provide recreational experiences

In the workshops held with the community, the following programming was recommended to support the growth of the Old Town:

- Festivals
- Public Art
- Special Events/Activities
- Cleaning and Maintenance
- Sponsorships and Partners
- Public safety, police patrol events
- Wayfinding, Signage
- Incentive programs for consumers
- Citywide advertising of Old Town

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TAMALE FESTIVAL. Placentia annual Tamale Festival is the type of events that can occur year around in the Old Town. Other events such as Rancher's Markets and Crafts Fairs bring people to public spaces for shopping and leisure and can introduce the area to new visitors.



OPEN STREETS. On certain days of the year, often on a Sunday, Placentia streets can be closed for cycling or other community activities. The "Flex Street" provisions in the Plan assists with these types of events.

RECOMMENDATIONS

- 1 Provide appropriate public amenities that will assist in successful programming efforts such as electrical outlets and hardware on light poles and removable bollards in the Old Town area which can easily be configured to support street festivals and fairs.
- 2 Encourage partnerships from within the Old Town and Citywide.
- 3 Conduct further survey and analysis to prioritize programming efforts.

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4. Maintenance

A clean and maintained Old Town will not only attract tenants and visitors but will make the area feel safer knowing that investment is occurring in an on-going manner. Maintenance includes upkeep of the public and private realms, including public improvements, trash pick-up, landscaping, graffiti abatement, addressing repair as need. Maintenance responsibilities are assigned to the City and area stakeholders and can be integrated with an ongoing code enforcement program.

Safety and security can be enhanced through improvements such as lighting included in this Plan, but also through crime prevention programs such as a neighborhood watch and other localized public safety programs. Increased security patrolling and police presence can assist with the feeling of safety and security. More multimodal travel will lead to more “eyes on the street” which will add to the feeling of safety.

RECOMMENDATIONS

1. Establish an Old Town CFD for all construction of public realm and streetscape improvements.
2. Establish an Old Town CFD for ongoing maintenance of the public realm.
3. Consider increased maintenance staff to address current and future needs of maintaining all public improvements, including the parking structure, train platform, and public realm.

5. Implementation

This Plan, once adopted, serves as an action plan to proceed with the implementation activities and funding strategies to bring about public and private improvements. Implementation will take place over time, as funding becomes available and as private development investment occurs. This Plan represents a collective agreement on the future direction of the City's Old Town with support by area stakeholders.

PHASING

This Plan assigns implementation phasing to best capture the investment and synergy of detailed streetscape and street plans.

IMMEDIATE (Years 1 and 2)

The "Immediate" phase focuses on:

1. Adoption of the General Plan Amendment, Zoning Code Amendment and Streetscape Master Plan to support the Old Town Revitalization Plan.
2. Securing funding sources for public improvements and develop a funding strategy.
3. Marketing and special events including branding and identity for the Old Town (wayfinding program).
4. Façade improvement program
5. Code enforcement and addressing unsafe conditions.
6. Conduct parking management studies.
7. Complete minor infrastructure improvements where feasible and plan for larger infrastructure implementation.
8. The completion of the Metrolink Station platforms and Metrolink service to Placentia.
9. The completion of the Metrolink Parking Structure.

The "Immediate" phase can also include marketing and public awareness campaigns about the history and plans for the Old Town including possible demonstration projects to show the public how a new street and pedestrian realm would work. Additional studies can be conducted to identify historic properties and significant features. Historic resource listing at the local, State, and National levels can also be achieved in this phase.

NEAR TERM (Years 3 to 5)

The "Near Term" phase focuses on:

1. Water and Sewer upgrades as necessary.
2. Redesign streets as called out in the Plan.

OLD TOWN PLACENTIA REVITALIZATION PLAN

3. Installing streetscape features and landscaping for Bradford and Santa Fe Avenues.
4. Install features and landscaping for Old Town

INTERGOVERNMENTAL AGENCY COORDINATION

Important to the implementation of this Plan is the support and coordination with other governmental agencies including the Orange County Transportation Authority (OCTA).

6. Funding

Funding for the implementation of this Plan will need to draw upon a mixture of public, quasi-public, private, and City resources under a funding strategy focusing on the phased implementation approach.

REGIONAL, STATE, AND FEDERAL GRANTS

Various federal and State funding programs are available through the year depending on priorities of larger scale resources. Opportunities are particularly appropriate for the sustainability aspects of this Plan including multimodal transportation improvements.

SPECIAL ASSESSMENTS

Special assessment districts, such as Community Facilities Districts (CFD), are available for public improvements and on-going maintenance of the public real estate such as business assessment districts, infrastructure districts, and lighting districts. Such districts require the support of the local property owners and/or businesses.

INFRASTRUCTURE FINANCE DISTRICTS

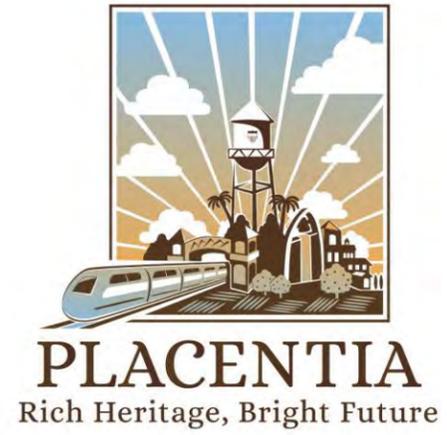
Although redevelopment funding is no longer available to California agencies, infrastructure finance districts are still available and operate similarly with “tax increment” financing available to install capital improvements.

FOUNDATION AND CORPORATE SUPPORT

Limited funding opportunities are available through large foundations, non-profits, and corporations that can fit into the overall funding strategy for the Old Town.

EXHIBIT D

**Old Town Streetscape Master Plan, with cross sections and photo simulations of
cross sections**



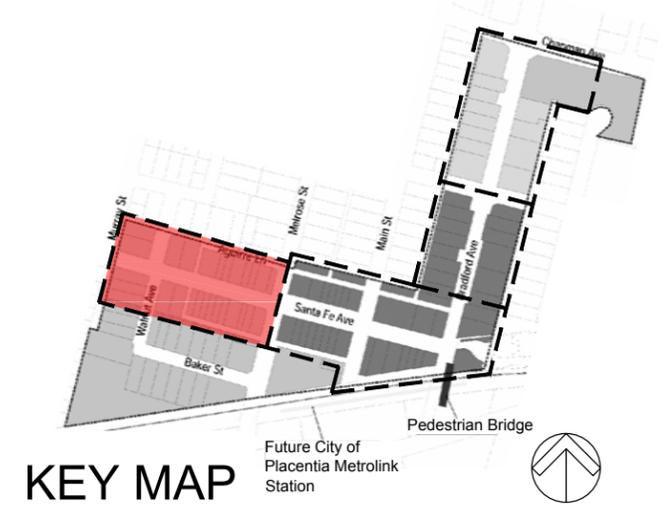
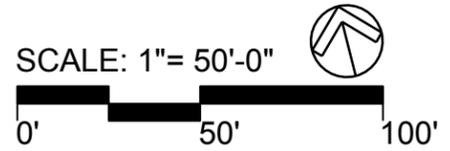
Old Town Placentia Revitalization Streetscape Master Plan

*Prepared by NUVIS Landscape Architecture
for the City of Placentia*

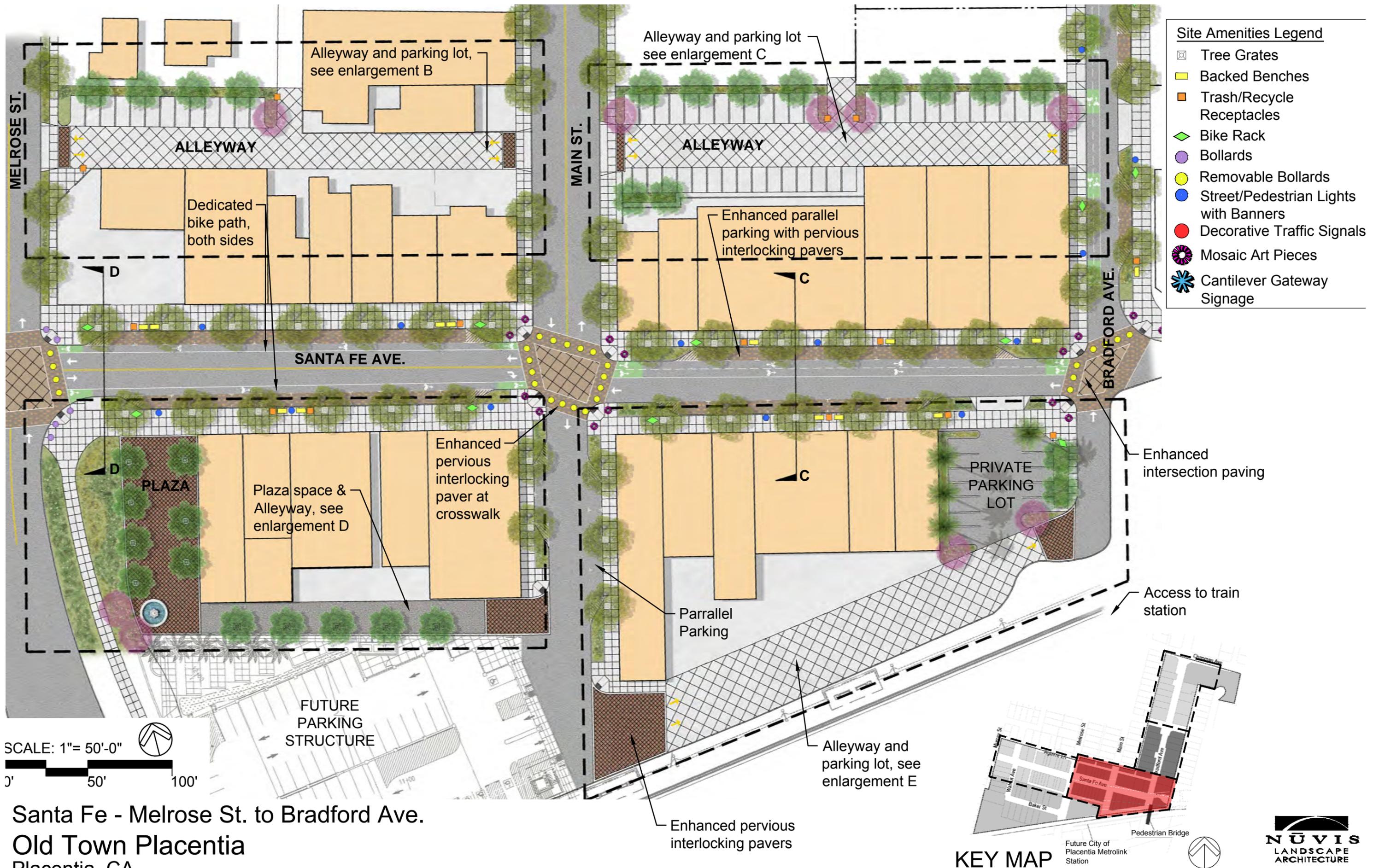




- Site Amenities Legend**
-  Tree Grates
 -  Backed Benches
 -  Trash/Recycle Receptacles
 -  Bike Rack
 -  Bollards
 -  Removable Bollards
 -  Street/Pedestrian Lights with Banners
 -  Decorative Traffic Signals
 -  Mosaic Art Pieces
 -  Cantilever Gateway Signage

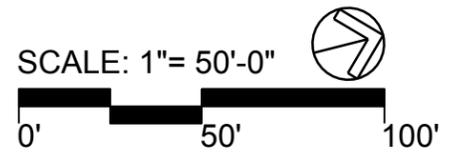
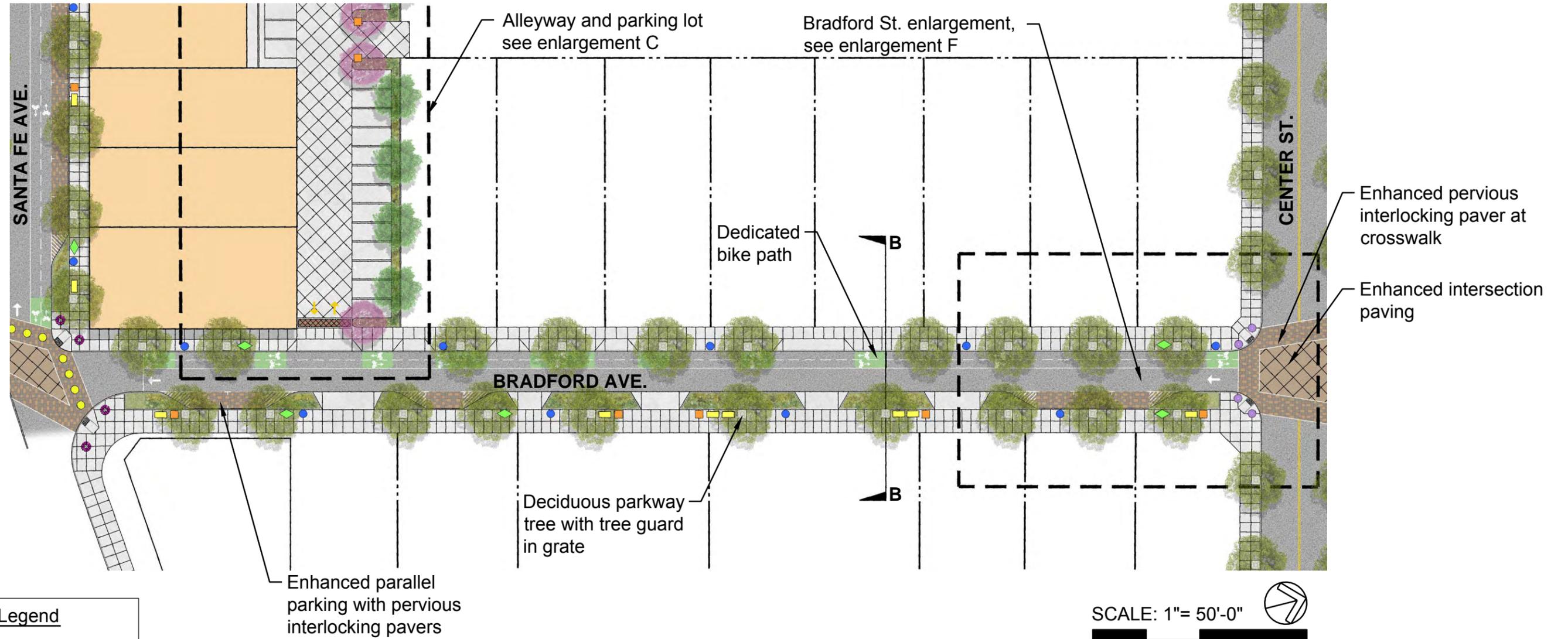


Santa Fe - Walnut Ave. to Melrose St.
Old Town Placentia
 Placentia, CA



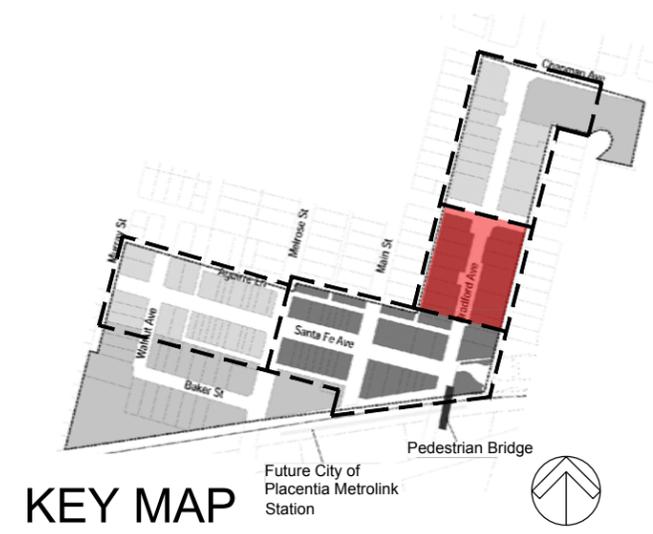
Santa Fe - Melrose St. to Bradford Ave.
 Old Town Placentia
 Placentia, CA

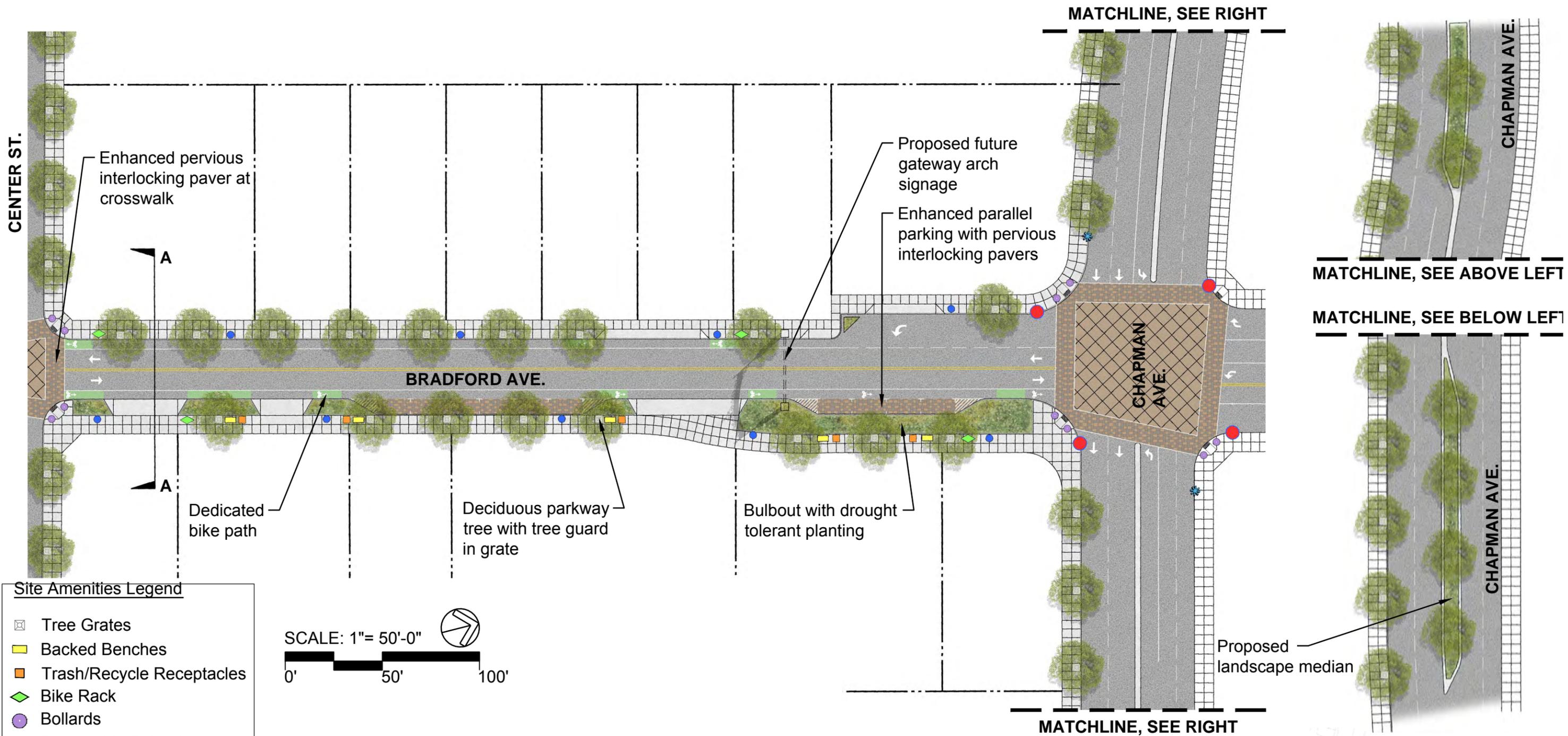
KEY MAP



- Site Amenities Legend**
-  Tree Grates
 -  Backed Benches
 -  Trash/Recycle Receptacles
 -  Bike Rack
 -  Bollards
 -  Removable Bollards
 -  Street/Pedestrian Lights with Banners
 -  Decorative Traffic Signals
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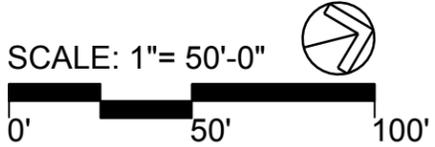
Bradford Avenue - Santa Fe to Center St.
Old Town Placentia
 Placentia, CA



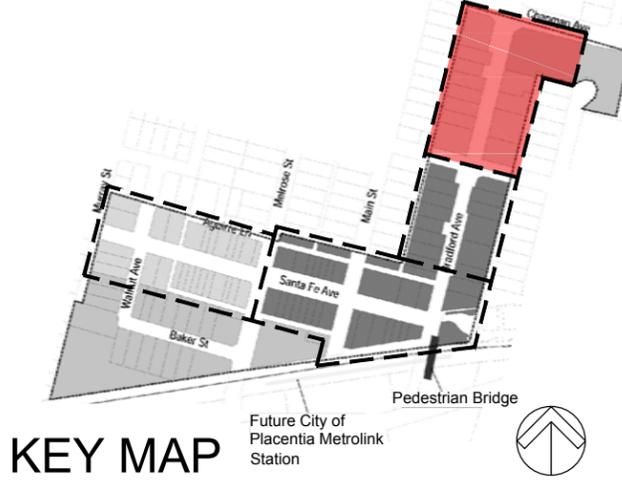


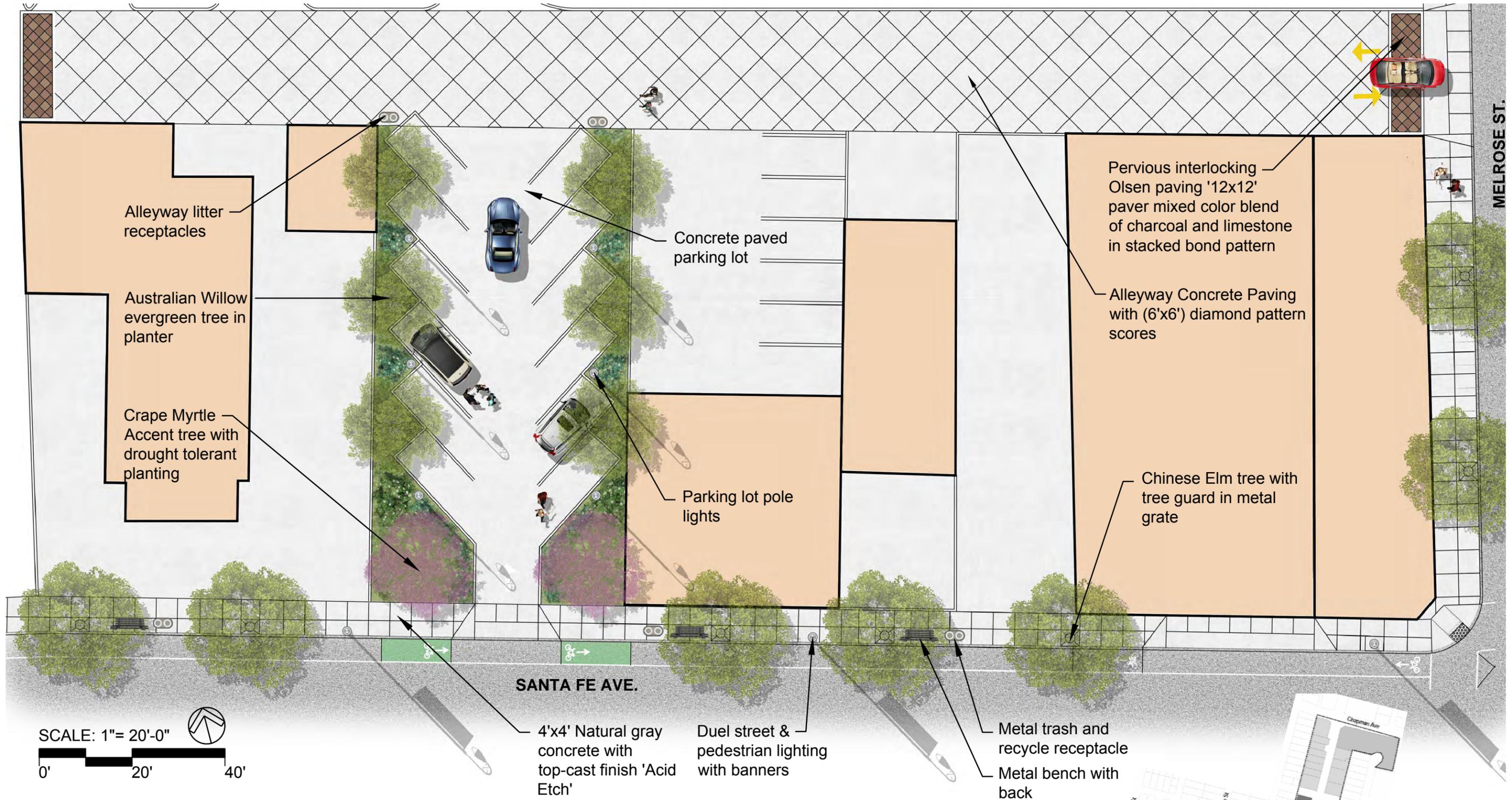
Site Amenities Legend

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-  Bollards
-  Removable Bollards
-  Street/Pedestrian Lights with Banners
-  Decorative Traffic Signals
-  Mosaic Art Pieces
-  Cantilever Gateway Signage



Bradford Ave. - Center St. to Chapman Avenue
Old Town Placentia
 Placentia, CA



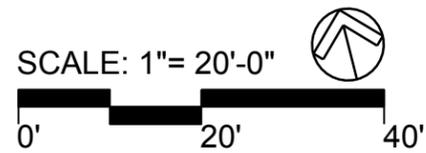
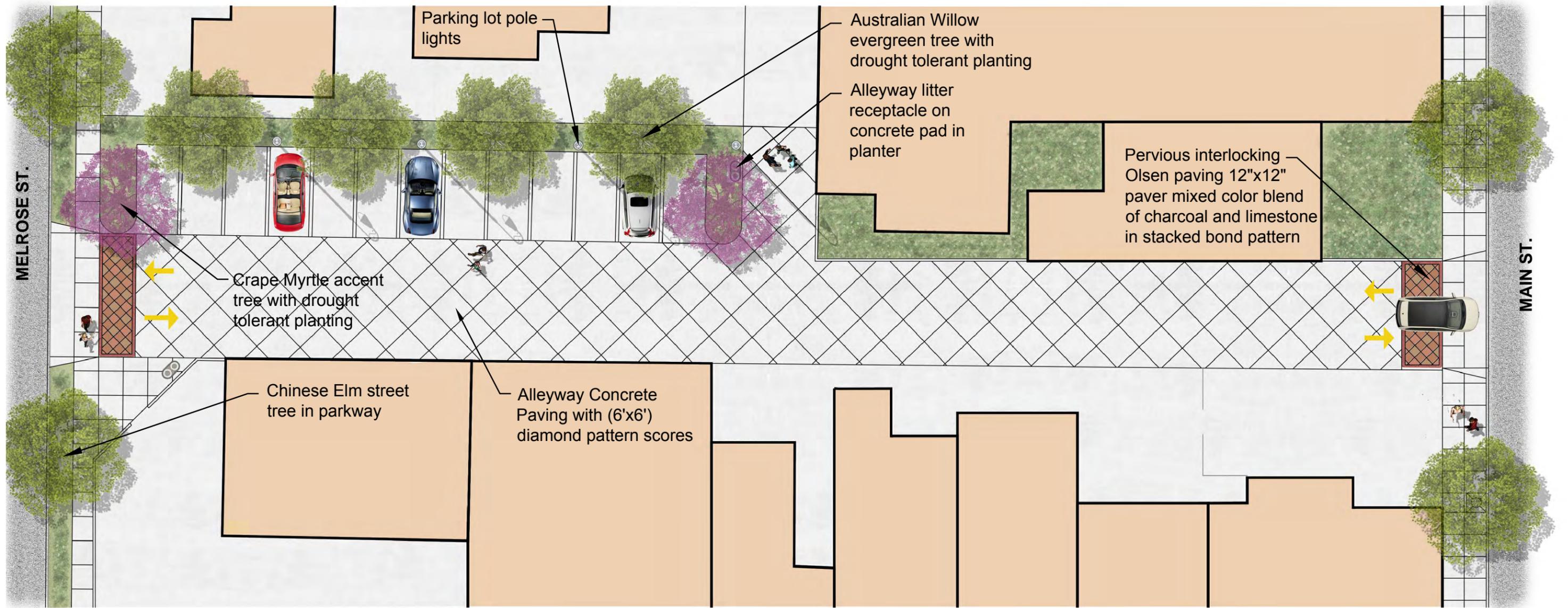


Enlargement A - Alleyway & Parking Lot
 Old Town Placentia
 Placentia, CA

KEY MAP

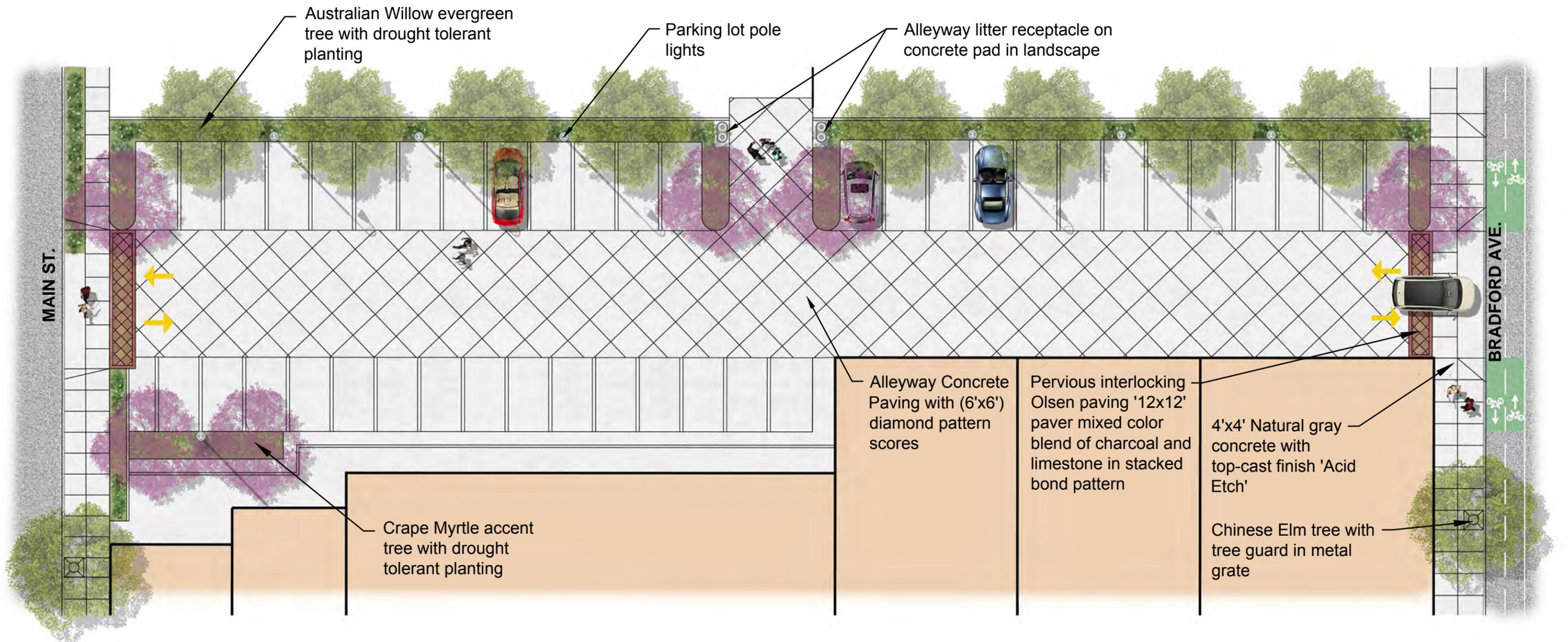
Future City of
 Placentia Metrolink
 Station



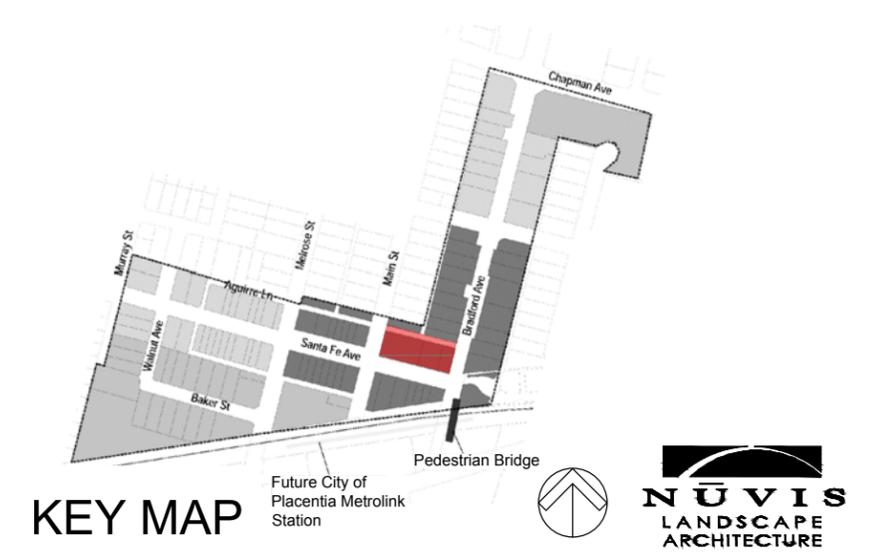


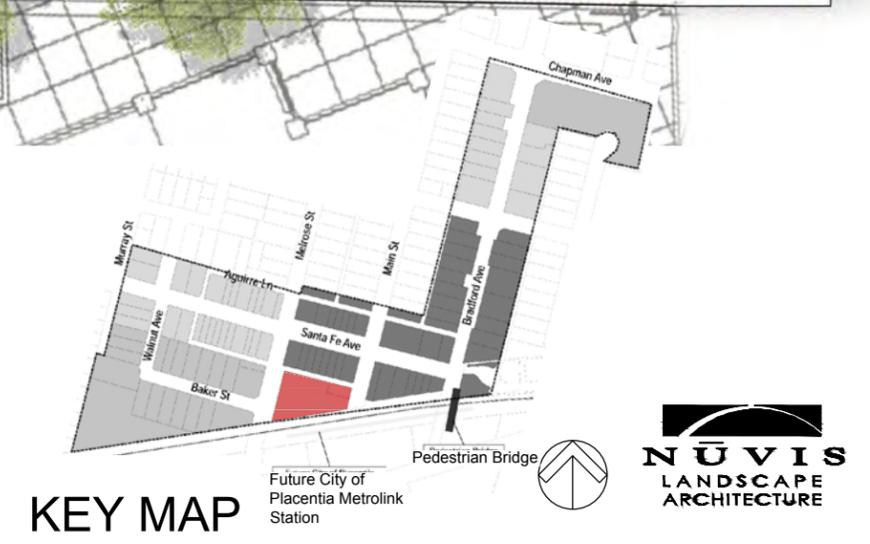
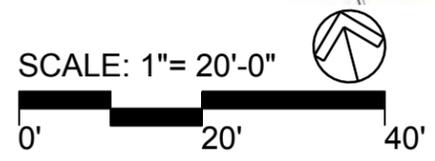
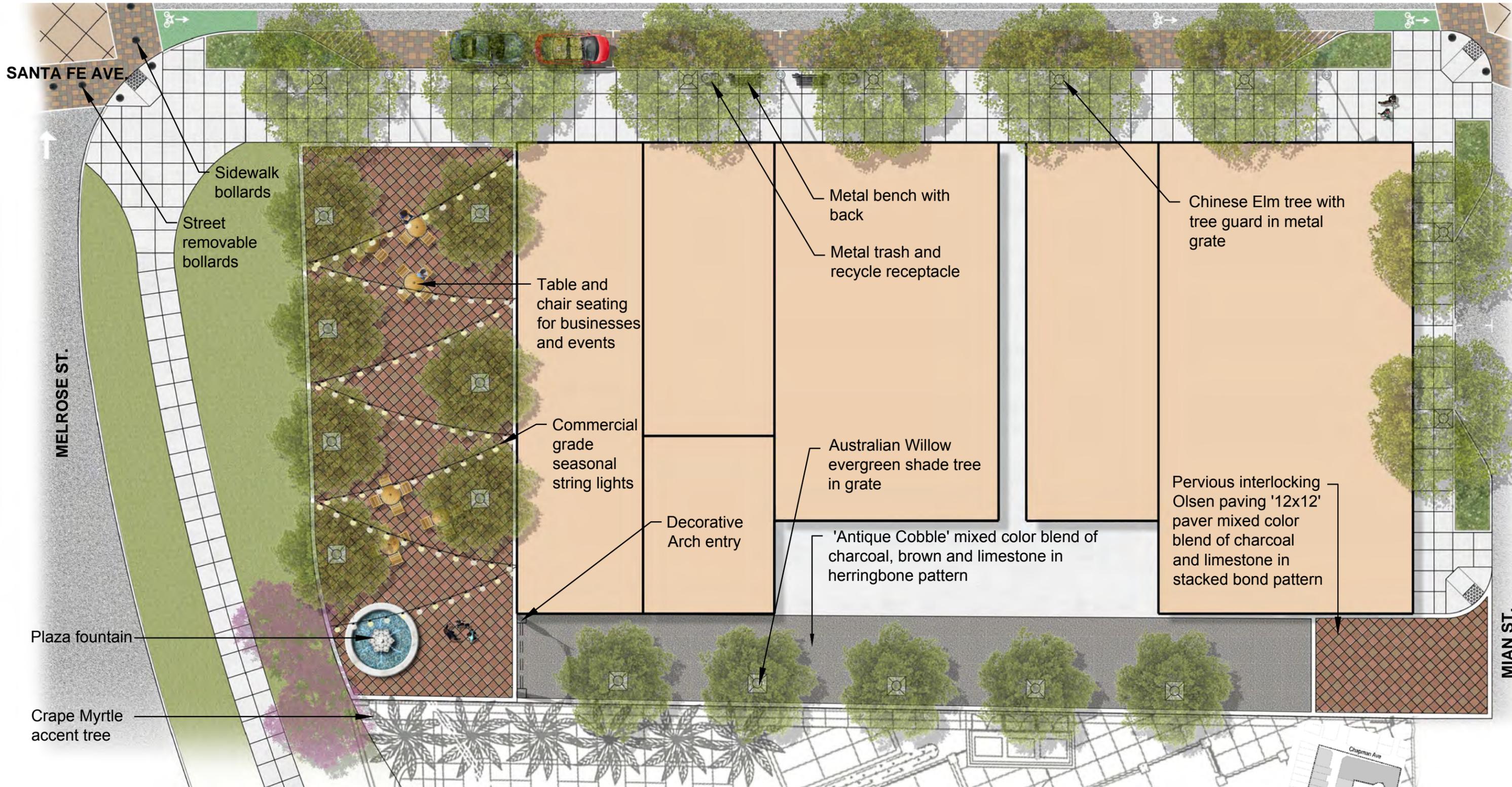
Enlargement B - Alleyway and Parking Lot
 Old Town Placentia
 Placentia, CA



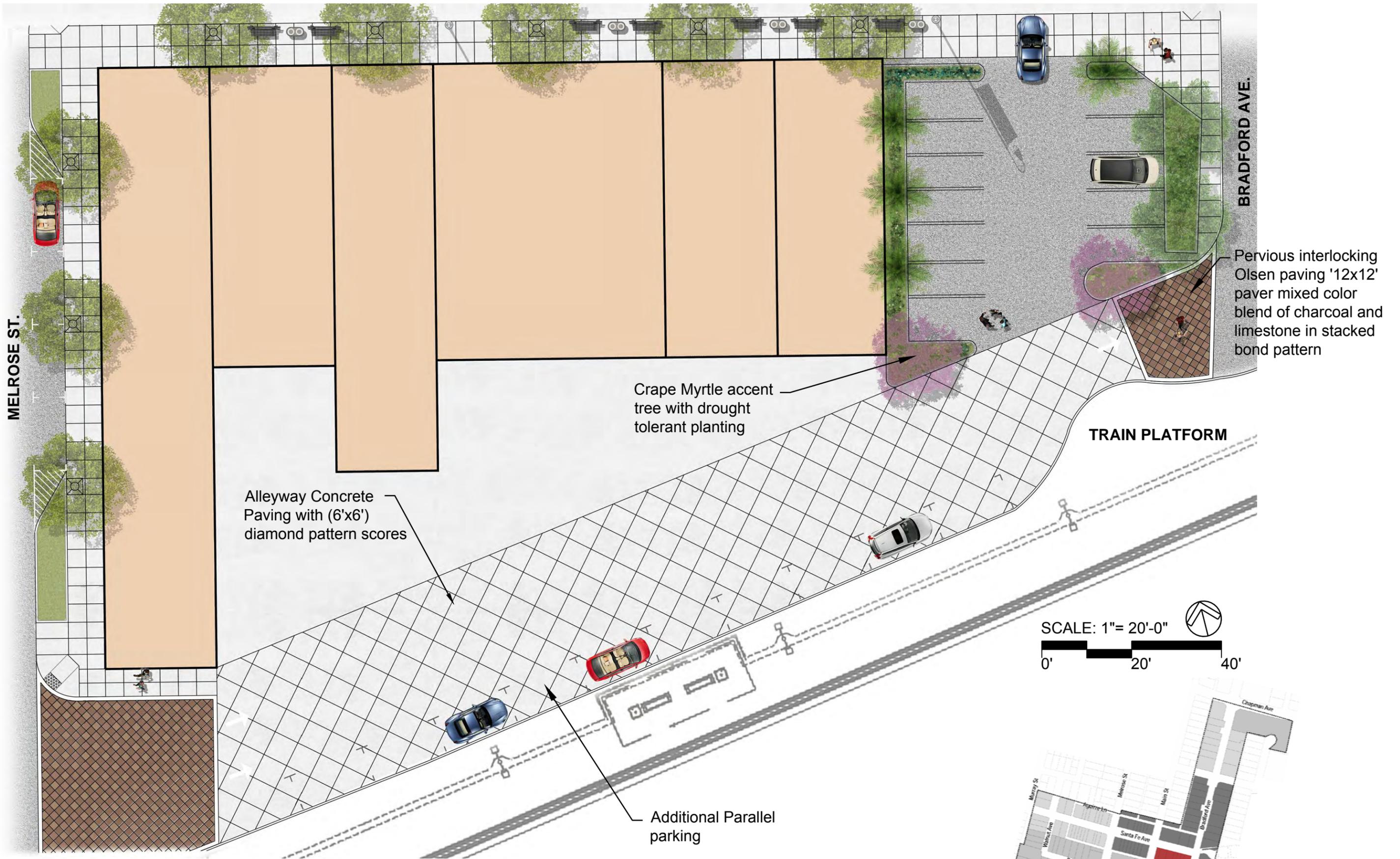


Enlargement C - Alleyway and Parking Lot
 Old Town Placentia
 Placentia, CA

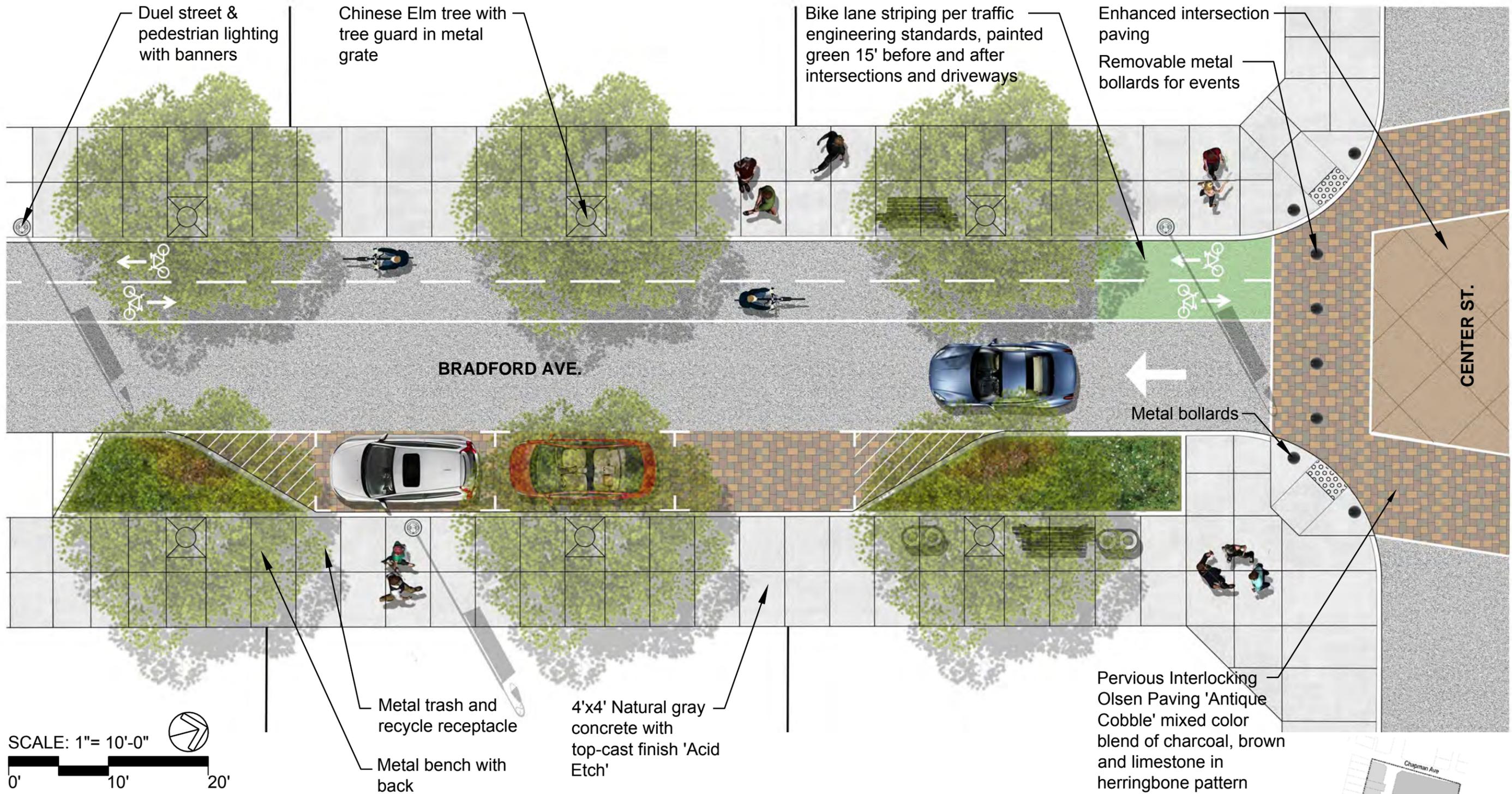




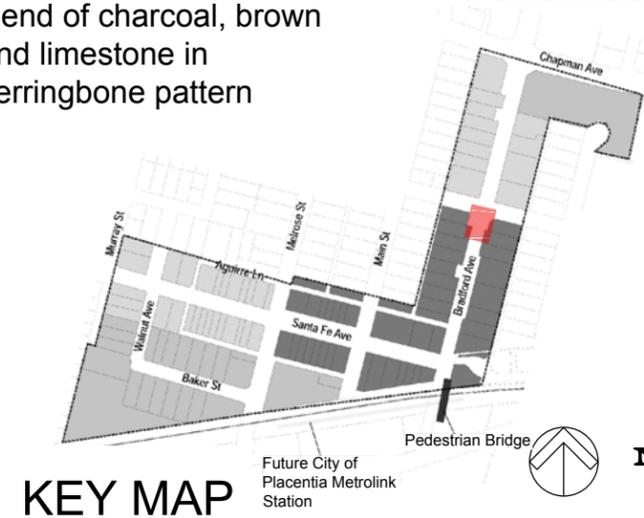
Enlargement D - Plaza Space and Alleyway
 Old Town Placentia
 Placentia, CA



Enlargement E - One way street and Alleyway
 Old Town Placentia
 Placentia, CA



Enlargement F - Bradford Avenue
 Old Town Placentia
 Placentia, CA





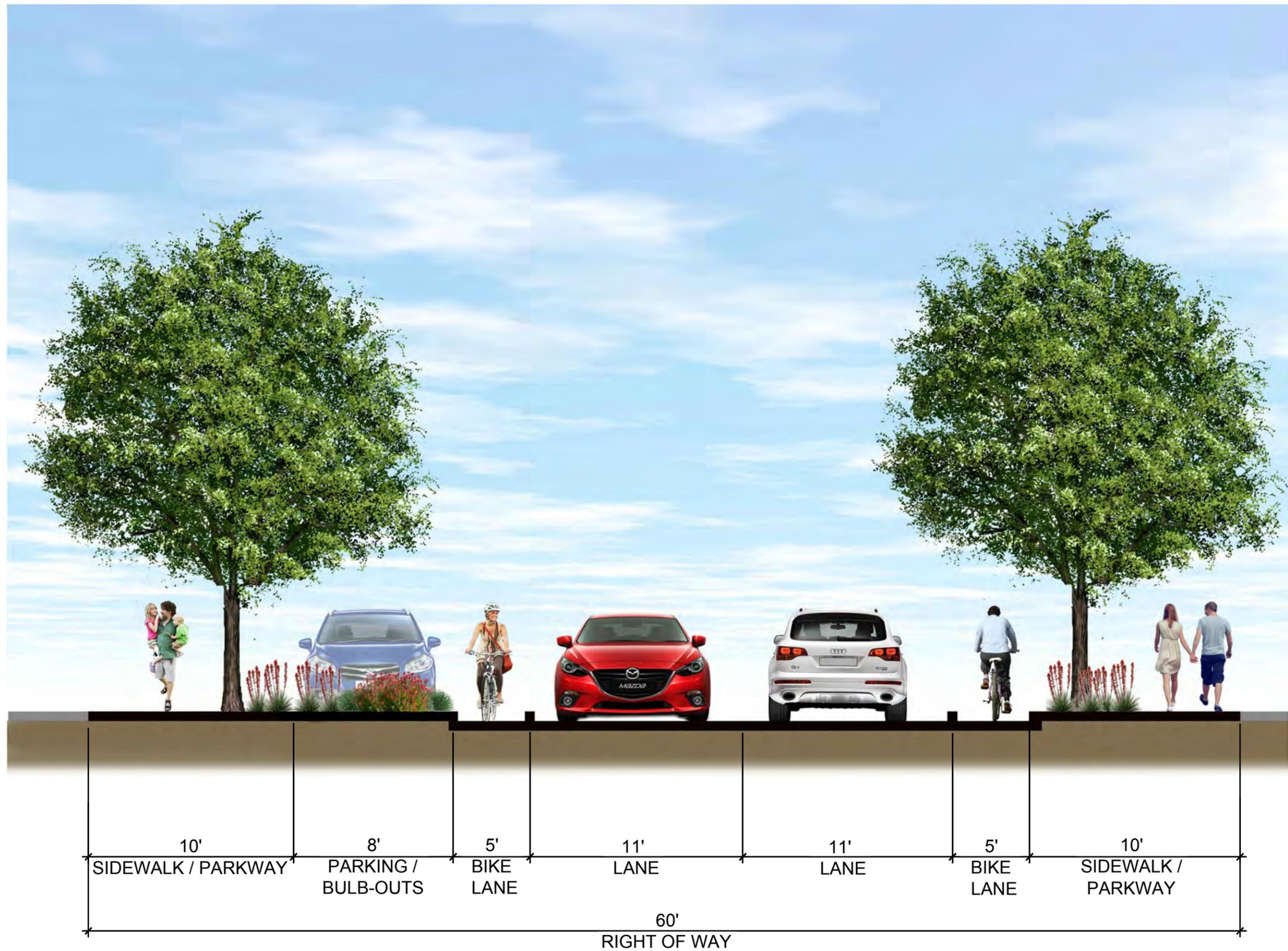
Simulation 1 - Bradford Avenue (Southbound)
OLD TOWN PLACENTIA
Placentia, CA



Simulation 2 - Bradford mid-way (Southbound)
OLD TOWN PLACENTIA
Placentia, CA

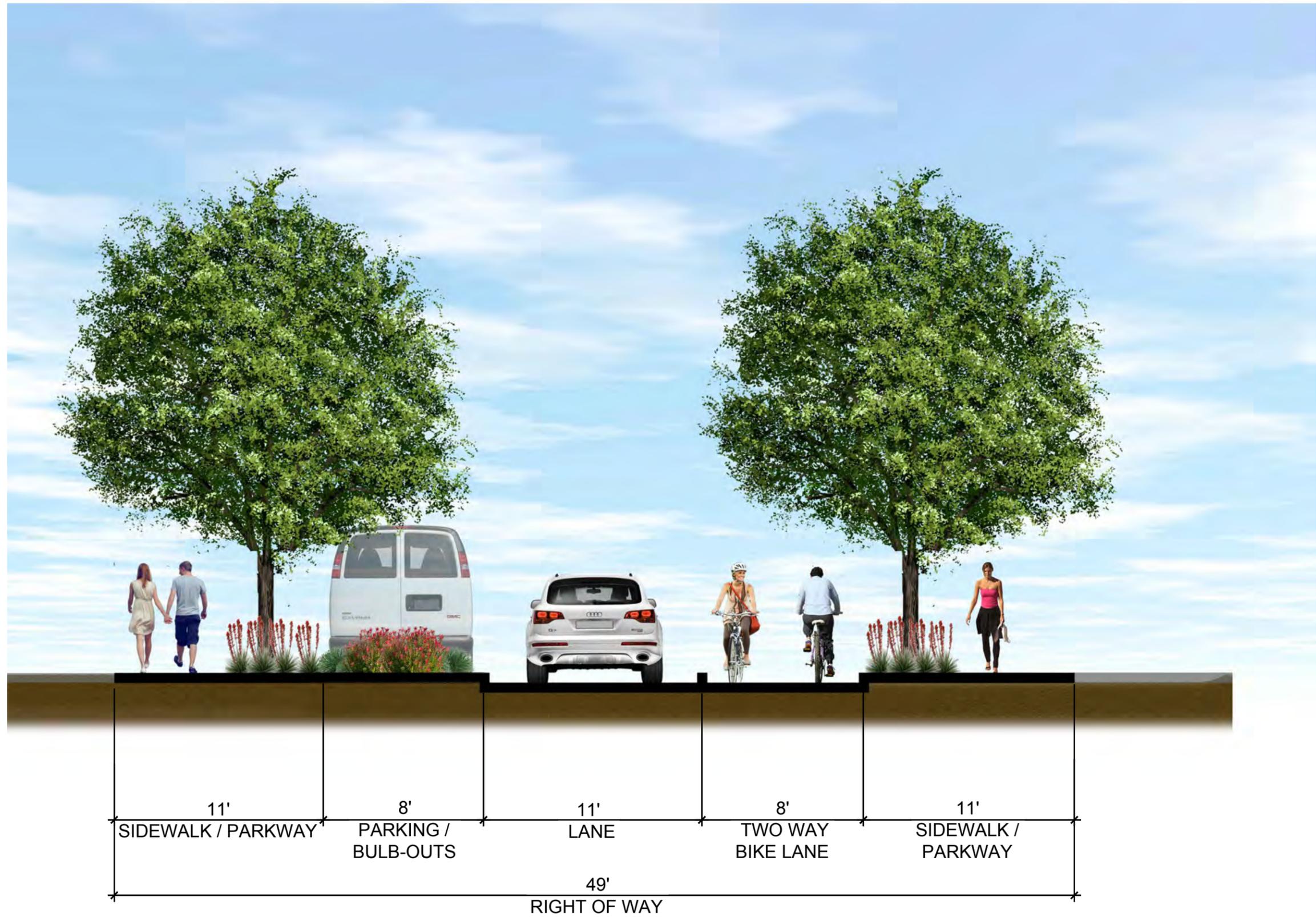


Simulation 3 (Alternative) - Santa Fe looking west
OLD TOWN PLACENTIA
Placentia, CA



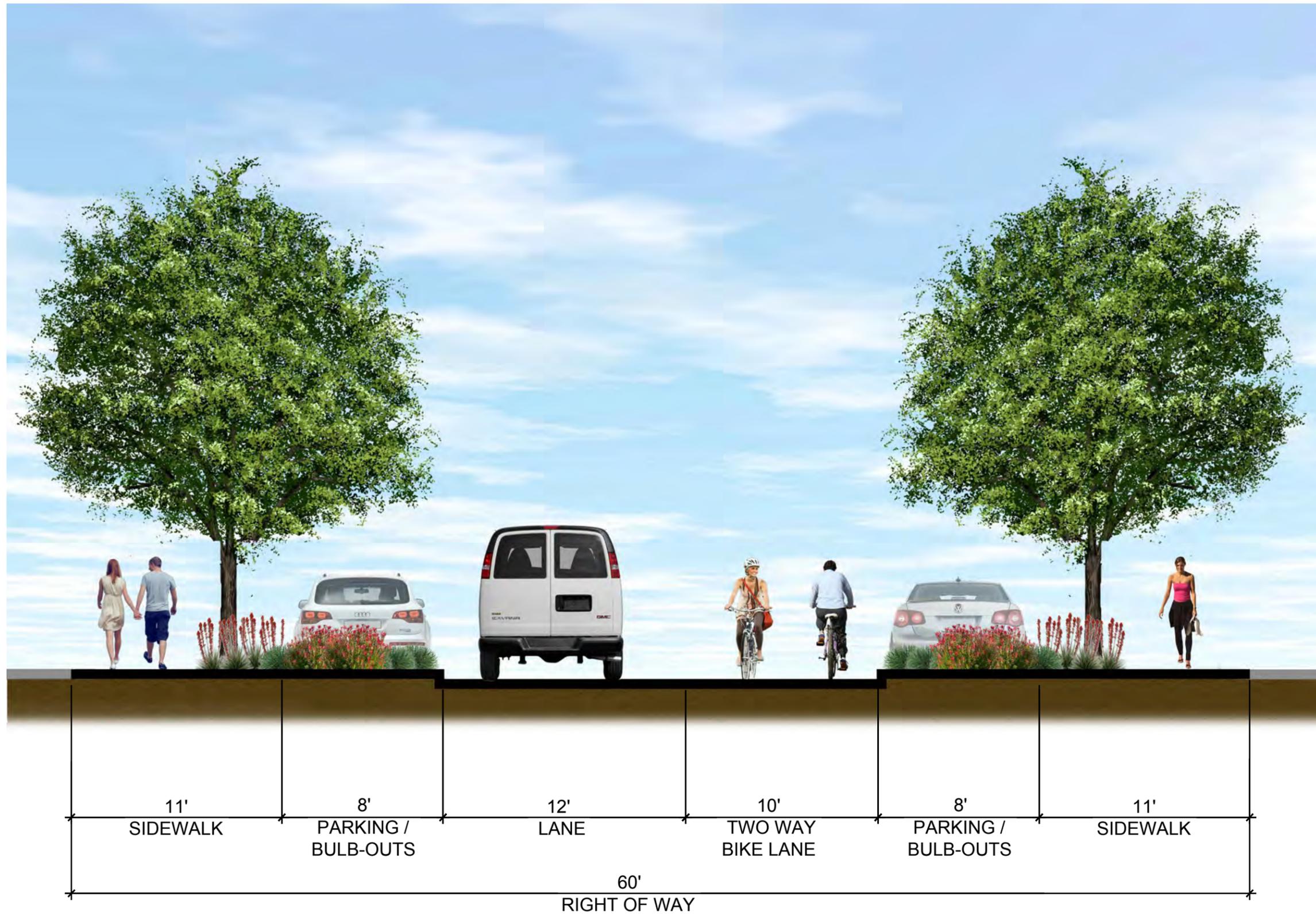
Section A - Bradford Avenue
 OLD TOWN PLACENTIA
 Placentia, CA

BRADFORD AVENUE
 CHAPMAN AVE. TO CENTER ST. (SOUTHBOUND)



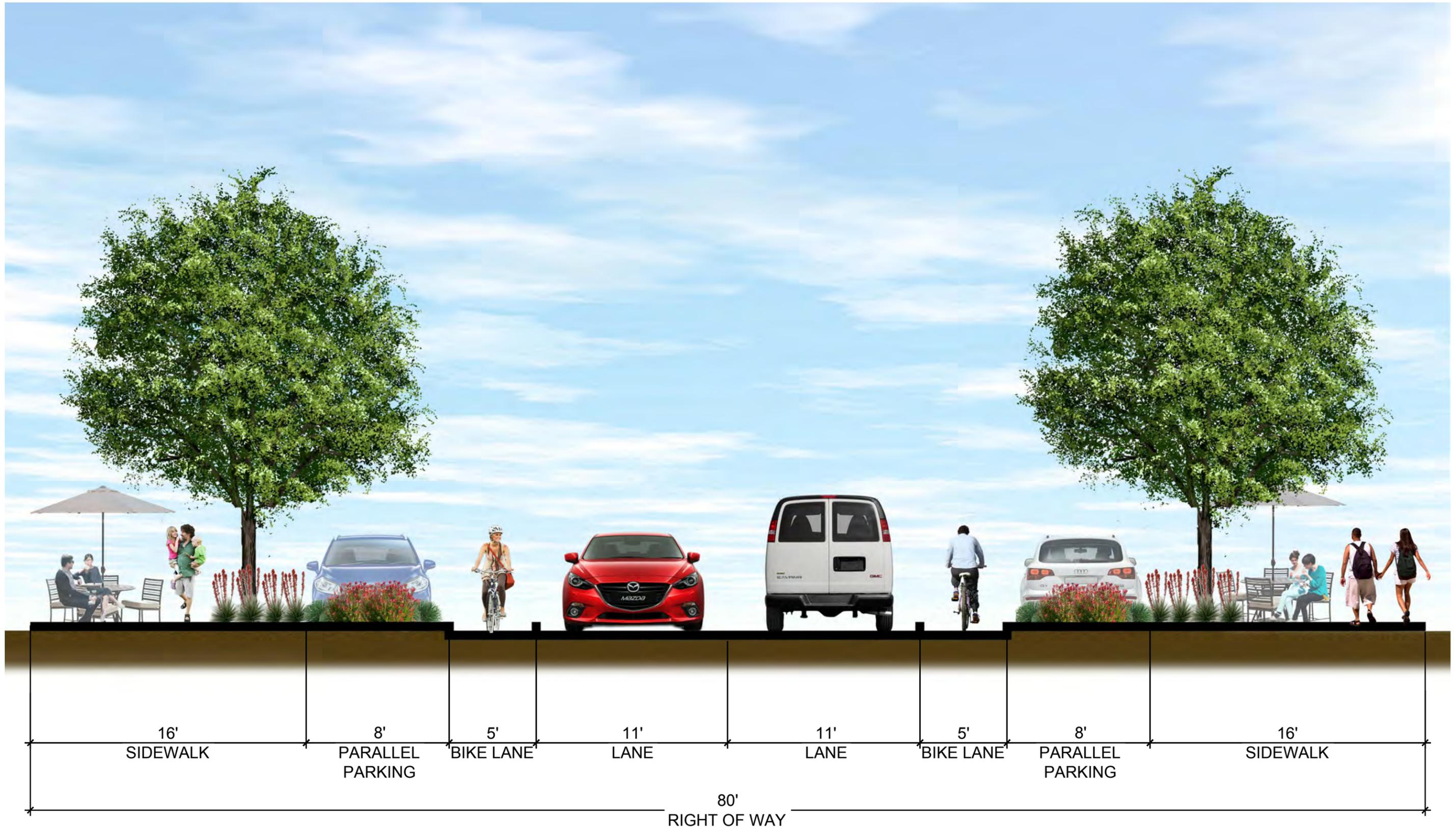
**BRADFORD AVENUE
CENTER ST. TO SANTA FE AVE. (SOUTHBOUND)**

Section B- Bradford Avenue
OLD TOWN PLACENTIA
 Placentia, CA



**SANTA FE AVENUE
BRADFORD AVE. TO MAIN ST. (WESTBOUND)**

Section C - Santa Fe Avenue
OLD TOWN PLACENTIA
 Placentia, CA



**SANTA FE AVENUE
MAIN ST. TO MELROSE AVE. (WESTBOUND)**

Section D - Santa Fe Avenue
OLD TOWN PLACENTIA
 Placentia, CA



CBF-10
 DUCTILE-IRON CASTING
 RECYCLED SOLID STEEL BAR

4' LENGTHS - STANDARD
 6' AND 8' LENGTHS WITH INTERMEDIATE ARMREST

COLOR BLACK

VICTOR STANLEY, INC.
 -Manufacturers of Quality Site Furnishings since 1962-

P.O. DRAWER 330 - DUNKIRK, MD 20754 USA
 TOLL FREE: (800) 368-2573 (USA & CANADA)
 TEL (301) 855-6300 - FAX (410) 257-7579
 WEB SITE: HTTP://WWW.VICTORSTANLEY.COM

* ALL DIMENSIONS ARE IN INCHES *

AVAILABLE OPTIONS:
 POWDER COATING
 10 STANDARD COLORS, 2 OPTIONAL METALLIC COLORS,
 CUSTOM COLORS (INCLUDING THE RAL RANGE)

LENGTHS
 STANDARD 4'
 STANDARD 6'
 STANDARD 8' LENGTH SHOWN

NOTES:
 1. DUCTILE IRON CASTINGS COME WITH A TEN YEAR WARRANTY AGAINST BREAKAGE.
 2. DRAWINGS NOT TO SCALE. DO NOT SCALE DRAWINGS.
 3. ALL FABRICATED METAL COMPONENTS ARE STEEL SHOTBLASTED, ETCHED, PHOSPHATIZED, PREHEATED, AND ELECTROSTATICALLY POWDER-COATED WITH T.G.I.C. POLYESTER POWDER COATINGS. PRODUCTS ARE FULLY CLEANED AND PRETREATED, PREHEATED AND COATED WHILE HOT TO FILL CREVICES AND BUILD COATING FILM. COATED PARTS ARE THEN FULLY CURED TO COATING MANUFACTURER'S SPECIFICATIONS. THE THICKNESS OF THE RESULTING FINISH AVERAGES 8-10 MILS (200-250 MICRONS).
 4. IT IS NOT RECOMMENDED TO LOCATE ANCHOR BOLTS UNTIL BENCH IS IN PLACE. THIS VICTOR STANLEY, INC. PRODUCT MUST BE PERMANENTLY AFFIXED TO THE GROUND. CONSULT YOUR LOCAL CODES FOR REGULATIONS.
 5. ANCHOR BOLTS NOT PROVIDED BY VICTOR STANLEY, INC.
 6. FOR HIGH SALT ABUSIVE CLIMATES, HOT DIP GALVANIZING BEFORE POWDER COATING IS AVAILABLE. SEE WRITTEN SPECIFICATIONS FOR DETAILS.
 7. ALL SPECIFICATIONS ARE SUBJECT TO CHANGE. CONTACT MANUFACTURER FOR DETAILS.
 8. THIS PRODUCT IS SHIPPED FULLY ASSEMBLED.

CBF-10
 CLASSIC SERIES
 BENCH WITH DUCTILE IRON END FRAMES
 SHOWN: STANDARD 8-FOOT LENGTH

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 REV. 3/15/12 DRAWN R.G.O. 2012-82

BENCHES
OLD TOWN PLACENTIA
 City of Placentia, CA

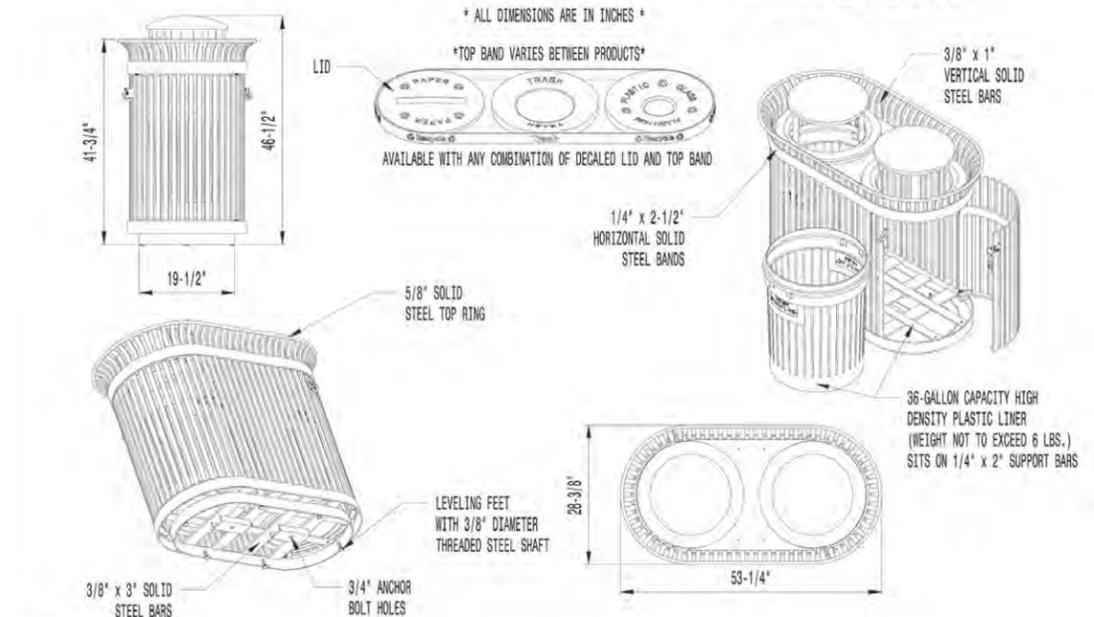




SD-242
 SIDE DOOR OPENING
 RECYCLED SOLID STEEL BAR
 'RECYCLE' TOP STEEL BAND
 TWO 36 GAL LINERS
 DOME LIDS
 COLOR BLACK

VICTOR STANLEY, INC.
 -Manufacturers of Quality Site Furnishings since 1962-

P.O. DRAWER 330 - DUNKIRK, MD 20754 USA
 TOLL FREE: (800) 368-2573 (USA & CANADA)
 TEL (301) 855-8300 - FAX (410) 257-7579
 WEB SITE: HTTP://WWW.VICTORSTANLEY.COM



AVAILABLE OPTIONS:
POWDER COATING
 10 STANDARD COLORS, 2 OPTIONAL METALLIC COLORS,
 CUSTOM COLORS (INCLUDING THE RAL RANGE)
SECURITY
 STANDARD WITH LOCKABLE LATCH (AS SHOWN). AVAILABLE WITH OPTIONAL KEYED
 LOCK BOX. LID BOLTED IN PLACE. AVAILABLE WITH OPTIONAL MOUNT WITH
 3 IN-LINE ANCHOR HOLES AND OPTIONAL BOTTOM PLATE COVER.

LIDS
 STANDARD WITH TAPERED FORMED LID. AVAILABLE WITH OPTIONAL DOME LID (AS SHOWN),
 DOME LID WITH ASHTRAY, RAIN BONNET LID, RAIN BONNET LID WITH ASHTRAY, CONVEX LID,
 CONVEX LID WITH SELF CLOSING DOOR, ENCLOSED DOME LID, ENCLOSED DOME LID WITH
 STAINLESS STEEL ASHTRAY, AND RECYCLE LIDS. ASHTRAYS AVAILABLE WITH
 OPTIONAL ASHTRAY COVER.
CUSTOM PLAQUES & DECALS
 AVAILABLE WITH STEEL PLAQUES IN VARIOUS SIZES AND PRESSURE SENSITIVE
 VINYL OUTDOOR DECALS.

NOTES:

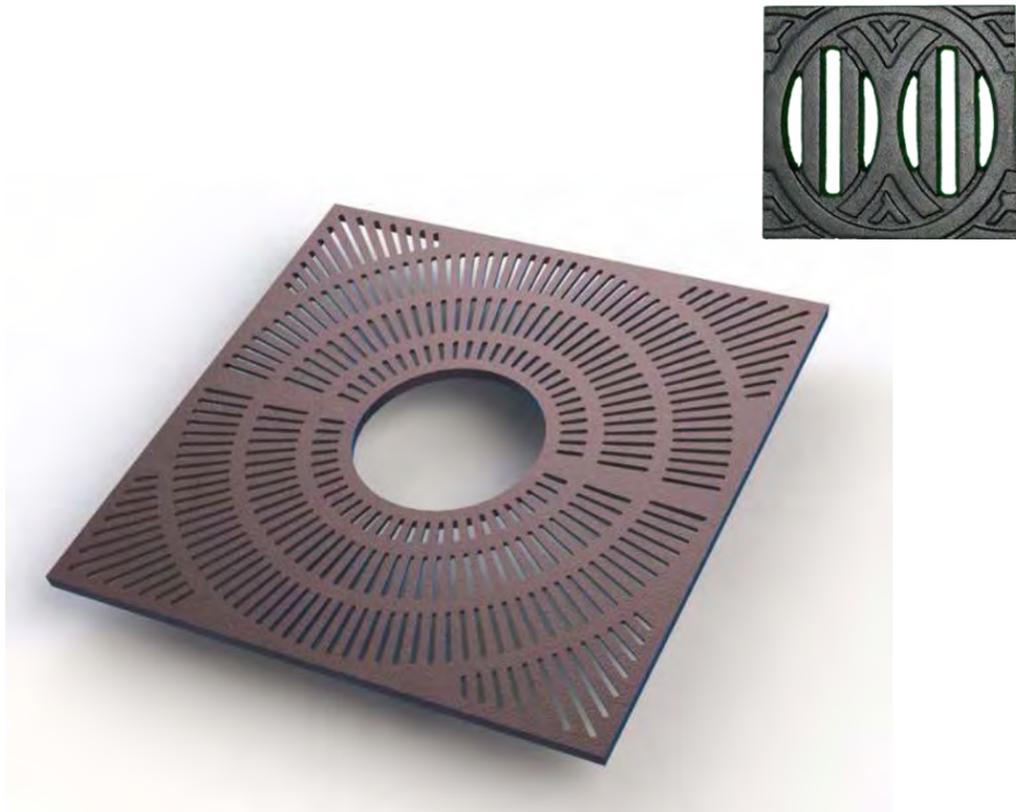
- DRAWINGS NOT TO SCALE. DO NOT SCALE DRAWINGS.
- ALL FABRICATED METAL COMPONENTS ARE STEEL SHOTBLASTED, ETCHED, PHOSPHATIZED, PREHEATED, AND ELECTROSTATICALLY POWDER-COATED WITH T.G.I.C. POLYESTER POWDER COATINGS. PRODUCTS ARE FULLY CLEANED AND PRETREATED, PREHEATED AND COATED WHILE HOT TO FILL CREVICES AND BUILD FILM COATING. COATED PARTS ARE THEN FULLY CURED TO COATING MANUFACTURER'S SPECIFICATIONS. THE THICKNESS OF THE RESULTING FINISH AVERAGES 8-10 MILS (200-250 MICRONS).
- OIL IMPREGNATED BRONZE BUSHINGS AND STAINLESS STEEL PIVOT PINS FOR DOOR MOVEMENT, STANDARD 3/16\" SOLID STEEL LATCH ASSEMBLY OR OPTIONAL PATENTED STAINLESS STEEL KEYED LOCK ASSEMBLY.
- THIS VICTOR STANLEY, INC. PRODUCT MUST BE PERMANENTLY AFFIXED TO THE GROUND. CONSULT YOUR LOCAL CODES FOR REGULATIONS.
- VICTOR STANLEY, INC., PLASTIC INNER LINERS ARE MOLDED ON TOOLING DESIGNED FOR AND OWNED BY VICTOR STANLEY, INC. THEY OFFER MAXIMUM CAPACITY AND STRENGTH WITH LIGHTWEIGHT CONSTRUCTION USING CRITICAL MOLDED RIBS, INTEGRAL HANDHOLDS, AND HIGH-STRENGTH MATERIALS. THIS MINIMIZES HANDLING DIFFICULTY AND FACILITATES EASY EMPTYING AND STORAGE WHILE AFFORDING LONG SERVICE LIFE.
- ANCHOR BOLT NOT PROVIDED BY VICTOR STANLEY, INC.
- FOR HIGH SALT ABUSIVE CLIMATES, HOT-DIP GALVANIZING BEFORE POWDER COATING IS AVAILABLE. HOT-DIP GALVANIZING IS PERFORMED FOR VICTOR STANLEY, INC. BY AN EXPERIENCED QUALIFIED FIRM TO WHICH PRODUCTS ARE SHIPPED FOR GALVANIZING. HOT-DIP GALVANIZING INCLUDES AN AGGRESSIVE PRE-TREATMENT AND IMMERSION IN A TANK OF CHARGED LIQUID ZINC AT OR AROUND 860°F (460°C). THE RESULTING SURFACE IS RESISTANT TO RUST BUT HAS SOME UNEVENNESS RESULTING FROM THE BONDING OF THE ZINC TO THE STEEL SURFACE. AS A RESULT, THE POWDER-COATING SURFACE FINISH OVER THAT GALVANIZED SURFACE MAY EXHIBIT BUMPS, UNEVENNESS, AND MAY NOT BE AS SMOOTH AS THE STANDARD FINISH; THIS UNEVEN AND INCONSISTENT FINISH IS NORMAL FOR GALVANIZING. CONTACT MANUFACTURER FOR DETAILS.
- ALL SPECIFICATIONS ARE SUBJECT TO CHANGE. CONTACT MANUFACTURER FOR DETAILS.
- THIS PRODUCT IS SHIPPED FULLY ASSEMBLED.

SD-242
 IRONSITES® SERIES SIDE-DOOR RECYCLING STATION
 SHOWN: (2) OPTIONAL DOME LID

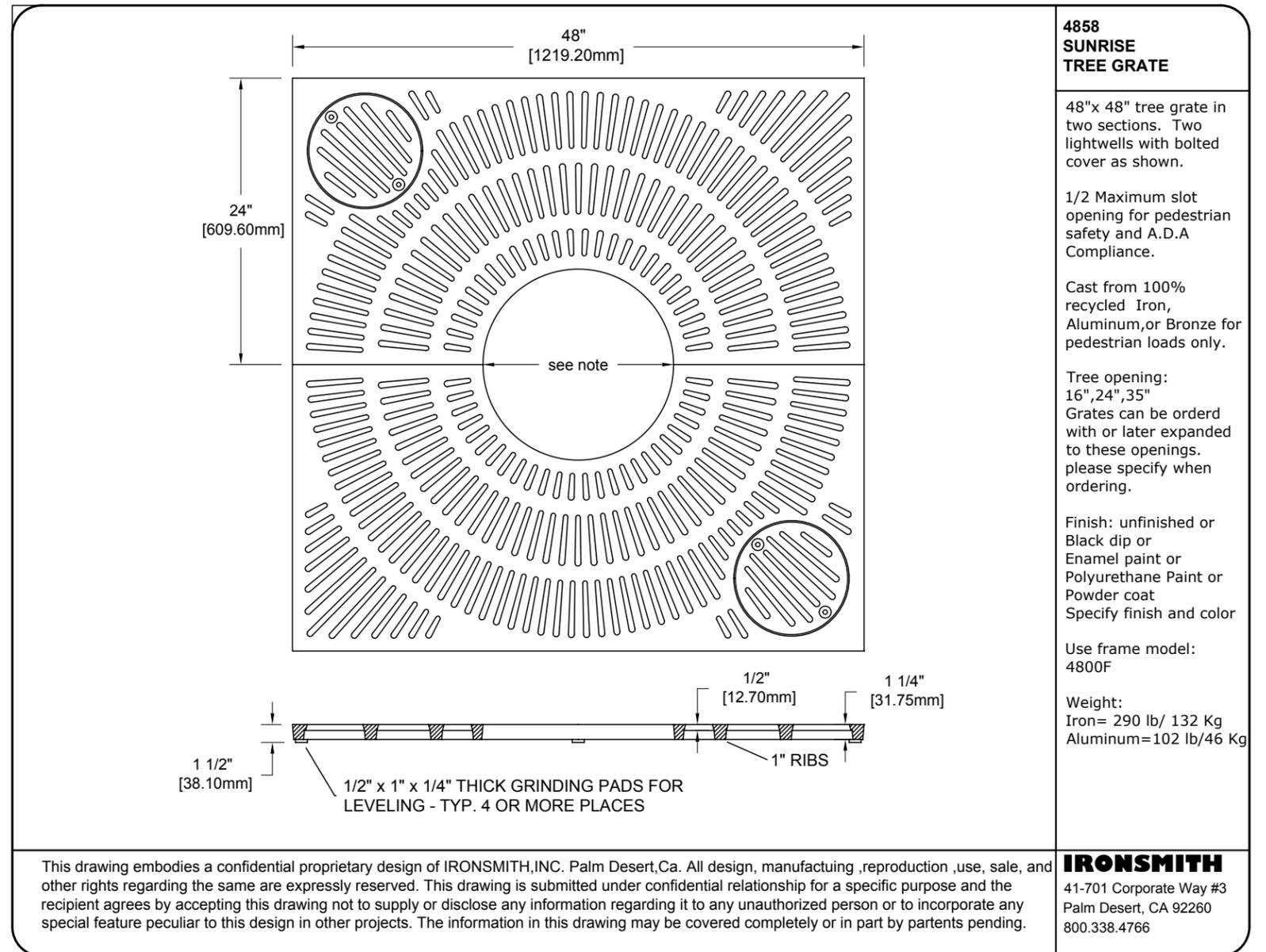
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 REV. 10/15/15 DRAWN C.D.B. 2015-1078

LITTER RECEPTACLES
 OLD TOWN PLACENTIA
 City of Placentia, CA





SUNRISE MODEL 48" SQUARE TREE GRATES
 TREE OPENINGS TO BE 24" WITH OPTION FOR EXPANSION TO 35"
 2 UP-LIGHTWELLS FLUSH OR BELOW GRADE
 PLUMBED WITH CONDUIT TO ACCOMODATE APPROX. 4 PLUGS
 COLOR TO BLACK POWDERED



TREE GRATE
 OLD TOWN PLACENTIA
 City of Placentia, CA



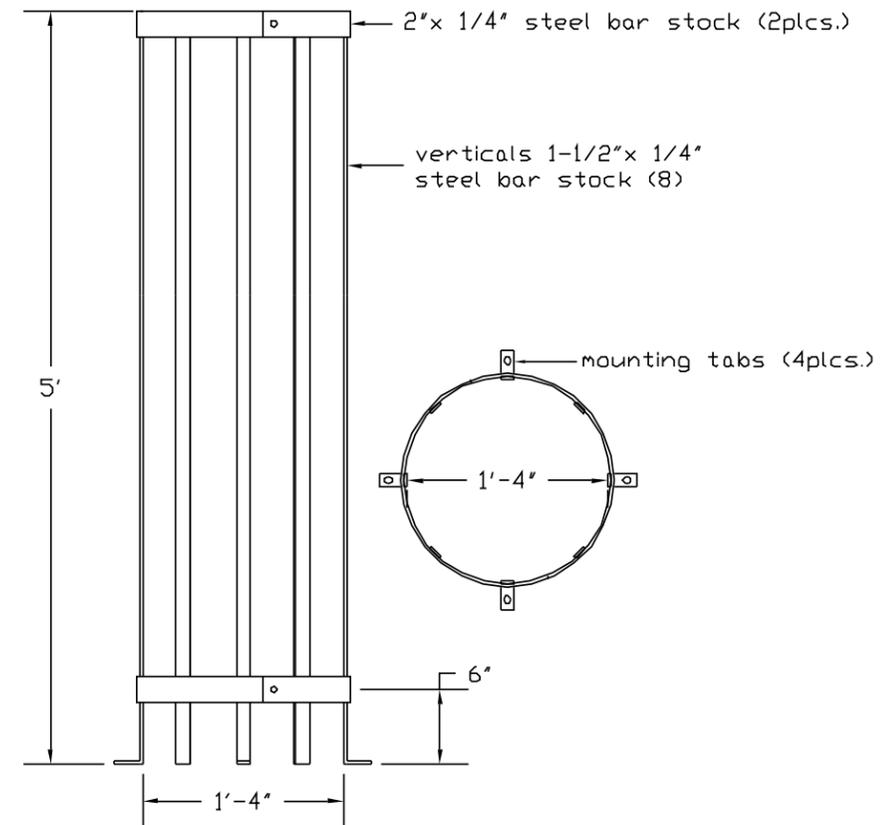


M6 TREE GUARD

BLACK POWDER
COAT FINISH

TREE GUARD

M-6



- Jig welded from hot rolled steel.
- Guards are constructed in halves and bolt together
- 60" height and 16" tree opening standard
other dimensions available for additional cost
- Alternate attachment methods available
- All hardware provided by IRONSMITH
- Black powder coat standard finish
other finish options available

IRONSMITH TREE GUARDS
FOR LANDSCAPE ARCHITECTURE

(800) 338-4766

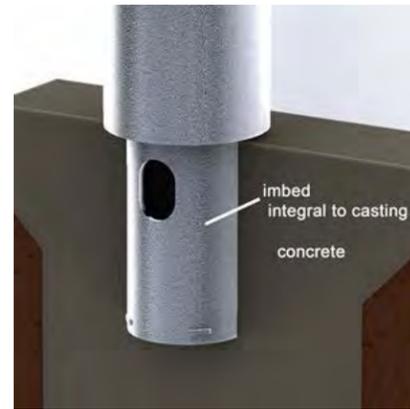
TREE GUARD
OLD TOWN PLACENTIA
City of Placentia, CA





SALEM - 9023
35" HIGH / 15" BASE
FIXED OR REMOVABLE

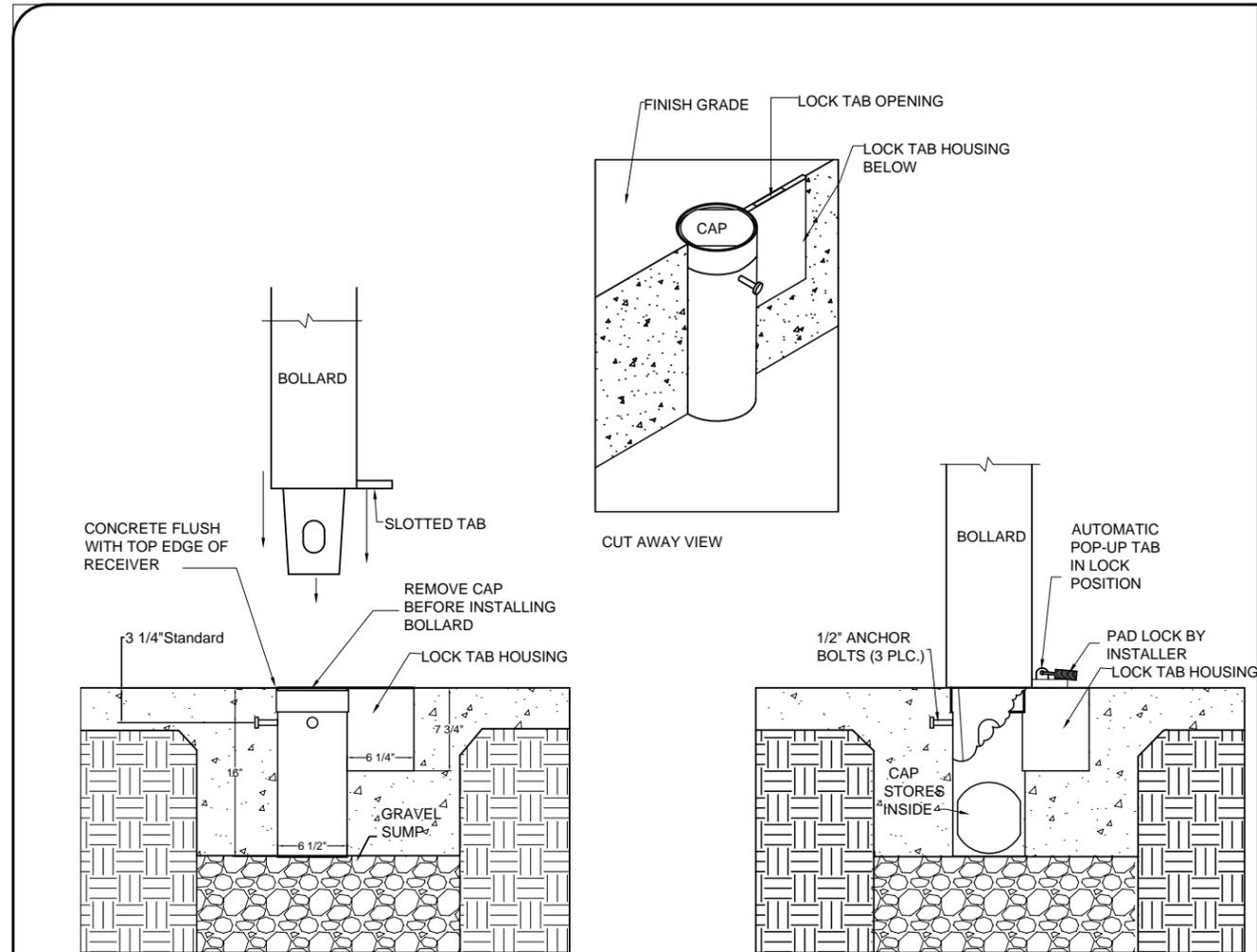
BLACK POWDER
COAT FINISH



FIXED INSTALL



REMOVABLE INSTALL



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Receiver for removable cast bollards.
9020, 9022, 9023, 9024

GRAVEL BASE IS SUGGESTED FOR DRAINAGE. RECEIVER IS OPEN AT THE BOTTOM.

CONCRETE PROFILE AND SUMP DEPTH TO BE DETERMINE BY INSTALLER BASED ON SITE CONDITIONS.

LOCK TAB OPENING MUST BE KEPT CLEAR. PROTECT WITH TAPE DURING CONCRETE POUR.

THE SUPPLIED CAP IS FOR TEMPORARY SHORT TERM COVERAGE AND NOT FOR HIGH SPEED VEHICLE TRAFFIC. AN OPTIONAL LONG TERM CAP MAY BE ORDERED.

IRONSMITH
41-701 Corporate Way #3
Palm Desert, CA 92260
(800) 338-4766
(760) 776-5080 Fax

BOLLARDS
OLD TOWN PLACENTIA
City of Placentia, CA



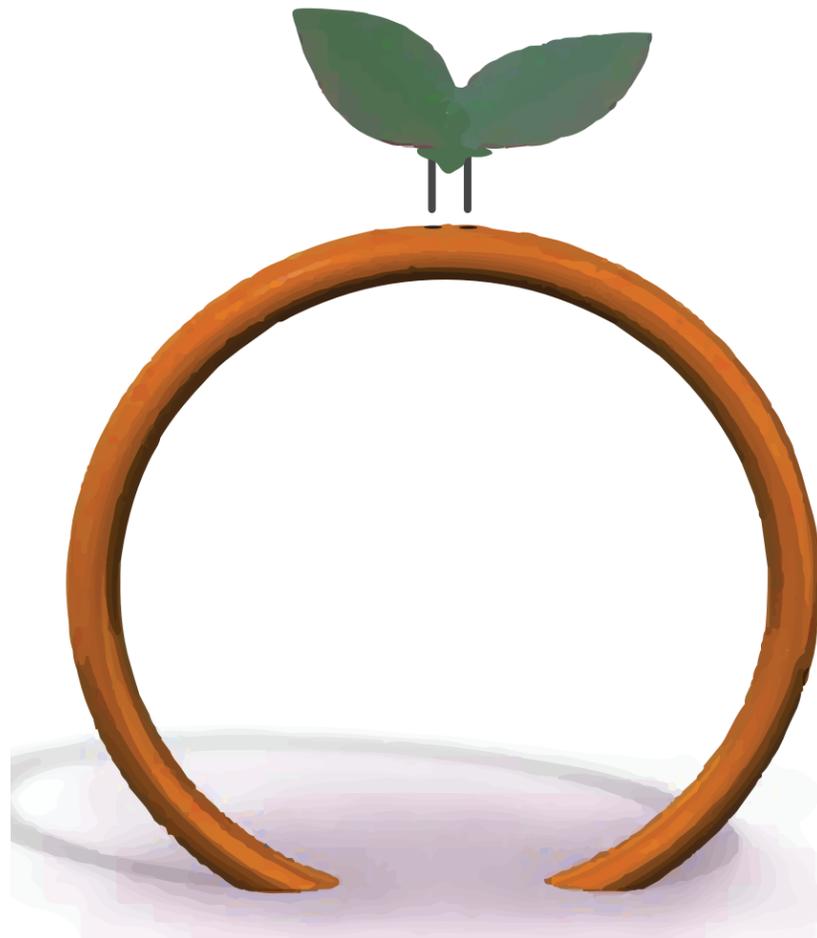


BRHS-101
CUSTOM 'ORANGE' CONCEPT
 CIRCULAR BIKE RACK
 60 MM (OD) TUBULAR STEEL
 POWDER COATED
 IN-GROUND MOUNT
 RACK COLOR RAL 2000 YELLOW ORANGE

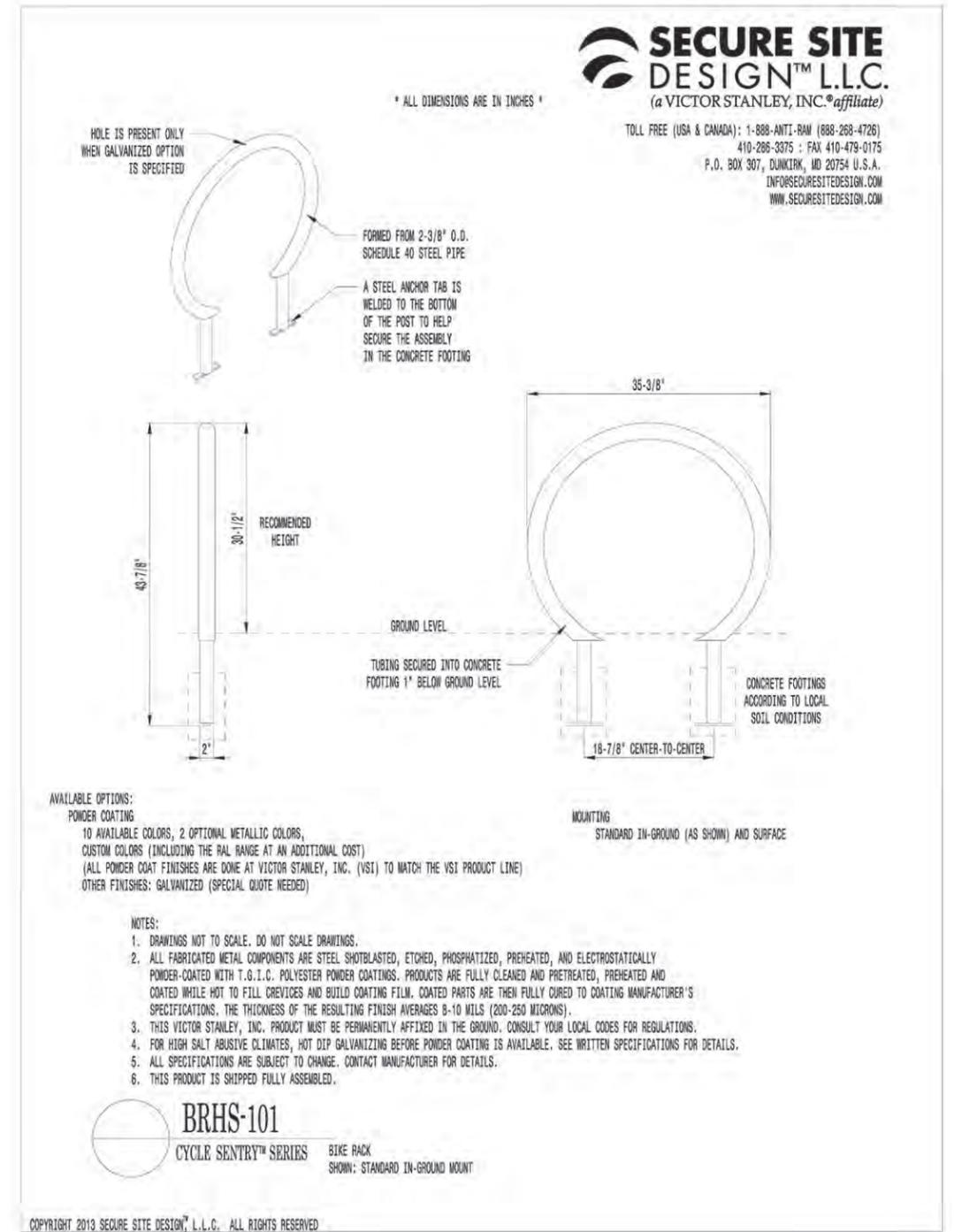
RAL 2000 237-118-014 #ED760E Yellow orange

CUSTOM METAL 'LEAVES'
 WITH VS CUSTOM BOLTED BRACKET
 LEAVES COLOR RAL 6010 GRASS GREEN

RAL 6010 053-104-045 #35682D Grass green



LEAVES & STEM
BRACKET CONNECTION
 'STAR-SHAPED' QUARTER ROUND BRACKET
 BOLTED ONTO FRAME



CUSTOM BIKE RACK
 OLD TOWN PLACENTIA
 City of Placentia, CA



CRPR-4 SEAT
 RECYCLED SOLID STEEL BAR MATERIAL
 IN-GROUND OR SURFACE MOUNT
 COLOR BLACK



CRPR-3 SEAT
 RECYCLED SOLID STEEL BAR MATERIAL
 IN-GROUND OR SURFACE MOUNT
 COLOR BLACK

VICTOR STANLEY, INC.
Manufacturers of Quality Site Furnishings since 1962

P.O. DRAWER 330 - DUNKIRK, MD 20754 USA
 TOLL FREE: (800) 369-2573 (USA & CANADA)
 TEL: (301) 855-8300 - FAX: (410) 257-7579
 WEB SITE: [HTTP://WWW.VICTORSTANLEY.COM](http://www.victorstanley.com)

AVAILABLE OPTIONS:
 POWDER COATING
 10 STANDARD COLORS, CUSTOM COLORS (INCLUDING THE RAL RANGE)
 MOUNTING
 STANDARD IN-GROUND (AS SHOWN) AND OPTIONAL SURFACE

TABLE SIZES
 STANDARD 3' SQUARE TOP
 STANDARD 4' SQUARE TOP (AS SHOWN)

NOTES:
 1. DRAWING NOT TO SCALE. DO NOT SCALE DRAWINGS.
 2. ALL FABRICATED METAL COMPONENTS ARE STEEL SHOTBLASTED, ETCHED, PHOSPHATIZED, PREHEATED, AND ELECTROSTATICALLY POWDER-COATED WITH T.G.I.C. POLYESTER POWDER COATINGS. PRODUCTS ARE FULLY CLEANED AND PRETREATED, PREHEATED AND COATED WHILE HOT TO FILL CREVICES AND BUILD FILM COATING. COATED PARTS ARE THEN FULLY CURED TO COATING MANUFACTURER'S SPECIFICATIONS. THE THICKNESS OF THE RESULTING FINISH AVERAGES 8-10 MILS (200-250 MICRONS).
 3. THIS VICTOR STANLEY, INC. PRODUCT MUST BE PERMANENTLY AFFIXED IN THE GROUND. CONSULT YOUR LOCAL CODES FOR REGULATIONS.
 4. FOR HIGH SALT ABUSIVE CLIMATES, HOT DIP GALVANIZING BEFORE POWDER COATING IS AVAILABLE. SEE WRITTEN SPECIFICATIONS FOR DETAILS.
 5. ALL SPECIFICATIONS ARE SUBJECT TO CHANGE. CONTACT MANUFACTURER FOR DETAILS.
 6. THIS PRODUCT IS SHIPPED PARTIALLY UNASSEMBLED.

CRPR-4
 STEEL SITES™ SERIES
 ALL STEEL CENTER-POST TABLE
 SHOWN: STANDARD 4-FOOT SQUARE TOP
 STANDARD IN-GROUND MOUNT

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Manufacturers of Quality Site Furnishings since 1962

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 TOLL FREE: (800) 369-2573 (USA & CANADA)
 TEL: (301) 855-8300 - FAX: (410) 257-7579
 WEB SITE: [HTTP://WWW.VICTORSTANLEY.COM](http://www.victorstanley.com)

AVAILABLE OPTIONS:
 POWDER COATING
 10 STANDARD COLORS, CUSTOM COLORS (INCLUDING THE RAL RANGE)
 MOUNTING
 STANDARD IN-GROUND AND OPTIONAL SURFACE (AS SHOWN)

TABLE SIZES
 STANDARD 3' SQUARE TOP
 STANDARD 4' SQUARE TOP (AS SHOWN)

NOTES:
 1. DRAWINGS NOT TO SCALE. DO NOT SCALE DRAWINGS.
 2. ALL FABRICATED METAL COMPONENTS ARE STEEL SHOTBLASTED, ETCHED, PHOSPHATIZED, PREHEATED, AND ELECTROSTATICALLY POWDER-COATED WITH T.G.I.C. POLYESTER POWDER COATINGS. PRODUCTS ARE FULLY CLEANED AND PRETREATED, PREHEATED AND COATED WHILE HOT TO FILL CREVICES AND BUILD FILM COATING. COATED PARTS ARE THEN FULLY CURED TO COATING MANUFACTURER'S SPECIFICATIONS. THE THICKNESS OF THE RESULTING FINISH AVERAGES 8-10 MILS (200-250 MICRONS).
 3. IT IS NOT RECOMMENDED TO LOCATE ANCHOR BOLTS UNTIL TABLE IS IN PLACE. THIS VICTOR STANLEY, INC. PRODUCT MUST BE PERMANENTLY AFFIXED TO THE GROUND. CONSULT YOUR LOCAL CODES FOR REGULATIONS.
 4. ANCHOR BOLTS NOT PROVIDED BY VICTOR STANLEY, INC.
 5. FOR HIGH SALT ABUSIVE CLIMATES, HOT DIP GALVANIZING BEFORE POWDER COATING IS AVAILABLE. SEE WRITTEN SPECIFICATIONS FOR DETAILS.
 6. ALL SPECIFICATIONS ARE SUBJECT TO CHANGE. CONTACT MANUFACTURER FOR DETAILS.
 7. THIS PRODUCT IS SHIPPED PARTIALLY UNASSEMBLED.

CRPR-3
 STEEL SITES™ SERIES
 ALL STEEL CENTER-POST TABLE
 SHOWN: STANDARD 4-FOOT SQUARE TOP
 OPTIONAL SURFACE MOUNT

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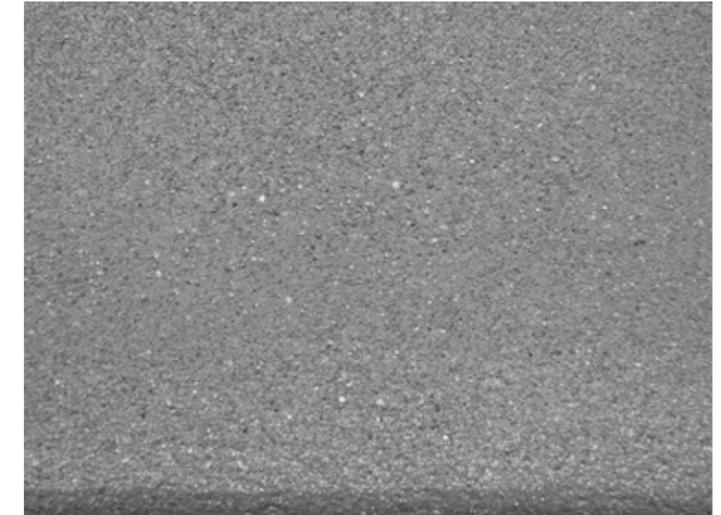
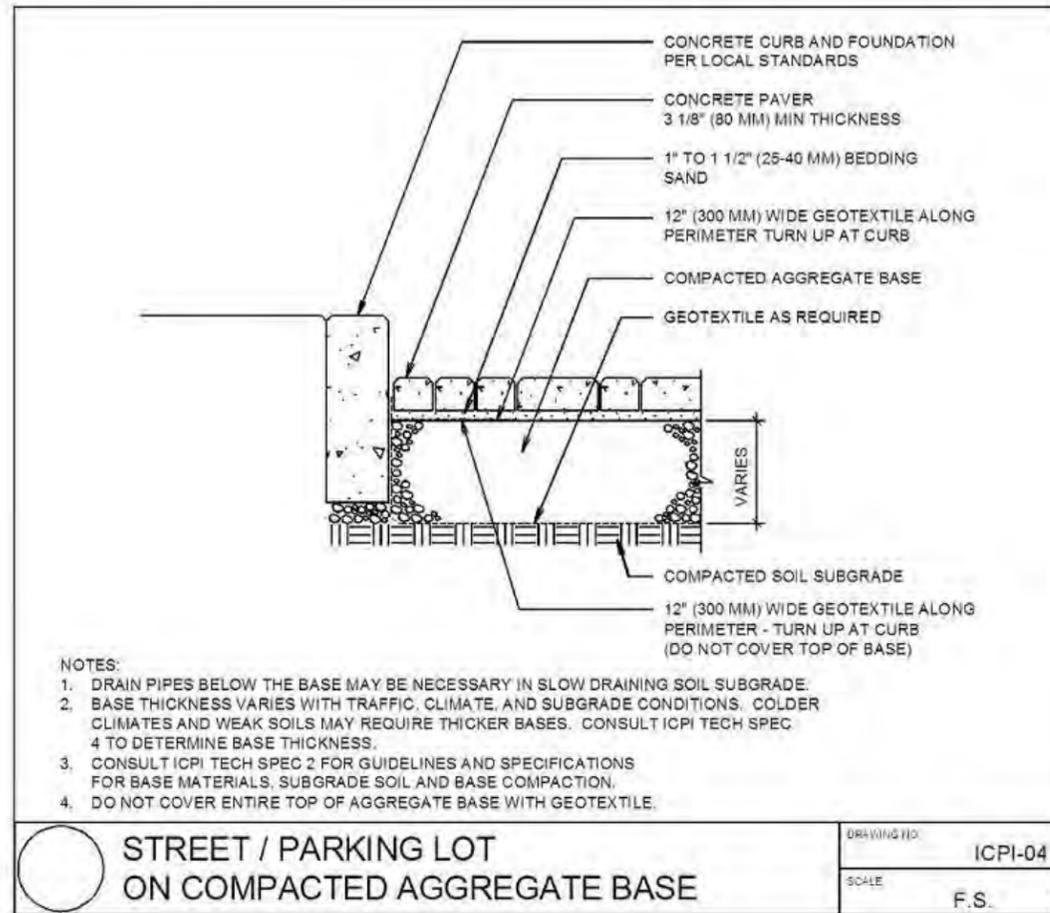


OLSEN PAVINGSTONE
ANTIQUE COBBLE (FLAT TOP) PAVERS

80 MM THICKNESS
RECTANGLES ONLY 8.25" X 5.5" SIZE
HERRINGBONE PATTERN
SET ON COMPACTED AGGREGATE BASE
NO GROUT FOR PERMEABILITY



COLOR MIX:
20% LIMESTONE
30% CHARCOAL
50% BROWN



STANDARD GREY CONCRETE PAVING
FINISH TO BE GRACE TOP CAST ACID ETCH 03

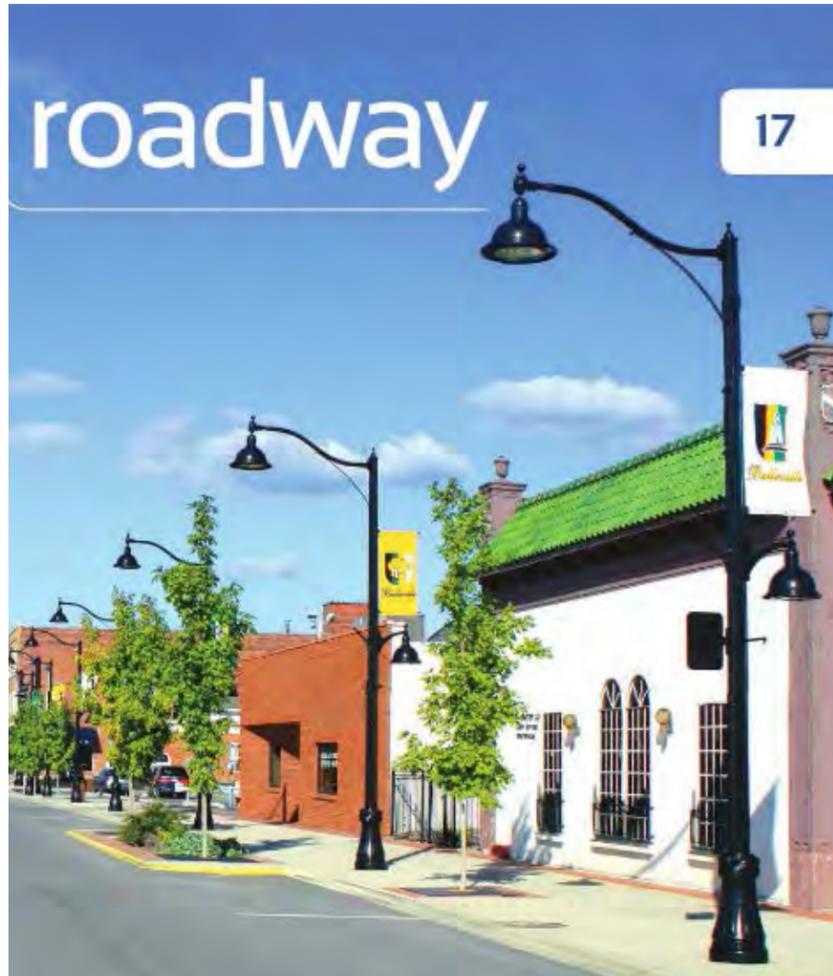
(SEE PLANS FOR SCORE LINES & EXPANSION JOINT PATTERNS)

GRACE

INTERLOCKING PERVIOUS PAVERS & CONCRETE PAVING

OLD TOWN PLACENTIA

City of Placentia, CA



BANNER ARMS
BUILDING SIDE OF POLE



WREATH HOOKS
BOTH SIDES OF POLE



OMEGA
LED LUMINAIRE FAMILY



FLAG MOUNTS
STREET SIDE OF POLE



RECEPTACLES
GFI IN BASE WITH COVER



SPEAKER HUBS
AUDIO CAPABILITY

omega®

Omega® is a timeless transitional design with a style that reminds the viewer of the past, but still takes modern flavor from arm and pole styling. Truly one of the most versatile form factors in the market, Omega® adds value to any street scape or architectural project. Its scalability ranges from ten to thirty feet, and will take design flavor from its surroundings. Applications include roadway, parking lot, and park applications, as well as commercial and residential developments.



roadway

Sternberg Lighting has an impressive collection of roadway pole options characterized by the large scale base sizes. Roadway poles are typically taller than decorative roadway assemblies and therefore need to be more robust in construction. The height of these poles dictates that an anchor plate design be used. This non-decorative element is then dressed up with large scale two-piece base castings that clamshell around the bottom of the pole presenting a decorative appearance.

QUICK FACTS

- Roadway bases are heavy cast aluminum.
- Cast bases are painted inside and out for corrosion protection.
- Heavy wall construction provides long service life.
- Tamper resistant hardware is available for security.
- Cast bases are available in several family sizes to provide the same look throughout a project site.
- Roadway bases can accommodate a variety of pole shaft diameters and present a clean joint line between pole shaft and base casting.



Birmingham Cambell



Birmingham Cambell

STREET LIGHTING

OLD TOWN PLACENTIA

City of Placentia, CA



1531LED OMEGA SERIES

LED

Optics

The luminaire shall be provided with individual, refractor type optics applied to each LED. The luminaire shall provide Type ___ (2, 3, 3R, 4 or 5) light distribution per the IESNA classifications. Testing shall be done in accordance with IESNA LM-79.

Electronic Drivers

The LED driver shall be U.L. Recognized. It shall be securely mounted inside the fixture, for optimized performance and longevity. It shall be supplied with a quick-disconnect electrical connector on the power supply, providing easy power connections and fixture installation. It shall have overload as well as short circuit protection, and have a DC voltage output, constant current design, 50/60HZ. It shall be supplied with line-ground, line-neutral and neutral-ground electrical surge protection in accordance with IEEE/ANSI C62.41.2 guidelines. It shall be dimmable using a 0-10v signal.

For sources over 50w: The driver shall have a minimum efficiency of 90%. The driver shall be rated at full load with THD<20% and a power factor of greater than 0.90. The driver shall contain over-heat protection which reduces output to less than half rating if the case temperature reaches 85°C.

For sources under 50w: The driver shall have a minimum efficiency of 88%.

Photocontrols

Button Style: On a single assembly the photocontrol shall be mounted on the fixture and pre-wired to driver. On multiple head assembly's the photocontrol shall be mounted in the pole shaft on an access plate. The electronic button type photocontrol is instant on with a 5-10 second turn off, and shall turn on at 1.5 footcandles with a turn-off at 2-3 footcandles. Photocontrol is 120-277 volt and warranted for 6 years.

Twist-Lock Style: The photocontrol shall be mounted externally on the fixture and pre-wired to driver. The twist lock type photocontrol is instant on with a 3-6 second turn off, and shall turn on at 1.5 footcandles with a turn-off at 2-3 footcandles. Photocontrol is 120-277 volt and warranted for 6 years.

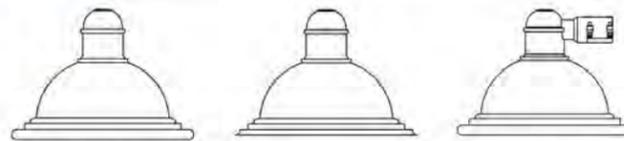
Warranty

Seven-year limited warranty. See product and finish warranty guide for details.

Finish

Refer to website for details.

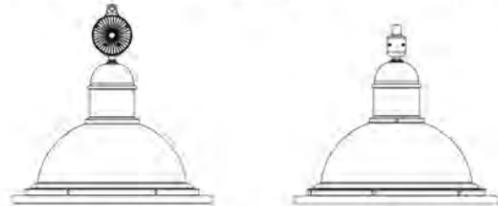
Fixtures



1531LED-R
31"W x 21-1/2"H

1531LED-F
31"W x 21-1/2"H

1531LEDSM-R
31"W x 21-1/2"H



CAT-1531LED-R
31"W x 21-1/2"H

1531LED-EZ
31"W x 21-1/2"H

Options

R-PE



Twist-lock receptacle with photocontrol

PEC



Electronic button photocontrol

FHD Fuse holder



SternbergLighting
ESTABLISHED 1923 / EMPLOYEE OWNED

800-821-3376
555 Lawrence Ave., Roselle, IL 60172
info@sternberglighting.com
www.sternberglighting.com

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SternbergLighting
ESTABLISHED 1923 / EMPLOYEE OWNED

LED Ornamental Bollards

12-653A

7701LED BIRMINGHAM LIGHTED BOLLARD SPECIFICATIONS

GENERAL

The Model 7701LED decorative lighted bollard shall be cast aluminum, one-piece construction. The 16" diameter cast aluminum base shall be constructed with a 7 1/4" diameter straight fluted cast aluminum shaft. The Model shall be Sternberg Lighting #7701LED lighted bollard or #7701LED-QR quick release lighted bollard. The bollard shall be U.L. or E.T.L. listed in U.S. and Canada.*

CONSTRUCTION

The base shall be designed with a tall and smoothly curved pedestal bottom section and transition to a tall twelve flute vertical base section and be made of heavy wall, alloy cast aluminum. It shall have a 1" thick floor cast as an integral part of the base. The bollard cap shall be welded in place. The overall height of the bollard shall be 51".

ELECTRICAL / LED

The optical assembly shall be constructed of twelve (12) fluted openings with a white acrylic lens. The LED light source shall have an IP65 rated assembly. The assembly shall consist of highly efficient, four (4) [optional three (3)] sided extruded aluminum heat sink, four (4) [optional three (3)] LED boards with 6 each high brightness LEDs and a sealed acrylic tube. The bollard shall be supplied with electrical surge protection in accord with IEEE/ANSI C62.41.2 and shall be U.L. or E.T.L. Listed in U.S. and Canada. The LED lighted bollard shall have an L70 expected life of 70,000 hours with the LED life ratings determined in accordance with IESNA LM-80. The electronic LED driver shall be a U.L. Recognized, constant current design with THD<20%.

QUICK RELEASE MOUNTING (Optional)

The Model 7701LED-QR shall have a quick release option which allows quick removal of the bollard for convenience or emergency access. The burial portion shall be made of aluminum extrusion and shall have a keyway and flexible connection system for securing to bollard. The bollard shall have a mated extension and anti-rotation key and padlock slot. The quick release system shall allow for a flush pavement installation after temporary bollard removal.

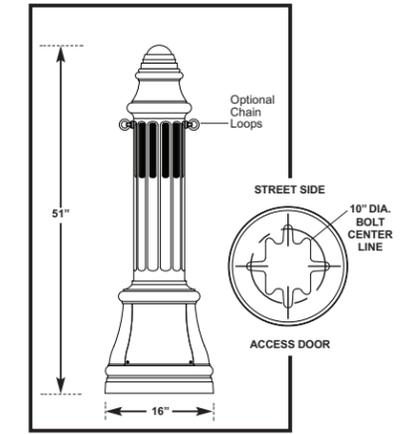
FINISH

Prior to coating, each assembly shall be chemically cleaned and etched in a 5-stage washing system which includes alkaline cleaning, rinsing, phosphoric etching, reverse osmosis water rinsing, and non-chrome sealing to ensure corrosion resistance and excellent adhesion for the finish coating. The finish coating shall be electrostatically applied semi-gloss, super durable polyester powder baked at 400 degrees for a durable and superior, color retentive finish. Our optional antique Verde Green finish and Swedish Iron finish are hand brushed using a 3-step process. The total assembly shall be wrapped in shockproof wrapping or fully enclosed in corrugated cartons.

INSTALLATION

Four, hot dipped galvanized "L" type anchor bolts shall be provided with the post for non-quick release bollard anchorage. Quick release anchorage requires no anchor bolts. A door shall be provided for wiring and anchor bolt access. It shall be secured with tamper-proof, stainless steel hardware. Bollard will be provided with a grounding stud mounted on the base floor opposite the access door.

WARRANTY Seven (7) year limited warranty. See product and finish warranty guide for details.



See installation template for exact door position.

BUILDING A PART NUMBER

PART NUMBER SELECTIONS

BOLLARD MODEL	OPTIONAL CHAIN LOOP	LIGHT SOURCE	DRIVER	OPTIONS	FINISH	MODELS	LIGHT SOURCES	STANDARD FINISHES*	RT Rust	
7701LED	CL2	3S45TLCA	ML	FHD	VG	• 7701LED • 7701LED-QR DRIVERS • ML-120-277V • MDL - Dimming 120-277V OPTIONAL CHAIN LOOP • CL1 Chain Loop • CL2 Chain Loops 180°	• 4S60TLCA 30W 6000K Type V • 4S45TLCA 30W 4500K Type V • 4S35TLCA 30W 3500K Type V • 3S60TLCA 23W 6000K Type V • 3S45TLCA 23W 4500K Type V • 3S35TLCA 23W 3500K Type V	• BKT Black Textured • WHT White Textured • PGT Park Green Textured • ABZT Architectural Medium Bronze Textured • DBT Dark Bronze Textured *Smooth Finishes are available upon request CUSTOM FINISHES • OI Old Iron	• WBR Weathered Brown • CD Cedar • WBK Weathered Black • TT Two Tone STERNBERG SELECT FINISHES • VG Verde Green • SI Swedish Iron • OWGT Old World Gray Textured	



* NOTES
U.L. listed components only. Bollard is not U.L. or E.T.L. listed with a photocell.

TO ORDER CHAIN: Specify total length of chain required for project.

• PEC1¹ Photocell-Bimetal 120 Volt
 • PEC2¹ Photocell-Bimetal 208-277 Volt
 • PEC1-E¹ Photocell-Electronic 120 Volt
 • PEC2-E¹ Photocell-Electronic 208-277 Volt
 • LP Low Power / Reduced Output 11W (3S TLCA only)

¹GFI not available with QR option

SternbergLighting
ESTABLISHED 1923 / EMPLOYEE OWNED

555 Lawrence Ave. Roselle, IL 60172 • 847-588-3400 • Fax 847-588-3440
www.sternberglighting.com Email: info@sternberglighting.com 7-15

STREET LIGHTING
OLD TOWN PLACENTIA
City of Placentia, CA





REPRESENTATIVE ABOVE INSTALLATION AT PECK ROAD & VALLEY BLVD



TRAFFIC SIGNAL ELEMENTS TO INCLUDE:

- POLES
- MAST ARMS
- PUSH BUTTON
- SIGN
- PEDESTRIAN COUNT DOWN MODULE
- LUMINAIRE
- COLOR TO BE BLACK

REFLECTORIZED STREET NAME SIGN

TRAFFIC SIGNAL POLES
OLD TOWN PLACENTIA
 City of Placentia, CA

valmont
STRUCTURES

ROUND TAPERED STEEL
Fluted
FL30

Job Name: _____	Client Name: _____
Job Location - City: _____ State: _____	Created By: _____ Date: _____
Product: FL30 _____	Quote: _____ Customer Approval: _____ Date: _____

SPECIFICATIONS

Pole - The pole shaft conforms to ASTM A595 Grade A or A572 Grade 55 with a constant linear taper of 0.14 in/ft.

Fluting Process - The tapered shafts are cold rolled over a precision hardened steel mandrel to form the desired flute shape. The flutes are uniform and equally spaced throughout the process. The termination of the flutes is no greater than 1.50" radii transition into the round section of the shaft. The results of this fluting process are flutes with architecturally pleasing radiused terminations and crisp uniform flutes.

Linear Flute Layout - Flutes start and stop to ensure decorative nut covers and anchor base connection fits a smooth round diameter. Flute start and stop locations should be specified.

Finial - A decorative spear finial with hardware is provided.

Luminaire Arm - Luminaire arms are made from 2" schedule 40 pipe (2.38" OD) with a minimum yield strength of 36,000 psi. Double luminaire arms are oriented 180° apart. For other orientations and scroll options please consult factory.

Luminaire Arm Attachment - Connection allows arm to be erected and held in place by gravity and secured by a single bolt.

Handhole - A covered handhole and grounding provision with hardware is provided.

Nut Covers - Decorative anchor bolt nut covers with hardware are provided.

Base Cover - Optional decorative base covers available as special order.

Anchor Base - The anchor base (base plate) conforms to ASTM A36.

Anchor Bolts - Anchor bolts conform to ASTM F1554 Grade 55 and are provided with two hex nuts and two flat washers. Bolts have an "L" bend on one end and are galvanized or zinc-plated carbon steel or stainless steel.

Hardware - All structural fasteners are galvanized high strength carbon steel. All non-structural fasteners are galvanized or zinc-plated carbon steel or stainless steel.

Finish - Standard finishes are galvanized, prime painted or any of Valmont's V-PRO™ Protection Systems. Additional finish options available upon request.

Design Criteria - Please reference Design Criteria Specification for appropriate design conditions.

VALMONT INDUSTRIES, INC. 7002 N. 288TH STREET, PO BOX 358 - VALLEY, NE 68064 USA 800.825.6668 VALMONTSTRUCTURES.COM

Pole Assembly - Steel

RTS / Fluted

FL30

valmont
STRUCTURES

WASHINGTON
Fluted Tapered/Non-Tapered
Aluminum

Job Name: _____	Client Name: _____
Job Location - City: _____ State: _____	Created By: _____ Date: _____
Product: WA17ASE _____	Quote: _____ Customer Approval: _____ Date: _____

SPECIFICATIONS

Pole - The pole shaft is either spun or extruded from 6063 alloy aluminum.

Pole Top - A pole top plate and tenon are provided for top mount luminaire and/or bracket. A removable finial is available for poles receiving drilling patterns for side-mount luminaire arm assemblies.

Structural Base - The structural base is cast from 356 alloy aluminum and provided with a 4.00" wide X 5.63" tall access door. A grounding provision with hardware is provided. The pole is inserted and welded into the structural base casting. The completed assembly is heat-treated to a T6 temper after the structural welding is completed.

Anchor Bolts - Anchor bolts conform to ASTM F1554 Grade 55 and are provided with two hex nuts and two flat washers. Bolts have an "L" bend on one end and are galvanized a minimum of 12" on the threaded end.

Finish - The standard finish for the pole assembly and components is polyester powder applied coating in accordance with Valmont's Specifications. Additional finish options available upon request.

Design Criteria - Please reference Design Criteria Specification for appropriate design conditions.

VALMONT INDUSTRIES, INC. 7002 N. 288TH STREET, PO BOX 358 - VALLEY, NE 68064 USA 800.825.6668 VALMONTSTRUCTURES.COM

Posts - Aluminum

Washington

Fluted Tapered / Non-Tapered

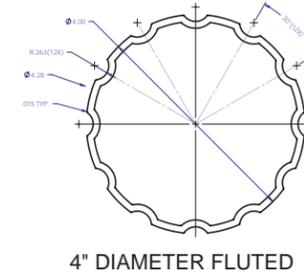


14 FT
13 FT
12 FT
11 FT
10 FT
9 FT
8 FT
7 FT
6 FT
5 FT
4 FT
3 FT
2 FT
1 FT



Part Name: Acorn Finial

Identification Number: FN-0103
 Height: 6½"
 Width: 4¼"
 Application: Fits 3" diameter post
 Finish: Powder coat finish (standard gloss black)
 Hardware Requirements: 3) 1/4-20 x 1/4" stainless set screws



Specifications

Material:
Extruded Aluminum

Alloy:
6063

Tolerance:
+/-0.020"

Wall Thickness:
0.125" +/-0.010"

Symmetry:
Shape is symmetrical about center lines

Estimated Weight:
3" Diameter: 1.34 lbs/ft
4" Diameter: 1.91 lbs/ft

Finish:
Black Powder Coat (Custom and Mill Finish Available)

Warranty:
1 Year Limited Manufacturer Warranty From Date of Purchase

Part Name: Classic Base

Identification Number: BS-04D
 Height: 35"
 Width/Footprint: 11"
 Application: Fits 4" diameter post
 Weight: - lb.
 Finish: Powder coat finish (standard gloss black)
 Hardware Requirements: 3) 1/4-20 x 1/4" stainless set screws



COLOR
 POLES, BRACKETS, SCROLLS, ARMS, AND BASES
 TO BE POWDER COATED GLOSS BLACK



PRIMARY CANOPY STREET TREE

Ulmus parvifolia 'Drake'
Drake Chinese Elm

35'-45' Height
35'-50' Spread
Moderate Water Use



MEDIUM EVERGREEN SHADE TREE

Geijera parviflora
Australian Willow

30' Height
20' Spread
Low Water Use



COLORFUL FLOWERING ACCENT TREE

Lagerstroemia indica
Crape Myrtle

25' Height
15' Spread
Moderate Water Use

SHRUBS, GROUNDCOVERS & ACCENTS

The plant palette was developed using tools such as UC Davis' Water Use Classification of Landscape Species (*WUCOLS IV) <http://ucanr.edu/sites/WUCOLS/> to select plants that have low or very low water use for the City of Placentia bioregion and are durable, longer-lived, and require low levels of maintenance. For example, Callistemon, Hesperaloe, Lantana, Myoporum, and Rosmarinus all have low or very low water use and are evergreen, durable, commercially available, low-maintenance shrub species which are appropriate for street medians and right-of-ways. Alternatively, Agave, Dasyliirion, Sedum, Senecio, and Yucca hybrids are low and very low water use succulents and accents which can create bold striking landscapes when paired with cobble and rock blankets.

Efficient and minimal water use for public landscapes is now a State-wide requirement due to multiple years of low annual rainfall levels in California. Successful landscape solutions are those which combine low and very low water use plants with non-living landscape materials like stabilized decomposed granite, rock and cobble blanket (both loose and stabilized with concrete), wood mulch, concrete pavers, permeable concrete, and other innovative sustainable materials.

This palette was specifically selected for medians and right-of-ways to create water-efficient landscape solutions which provide visual interest for both pedestrian and motorists.

<i>Botanical Name</i>	<i>Common Name</i>	<i>Water Use*</i>	<i>Spacing</i>
SHRUBS			
Bougainvillea 'Rosenka'	Dwarf Bougainvillea	Low	36"
Callistemon 'Little John'	Dwarf Bottlebrush	Low	24"
Leucophyllum frutescens 'Green Cloud'	Green Cloud Texas Ranger	Low	30"
Nandina domestica 'Nana'	Dwarf Heavenly Bamboo	Low	12"
Punica granatum 'Nana'	Dwarf Pomegranate	Low	24"
Rhaphiolepis indica 'Ballerina'	Dwarf Indian Hawthorne	Low	30"
Teucrium chamaedrys	Germander	Low	18"
Westringia frutescens	Coast Rosemary	Low	36"
Xylosma congestum	Shiny Xylosma	Low	48"
GROUNDCOVERS			
Arctostaphylos 'Pacific Mist'	Pacific Mist Manzanita	Low	36"
Carissa macrocarpa 'Tomlinson'	Dwarf Natal Plum	Low	36"
Ceanothus 'Diamond Heights'	Diamond Heights Ceanothus	Low	30"
Cotoneaster dammeri 'Lowfast'	Bearberry Cotoneaster	Low	48"
Gazania rigens 'Sun Gold'	Sun Gold Gazania	Low	18"
Myoporum parvifolium	Myoporum	Low	48"
Rosmarinus o. 'Huntington Carpet'	Rosemary	Low	42"
SUCCULENT ACCENTS			
Agave desmettiana	Variegated Smooth Agave	Very Low	42"
Agave amer. v. medio-picta 'Alba'	White-Striped Century Plant	Very Low	48"
Agave 'Blue Glow'	Blue Glow Agave	Very Low	42"
Aloe 'Blue Elf'	Blue Elf Aloe	Low	12"
Crassula argentea 'Crosby's Compact'	Compact Jade	Low	30"
Dasyliirion wheeleri	Spoon Yucca	Very Low	30"
Hesperaloe parviflora	Red/Yellow Yucca	Very Low	18"
Portulacaria afra 'Prostrate Form'	Dwarf Elephant's Food	Very Low	36"
Sedum 'Verde Yellow'	Verde Yellow Sedum	Low	24"
Sedum adolphii 'Firestorm'	Golden Sedum	Low	18"
Senecio mandraliscae	Blue Dicks	Low	18"
Senecio serpens	Blue Chalksticks	Low	12"
Yucca filamentosa 'Bright Star'	Adam's Needle	Very Low	24"

*Water use information was determined by consulting UC Davis' Water Use Classification of Landscape Species (WUCOLS IV) <http://ucanr.edu/sites/WUCOLS/>

SHRUBS



Bougainvillea 'Rosenka'
Dwarf Bougainvillea



Callistemon 'Little John'
Dwarf Bottlebrush



Leucophyllum frutescens
Texas Ranger



Nandina domestica 'Nana'
Dwarf Heavenly Bamboo



Punica granatum 'Nana'
Dwarf Pomegranate



Raphiolepis i. 'Ballerina'
Dwarf Indian Hawthorne



Teucrium chamaedrys
Germander



Westringia frutescens
Coast Rosemary



Xylosma congestum
Shiny Xylosma

GROUNDCOVERS



Arctostaphylos 'Pacific Mist'
Pacific Mist Manzanita



Ceanothus 'Diamond Heights'
Diamond Heights Lilac



Gazania lin. 'Colorado Gold'
Sun Gold Gazania



Myoporum parvifolium
Myoporum



Rosmarinus off. 'Hunt. Carpet'
Trailing Rosemary

SUCCULENTS



Agave desmettiana
Variegated Smooth Agave



Aloe rudikoppe 'Little Gem'
Little Gem Aloe



Dasylirion wheeleri
Spoon Yucca



Sedum adolphii 'Firestorm'
Golden Sedum



Senecio serpens
Punk Pickle

SHRUB PALETTE IMAGERY
OLD TOWN PLACENTIA
Placentia, CA

EXHIBIT E

Old Town District Development Standards adding Chapter 23.112 to the Placentia Municipal Code and amending Chapter 23.08.010 of the Placentia Municipal Code



DRAFT

June 13, 2017

Old Town Placentia Revitalization Development Standards

*Prepared by High Peaks Planning
for the City of Placentia*



DEVELOPMENT STANDARDS

Chapter 23.08 DISTRICTS ESTABLISHED

23.08.010 Established.

Zoning districts are established as follows:

R-A	Residential agricultural district
R-1	Single-family residential district
R-2	Low-medium density multiple-family district
R-G	Medium density multiple-family district
R-3	High density multiple-family district
RPC	Residential planned community district
T-C	Town center district
P-V	Combining parkway vista district
C-0	Commercial office district
C-1	Neighborhood commercial district
C-2	Community commercial district
SF-C	Santa Fe commercial district
C-M	Commercial manufacturing district
M	Manufacturing district
PMD	Combining planned manufacturing district
MHP	Combining mobile home park district
O	Combining oil district
O-1	Combining oil district
PUD	Planned unit development district
SP-1	Specific Plan 1 district
SP-2	Specific Plan 2 district
SP-3	Specific Plan 3 district
SP-4	Specific Plan 4 district
SP-5	Specific Plan 5 district
SP-6	Specific Plan 6 district
SP-7	Specific Plan 7 district
SP-8	Specific Plan 8 district
TOD	TOD Packing House District
<u>OT</u>	<u>Old Town Placentia</u>

OLD TOWN PLACENTIA REVITALIZATION PLAN

DEVELOPMENT STANDARDS

23.112.010 Purpose and Intent

The following provides detailed regulations for development of land uses within Old Town Placentia (Old Town). The purpose of these standards is to facilitate the enhancement of the City's historic core, characterized by its small-scaled village atmosphere, shopfront architecture, mix of uses and inviting streetscapes. Specifically, these regulations are aimed at accomplishing the following objectives, complimenting the objectives of the TOD Packing House District to the south:

- A. Meet the goals, objectives and follow the recommendations for Old Town as detailed in the Old Town Revitalization Plan;
- B. Preserve and enhance the distinguishable "village" look and feel of Old Town Placentia;
- C. Preserve the original Old Town grid pattern comprised of short, walkable blocks;
- D. Preserve the diverse mix of land use types while attracting a more diversified commercial base that creates a shopping destination and contributes to a more pedestrian-friendly environment;
- E. Encourage infill development that contributes to creating a compact walkable environment important to a vital town center;
- F. Support businesses and attract new investment through enhanced safety and security measures, development incentives, a high level of amenities and adaptive reuse standards;
- G. Create fine-grained detail in architectural and urban form that provides interest and complexity at the level of the pedestrian and bicyclist;
- H. Enhance public safety by improving street lighting, public services, streetscapes and alleys, and police surveillance;
- I. Support multimodal transportation that improves ease of access to a variety of transportation methods such as walking, cycling and public transit;
- J. Create a sustainable environment through standards that result in "green" buildings and infrastructure, as well as a safe, healthy and attractive environment;
- K. Preserve and highlight historic features that make Old Town Placentia a unique destination;
- L. Provide for public gathering spaces and small passive recreation places;
- M. Improve connections between Old Town and the region through increased transit ridership and the provision of hubs for various transportation ;
- N. Maintain an adequate level of parking and access for automobiles;
- O. Continue to brand Old Town Placentia through wayfinding, welcome signs, banners, public art, creative public spaces, a focal point (i.e., gazebo), etc.;
- P. Create a physical and visual connection with the TOD Packing House District to the south through interconnected plazas on both sides of the rail line; and
- Q. Ensure high quality architecture (360 degree design) that embraces the character of Old Town and its architecture and urban design.

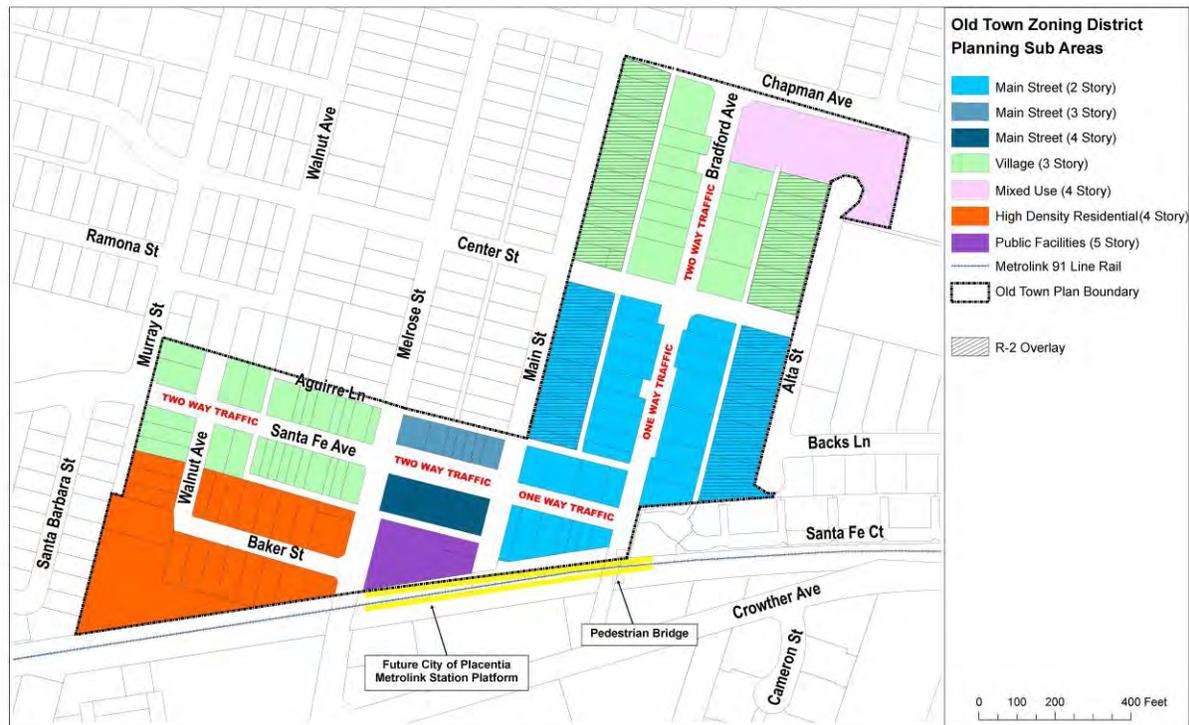
23.112.020 Applicability and General Provisions

Old Town Placentia shall apply to lands delineated as such on the City's official zoning map and shown in Figure 1. All land uses and development within Old Town shall be located and developed in accordance with the following provisions. The standards of Old Town shall not apply to development for which approvals were granted prior to the adoption of these regulations and which entitlements are still valid and for development which has current, valid building permits.

23.112.030 Planning Subareas and Overlay Zone

Old Town Placentia is divided into five subareas and one overlay zone described below:

- Main Street (MS) (this subarea has 2,3 and 4 story areas)
- Village (V)
- Mixed-Use (MU)
- High-Density Residential (HDR)
- Public Facilities (PF)
- R-2 Overlay Zone



Old Town Zoning Map

23.112.040 Land Use and Permit Requirements

This section identifies the land use types allowed by the City in Old Town Placentia.

- A. Allowable Land Uses.** A parcel or building within Old Town shall be occupied by only the land uses allowed by Table 1. Each land use in the table is defined in the glossary of this Ordinance or in the Placentia Municipal Code (PMC) (Definitions, Chapter 23.04).
1. **Multiple Uses.** Any one or more land use identified by Table 1 as being allowable within Old Town may be established on any parcel, subject to the planning permit requirement listed in the table, and in compliance with all applicable requirements of this Code.
 2. **Unlisted Uses.** The Development Services Director may determine an unlisted use is similar to another allowable permitted or conditionally permitted use and if all of the following findings can be made:

- a. The use is no greater in density or intensity than other uses allowed, or conditionally allowed in the Planning Subarea;
- b. The use is compatible with permitted or conditionally permitted uses in the Planning Subarea;
- c. The use will meet the purpose of the Planning Subarea;
- d. The use is consistent with the goals and policies of the General Plan; and
- e. The use will not be detrimental to the public health, safety or welfare.

Applicants may appeal this decision using the Use Conformity Determination process, outlined in Section 23.39.035 of the PMC.

- 3. **Residential Uses.** In the R-2 Overlay area as shown on the Old Town Zoning District Planning Sub Area Map, existing and residential are permitted pursuant to Chapter 23.15 of the Zoning Code.

B. Permit Requirements. Table 1 provides for land uses that are:

- 1. **Permitted.** These uses are permitted subject to compliance with all applicable provisions of this Chapter and may require a Development Plan Review or Site Plan Review in compliance with Chapter 23.75 of the PMC. These uses are shown as “P” uses in Table 1. All new construction projects as defined in this Chapter and in this Zone must be reviewed by the Planning and Development Ad Hoc Committee.
- 2. **Conditionally Permitted Uses.** These uses are allowed subject to the approval of a Use Permit and require a public hearing before the Planning Commission in compliance with Chapter 23.87 of the PMC. These uses are shown as a “UP” in Table 1. All proposed projects in this Zone must be reviewed by the Planning and Development Ad Hoc Committee.
- 3. **Not Permitted.** These uses are not permitted, and shown as “NP” in Table 1. A land use that is not listed in Table 1 is not allowed within the District, except as otherwise provided in Section 23.11.030 (A.3). Uses that are expressly listed as not permitted are prohibited.

C. Standards for Specific Land Uses. Where the last column in Table 1 (Specific Use Regulations) includes a section number, the regulations in the referenced section of this chapter and/or the PMC apply to the use. Provisions in other sections of this chapter may also apply.

23.112.040 Table 1: Allowed Land Uses and Permit Requirements

PERMIT REQUIREMENT	PLANNING SUBAREAS					NOTES AND SPECIAL USE REGULATIONS
	Main Street	Village	Mixed-Use (1)	High-Density Residential	Public Facility	
P – permitted UP – use permit NP – not permitted						
D. Recreation, Education, Public Assembly Uses						
1. Commercial recreation facility, indoor	NP	NP	NP	NP	NP	
2. Community Recreation Facility	NP	NP	NP	NP	NP	
3. Conference/Convention Facility	NP	NP	NP	NP	NP	

PERMIT REQUIREMENT P – permitted UP – use permit NP – not permitted	PLANNING SUBAREAS					NOTES AND SPECIAL USE REGULATIONS
	Main Street	Village	Mixed-Use (1)	High-Density Residential	Public Facility	
4. Health/Fitness Facility, including stand alone or roving fitness classes	NP	UP	UP	NP	NP	
5. Library, Museum	NP	UP	NP	NP	NP	
6. Public Park, Playground or Plaza	P	P	P	P	P	
7. School – specialized Education, training	NP	NP	NP	NP	NP	
8. Studio – art, dance, martial arts, music, photography, cooking, instructional, fitness (such as yoga, Pilates, spin, etc)	UP	UP	UP	NP	NP	Not to exceed 1,500 square feet
9. Theatre (live performing arts, live music)	UP	UP	UP	NP	NP	Movie cinemas only permitted in Mixed Use.
E. Residential Uses						
1. Emergency/ Transitional shelter	NP	NP	NP	NP	NP	
2. Home Occupation	NP	P	P	P	NP	Not permitted in ground floor units, must comply with Home Occupation provisions of PMC
3. Live Work	NP	UP	NP	NP	NP	
4. Multi-Family Residential, New Construction Only, Maximum of 29	UP (only permitted above the	UP (upper floors only)	UP (upper floors only)	NP	NP	See definition of “new construction.”

PERMIT REQUIREMENT P – permitted UP – use permit NP – not permitted	PLANNING SUBAREAS					NOTES AND SPECIAL USE REGULATIONS
	Main Street	Village	Mixed-Use (1)	High-Density Residential	Public Facility	
du/ac	second floor)					
5. Multi-Family Residential, New Construction Only, Minimum of 30 du/ac	NP	UP (upper floors only)	UP (upper floors only)	P (By Right)	NP	By Right Developments must meet: <ul style="list-style-type: none"> • The Purpose and Intent of this Chapter as well as all standards. • All Zoning Code Regulations. • All City Policies. • Requires review by the Planning & Development Ad Hoc Committee • All existing and future design guidelines.
6. Assisted Living Facility	NP	NP	NP	NP	NP	
7. Single Family or Duplex Residential, New	NP	NP	NP	NP	NP	Only permitted in the R-2 Overlay zone. Pursuant to Chapter 23.15 of the Municipal Code applies.
8. Existing Residential uses fronting on Main and Alta Streets	P	P	NP	NP	NP	Only permitted in the R-2 Overlay zone. Pursuant to Chapter 23.15 of the Municipal Code applies.
9. Mixed Use Development	UP (only permitted)	UP	UP	NP	NP	

PERMIT REQUIREMENT P – permitted UP – use permit NP – not permitted	PLANNING SUBAREAS					NOTES AND SPECIAL USE REGULATIONS
	Main Street	Village	Mixed-Use (1)	High-Density Residential	Public Facility	
	above the second floor)					
F. Retail/Commercial Uses (2)						
1. New Ground Floor Residential and Office Uses in R-2 Overlay zone.	UP	UP	NA	NA	NA	50% of the lot depth, measured from Alta or Main, must be either residential or office along Alta and Main. The rear 50%, facing the alley, may be any other use permitted in the subarea.
2. Accessory Retail or services	P	P	P	NP	NP	Only permitted when primary commercial use is established. Must be incorporated into mixed-use; cannot stand alone.
3. Adult Entertainment Facility or Business	NP	NP	NP	NP	NP	Pursuant to PMC Chapter 23.89
4. Liquor Stores/Alcoholic beverage sales (not associated with bar, brewery, distillery, restaurant, or neighborhood market or grocery)	NP	NP	NP	NP	NP	
5. Antique or collectible store	P*	P*	P	NP	NP	
6. Artisan/Handcraft Shop	P*	P*	P	NP	NP	
7. Auto repair, auto	NP	NP	NP	NP	NP	

PERMIT REQUIREMENT P – permitted UP – use permit NP – not permitted	PLANNING SUBAREAS					NOTES AND SPECIAL USE REGULATIONS
	Main Street	Village	Mixed-Use (1)	High-Density Residential	Public Facility	
sales or auto parts sales						
8. Bar, tavern, brewery, distillery, tasting rooms, wine cellar	UP	UP	UP	NP	NP	
9. Drive-through (any uses)	NP	NP	NP	NP	NP	
10. General retail – less than 5,000 sf	P*	P*	P	NP	NP	
11. General retail –5,000 sf to 20,000 sf	UP*	UP*	UP	NP	NP	
12. General retail – more than 20,000 sf (max 60,000 sf)	NP	NP	NP	NP	NP	
13. Groceries, specialty foods – 10,000 sf or less	P*	P*	P	NP	NP	With alcohol sales, a use permit is required.
14. Groceries, specialty foods – more than 10,000 sf	UP*	UP*	UP	NP	NP	
15. Medical Marijuana Facilities	NP	NP	NP	NP	NP	Pursuant to PMC Chapter 23.46
16. Neighborhood Market (without alcohol beverage sales) – 10,000 sf or less	P*	P*	P	NP	NP	With alcohol sales, a use permit is required.
17. Nightclub	UP	UP	UP	NP	NP	
18. Outdoor Dining	P	P	P	NP	NP	Permitted in public right-of-way with an encroachment permit. Pursuant to

PERMIT REQUIREMENT P – permitted UP – use permit NP – not permitted	PLANNING SUBAREAS					NOTES AND SPECIAL USE REGULATIONS
	Main Street	Village	Mixed-Use (1)	High-Density Residential	Public Facility	
						ABC requirements as well as the City’s Outdoor Dining Permit and Guidelines.
19. Outdoor display and sales	NP	NP	NP	NP	NP	May not encroach into public right-of-way
20. Pet store, pet boarding, animal shelter	UP*	UP*	UP	NP	NP	Sheltering and boarding shall require a Use Permit.
21. Restaurant	P	P	P	NP	NP	
22. Restaurant with alcohol sales	UP	UP	UP	NP	NP	
23. Secondhand/Thrift/Consignment/Pawn/Charity store	UP*	UP*	UP	NP	NP	
24. Service Station	NP	NP	NP	NP	NP	
25. Tobacco Sales, including electronic smoking devices	UP*	UP*	UP	NP	NP	
G. Services – Business, Financial, Medical, Professional						
1. ATM	P	P	P	NP	NP	Must be integrated into building façade. Stand along kiosks not permitted.
2. Bank (2,000 sf or less)	P	P	P	NP	NP	Banks over 2,000 sf are not permitted.

PERMIT REQUIREMENT P – permitted UP – use permit NP – not permitted	PLANNING SUBAREAS					NOTES AND SPECIAL USE REGULATIONS
	Main Street	Village	Mixed-Use (1)	High-Density Residential	Public Facility	
3. Business support services (copying, printing, office supplies, etc.)	P	P	P	NP	NP	
4. Hospital	NP	NP	NP	NP	NP	
5. Medical office	P	P	P	NP	NP	See definition. Only permitted on upper floors, except in R-2 Overlay.
6. Office	P	P	P	NP	P	See definition. Only be permitted on upper floors except in R-2 Overlay.
H. Services – General						
1. Adult daycare	NP	NP	NP	NP	NP	
2. Cigar or Hookah Lounge	UP	UP	UP	NP	NP	
3. Commercial daycare center	UP	UP	UP	NP	NP	
4. Hostess Bars	NP	NP	NP	NP	NP	
5. Laundromat and laundry services	UP	UP	UP	NP	NP	
6. Lodging – B&B	UP	UP	NP	NP	NP	
7. Lodging – Hotel	UP	UP	UP	NP	NP	
8. Massage Establishments	UP	UP	UP	NP	NP	Not permitted on the ground floor in any subarea. Pursuant to PMC

PERMIT REQUIREMENT P – permitted UP – use permit NP – not permitted	PLANNING SUBAREAS					NOTES AND SPECIAL USE REGULATIONS
	Main Street	Village	Mixed-Use (1)	High-Density Residential	Public Facility	
						Section 23.30.030
9. Meeting Halls, Banquet Centers (Stand alone)	NP	NP	NP	NP	NP	
10. Personal services	P	P	P	NP	NP	
11. Public Safety Facility	NP	NP	NP	NP	P	
12. Spa Services (with or without alcohol)	UP	UP	UP	NP	NP	
13. Tattoo Parlors/Body Modification	UP	UP	UP	NP	NP	
I. Transportation, Communications & Infrastructure						
1. Broadcasting or Recording Studio	UP	UP	UP	NP	NP	May only be permitted on upper floors of mixed-use development.
2. Public Parking Structure or surface parking lot	UP	UP	UP	UP	P	
3. Transit Station or terminal	NP	NP	NP	NP	NP	
4. Telecommunication Cell Tower/microcell and small cell sites						Pursuant to PMC Chapter 23.82

- (1) See Section 25.112.050, Development Standards, for frontage requirements regarding building located at 102 S. Bradford Ave.
- (2) Within the R-2 Overlay zone, no retail or commercial uses are permitted on the ground floor.

23.112.050 Development Standards

Table 2 identifies the development standards required for new land uses in new or modified buildings in Old Town Placentia.

23.112.050 Table 2. Development Standards

DEVELOPMENT STANDARD	PLANNING SUBAREAS					NOTES AND ADDITIONAL REQUIREMENTS
	Main Street	Village	Mixed-Use	High-Density Residential	Public Facility	
D. Residential Density						

DEVELOPMENT STANDARD	PLANNING SUBAREAS					NOTES AND ADDITIONAL REQUIREMENTS
	Main Street	Village	Mixed-Use	High-Density Residential	Public Facility	
1. Density (max du/ac)	25	35	55	65	NA	Density shall be calculated using gross lot size, prior to any required right-of-way dedications.
D. Setbacks. Minimum setbacks required and, where noted, maximum setbacks established, except where a frontage type standard allows exceptions or establishes different requirements. Setbacks are measured from property line after any required dedications. Fire Department requirements supersede any setback listed below.						
1. Setback from Railroad Track	NA	NA	NA	0 ft	0ft	10' from rear ROW preferred by BNSF for above ground structures. Applicants should consider access to rear portion of new development.
2. Front Yard Setback (min-max)	0 ft	0-5 ft	5-15 ft	5-15 ft	NA	All setback areas facing a street or alley not devoted to walkways and driveways shall be properly landscaped and maintained in compliance with PMC Chapter XX. In the R-2 Overlay zone, the front yard setback shall be 15'.
3. Front Yard Setback for “through lots” and cross-alley developments in the R-2 Overlay, fronting Alta and Main. See definitions section.	15 ft	15 ft	15ft	NA	NA	
4. Side Yard Setback	0 ft	0 ft	0 ft	0 ft	NA	
5. Side Yard Setback for “through lots” and cross-alley developments in the R-2 Overlay, fronting Alta and Main. See definitions section	0-5 ft*	0-5 ft*	N/A	0 ft	NA	*Less than 5' setback adjoining an R-2 use requires approval from adjoining property owner of said R-2 use.
6. Rear Yard Setback (min-max)	0-15 ft	0-15 ft	0-15 ft	0 ft	NA	
7. Street Side Yard Setback	0 ft	0-5 ft	5* ft	5-15 ft	NA	*Refers to

DEVELOPMENT STANDARD	PLANNING SUBAREAS					NOTES AND ADDITIONAL REQUIREMENTS
	Main Street	Village	Mixed-Use	High-Density Residential	Public Facility	
(min-max)						Chapman Avenue setback
E. Projections and Encroachments						
1. Allowable Setback Projections	<ul style="list-style-type: none"> a. Ground Floor: <ul style="list-style-type: none"> i. Awnings and canopies over windows: 60 inches; ii. Barriers for defining outdoor dining areas such as fences, railings, planter boxes: as needed to encompass outdoor dining area; iii. Sun Shade Structures: 15 feet; iv. Bay Windows: 60 inches (not wider than 10 feet); v. Cornices, belt courses, and similar architectural features: 12 inches; vi. Eaves, roof overhangs: 30 inches; and vii. Uncovered porches, decks and landings (may be covered by arbors or trellises): 10 feet. b. Above Ground Floor – Awnings, galleries, balconies, bay windows: 48 inches c. Art, as determined by the approval of the public art component of the project. d. For signs, see Sign Regulations, 23.112.070. e. All projections must maintain a minimum of 8' vertical clearance from ground. 					
2. Public Right-of-Way Encroachments (all require approval of an encroachment permit)	<ul style="list-style-type: none"> a. Art, as determined by the approval of the public art component of the project. b. For signs, see Sign Regulations, 23.111.070. c. Outdoor dining is allowed by approval of the Director of Development Services and Director of Public Works or their Designees and may require Alcoholic Beverage Control Board (ABC) approval. Approval is pursuant to the Outdoor Dining Permit and Guidelines. 					
F. Heights, Rooftop Amenities, Frontages, Lot Sizes						
1. Building Height	2-Story Area: 2 stories, 30 ft. 3 Story Area: 3 stories, 40 ft. 4-Story Area: 4 stories,	2 min/3 max stories, 40 ft.	3 min/4 max stories, 60 ft.	3 min/4 max stories, 55 ft.	5 stories, 65 ft.	See “Old Town Zoning map” for height subareas. New stories built on top of existing buildings shall be fully integrated into the design of the existing building.

DEVELOPMENT STANDARD	PLANNING SUBAREAS					NOTES AND ADDITIONAL REQUIREMENTS
	Main Street	Village	Mixed-Use	High-Density Residential	Public Facility	
	55 ft					
2. Minimum lot size required for a development at or over 3 stories.	0	8,000 sf	10,000 sf	10,000 sf	0	
3. Ground Floor Height measured from finished floor to ceiling (min)	15 ft	15 ft	15 ft	NA	NA	
4. Frontage Requirements	In order to support the pedestrian environment, building frontages onto streets and open spaces shall be maximized. No visible parking is permitted along frontages. A minimum of 75% of the site frontage shall be occupied as building frontage. A section of blank wall shall not exceed 20 linear feet without being interrupted by a window or entry or other façade treatment.					
5. Rooftop Amenities	In multi-family residential and mixed-use residential projects, rooftop amenities, such as and not limited to, clubhouses, swimming pools, tennis courts, open space areas, fitness centers, are permitted to project 16' above the maximum height limit if integrated into the overall design of the project and the maximum rooftop building coverage is limited to 30% of the rooftop floor area. Rooftop amenities shall be setback from the building edge such that no more than 20% of the rooftop structure can be visible from the primary public right of way at centerline of the street. Rooftop amenities are intended for the use of building residents. Rooftop amenities do not count as a "story" or "floor."					
6. Height Exceptions	Non-habitable building features such as chimneys (up to 6' in width), cupolas, flagpoles, monuments, steeples, roof screens, equipment, and similar structures, covering no more than 10% of the top floor roof area to which they are accessory, may exceed maximum permitted height standards by up to 8'.					
7. Building Orientation	New commercial or mixed use construction must orient the principal façade onto Bradford, Santa Fe or Chapman Avenue or Walnut Avenue. In the Mixed Use Subarea, new building must be oriented toward Bradford and Chapman			NA	NA	
8. Building Façade for building located at 102 S. Bradford Avenue (at Chapman)	The building façade of this building shall be preserved and integrated into the new design of any new development proposed on this site. This includes the façade along Bradford Ave (including the tower and the front section containing the large round window opening) and up to and including the façade (containing the stained glass windows) along Chapman Avenue. The Chapman Avenue façade preservation can end at the point where the planes of the two sections of that façade meet (where the east/west oriented portion of the building meets the north/south oriented portion of the building). The preservation does not necessarily have to include the small vestibule. The proposed architectural design of any new development shall be reviewed by a preservation architect; the selection of such preservation expert shall be approved by the City.					
G. Provision of Common Open Space (Residential Only)						

DEVELOPMENT STANDARD	PLANNING SUBAREAS					NOTES AND ADDITIONAL REQUIREMENTS
	Main Street	Village	Mixed-Use	High-Density Residential	Public Facility	
1. Amount per residential use	1. 50 sf/unit for residential units; 2. 50 sf/unit for 5 or more Live Work Units					
2. Types of Common Open Space Permitted	a. Common open space can be active or passive but must be accessible to all non-residential tenants (i.e. employees and employers) and residential residents. b. Required setbacks may not be counted as common open space. c. Common open space shall be fully landscaped and requires an approved landscape plan. d. Examples may include: courtyards, clubhouses with accompanying landscaped areas, swimming pools, plazas, greens, parks, playgrounds, picnic areas, outdoor seating.					
3. Rooftop Amenities	1. Rooftop amenities are permitted if they provide additional recreational or common open space activities. 2. Amenities, such as and not limited to, clubhouses, swimming pools, tennis courts, open space areas, fitness centers, are permitted to project 16' above the maximum height limit if integrated into the overall design of the project and the maximum rooftop building coverage is limited to 30% of the rooftop floor area. Roof top amenities shall be setback from the building edge such that no more than 20% of the rooftop structure can be visible from the primary public right of way at centerline of the street. Rooftop Amenities are intended for the use of building residents. 3. 50% of the rooftop amenities (structures and active recreation amenities) may count towards the square footage requirement for either private or common open space. 4. Rooftop buildings supporting recreational activities may cover a maximum of 30% of the rooftop area and may project above the maximum height limit by 16 feet.					
4. Courtyard and Common Open Space Requirements	E. Courtyards shall be designed as a central courtyard or as partial, multiple, separated or interconnected courtyards. F. Minimum courtyard dimension shall be 40 feet when the long axis of the courtyard is oriented EW and 30 feet for a NS orientation. The courtyard proportion is 1:1 between its width and height for at least 2/3 of the court's perimeter. As long as total open space requirement is met, this ratio could be modified by up to 10%. G. When there are two or more courtyards, they shall be connected to each other.					

DEVELOPMENT STANDARD	PLANNING SUBAREAS					NOTES AND ADDITIONAL REQUIREMENTS
	Main Street	Village	Mixed-Use	High-Density Residential	Public Facility	
H. Provision of Private Open Space (Residential and Live/Work Only)						
Amount per residential unit	64 sf/unit					6 feet min in any direction; the total of 64 sf must be provided as one private open space area, not broken up into smaller sizes.
I. Shopfronts						
<p>The following are specific guidelines for shopfronts in the Old Town area:</p> <ol style="list-style-type: none"> 12 feet to 16 feet tall, as measured from the adjacent sidewalk. The corresponding storefront(s) opening(s) along the primary frontage shall comprise 65% of the 1st floor wall area facing the street and not have opaque, stucco or reflective glazing. Storefronts may be recessed from the frontage line by up to 10 feet. “Bulkheads,” which are a segment of wall which transitions between the window opening(s) and the adjacent grade, are also encouraged. The bulkhead shall be between 24 inches and 36 inches tall (aluminum storefront or spandrel panel may not substitute for a bulkhead). 						
J. Architectural Styles						
<p>1. Main Street Commercial Multi-story facades are typically divided into base, body and top with the ground floor taller than the shorter upper floor which is finished by a significant parapet. The ground floor has recessed entries and any expansive glass is interrupted by structural columns with transoms to allow light to penetrate deep into the interior. Upper floor windows are smaller with vertical windows directly relating to the ground floor openings.</p>						Style of architecture required in Main Street Subareas
<p>2. Mission Revival Prominent features of the style include red clay tile roofs, use of balconies, smooth-stuccoed exterior walls usually painted white, arched openings, colorful tile work and elaborate landscaping. The buildings frequently have courtyards.</p>						<p>Choices of architecture for the following:</p> <ul style="list-style-type: none"> Village Subarea Mixed Use Subarea High Density Residential Subarea
<p>3. Spanish Colonial</p>						This style of architecture permitted only in High Density

DEVELOPMENT STANDARD	PLANNING SUBAREAS					NOTES AND ADDITIONAL REQUIREMENTS
	Main Street	Village	Mixed-Use	High-Density Residential	Public Facility	
<p>Typical features include a low-pitched roof with little or no eave overhang, a red-tiled roof, perhaps a prominent rounded arch over a door, window or porch, a stucco wall surface, and usually an asymmetrical façade. The features elaborate molded ornament around doors and windows, polychrome tile at ingresses and wrought iron grilles and balconies.</p>						Residential Subarea

23.112.060 Parking Standards

- A. **Parking Standards.** Table 3 identifies the parking requirements for new land uses in new or modified buildings in Old Town Placentia. All subareas shall be subject to the parking requirements listed in Table 3, with the exception of the Main Street Subarea as described in subsection (B) below. Applicants may also elect to pay an in-lieu fee in exchange for meeting the parking requirement, as described in subsection (C) below.
- B. **In-Lieu Parking Fee.** Applicants may also elect to pay an in-lieu fee in exchange for meeting the parking requirements in the Main Street Subarea or in the Village Subarea. Payment made to the City of Placentia in-lieu of providing some or all of the required off-street parking spaces of a project in Old Town shall be allowed by right.
- C. **Main Street Subarea Parking Exception.** Providing parking in this subarea is optional. However, in the 3 and 4 story areas of Main Street Subarea, parking shall be required for third and fourth floor uses, according to this section, or by payment of an in-lieu parking fee.
- D. **Village Subarea Parking Exception.** Parking only required for the second and third stories, or by payment of an in-lieu parking fee. Parking for the first (ground) floor of existing and future buildings shall not be required.
- E. **Un-Listed Uses.** On-site parking requirements for unlisted but similar uses shall be based on the parking requirements of similar uses found in this section and shall be at the discretion of the Development Services Director. The Development Services Director may require the preparation of a parking demand study by a qualified, licensed traffic engineer approved by the City to determine the parking requirement for unlisted but similar uses.
- F. **Parking Reduction.** Applicants may apply for a parking reduction before the Planning Commission for Village, High Density Residential and Mixed Use Subareas up to a maximum reduction of 25% through a parking demand study or shared parking analysis. One such incentive could include a Zip car or shared car plan. Parking demand studies shall be reviewed and approved at a noticed public hearing before the Planning Commission, or by the Director if a non-discretionary review.
- G. **Bicycle Parking-Short Term.**
 - a. Residential: One (1) resident bicycle parking space for every five (5) residential units, or portion thereof.
 - b. Non-Residential: One (1) bicycle parking space for every 5,000 square feet, or portion thereof, of non-residential floor area.

H. Bicycle Parking-Long Term.

- a. Residential: Two (2) bicycle storage units for every five (5) dwelling units for the first 20 units, and one (1) for every five (5) additional units, or portion thereof;
- b. Non-Residential: Any establishment with a parking structure and a minimum of 10,000 square of non-residential space shall provide long-term bicycle parking at a minimum ratio of one (1) space per 20 vehicle spaces.

- I. **Electric Vehicle (EV) Charging Stations.** A minimum 10% of a project’s parking spaces shall provide EV Level 2 charging stations, or 5% of total spaces if installing DC Fast Charging Stations.
- J. **Surface Parking.** Surface parking is permitted as long as it is not visible from a public street and is fully landscaped and screened from public view.
- K. **Parking Structure.** Structured parking is permitted only if integrated into the overall design of the building and “wrapped” with the building, such that the parking area is not visible from any portion of the front, sides, rear or interior courtyards of the project.
- L. **Podium Parking.** Permitted if fully integrated into a development with a “wrapped” parking structure.
- M. **Underground Parking.** Permitted if fully integrated into the design of the development.

23.112.060 Table 3. Parking Standards

PARKING STANDARDS	STANDARDS	APPLICABLE LAND USES
Non-Residential		
A. Retail – spaces per 1,000 sf	2 min./4 max.	Accessory retail, Antique, Artisan, Bank, Business support services, General retail, Grocery, Retail complex, Personal services, Pet store/boarding
B. Eating and Drinking Establishments– spaces per 1,000 sf	5 min./10 max.	Bar/Tavern, Restaurant, Brewery, etc.
C. Outdoor Dining	0	Provided the outdoor dining area does not exceed the interior dining area. Any square footage over the interior dining area shall provide parking pursuant to the eating and drinking establishment requirement above.

PARKING STANDARDS	STANDARDS	APPLICABLE LAND USES
D. Specialty Goods & Foods– spaces per 1,000 sf	2 min./4 max.	
E. Entertainment & Recreation– spaces per 1,000 sf	6 min./10 max.	Health/Fitness, Recreation, Playgrounds, Studios, Theatres cannot be stand alone
F. Commercial Goods– spaces per 1,000 sf	2 min./4 max.	
G. Civic & Cultural, including Libraries, Museums– spaces per 1,000 sf	3 min./no max.	
H. Office Professional – spaces per 1,000 sf	2 min./4 max.	
I. Personal Services	3 min./no max.	
J. Live Work	1 min./1.5 max.	
K. ATM	0	
L. Lodging – Bed & Breakfast	1 per sleeping room	No assembly space permitted.
M. Lodging – Hotel	1 per sleeping room, plus 1 space for every 75 sf of assembly area.	
Residential		
N. Spaces per studio unit	1 min./1 max.	
O. Spaces per 1 bed unit	1 min./1.5 max.	
P. Spaces per 2 bed unit	1.5 min./2 max.	
Q. Spaces per 3 or more bed unit	2 min./ 2.5 max.	
R. Guest spaces per 10 units	2 min./3 max.	
Mixed-Use		
S. Mixed Use	Parking shall meet the requirements for individual land uses. Residential parking shall be separated from non-residential parking and easily accessible through a	Reduced parking may be permitted through a parking study

PARKING STANDARDS	STANDARDS	APPLICABLE LAND USES
	controlled mechanism.	
Other		
T. Transit Station or terminal	As per Director of Development Services in coordination with transportation authority	
U. Telecommunication Facility, Small/micro cell sites	1 space to service facility.	

23.112.070 Sign Regulations Purpose and Intent

These sign regulations are intended to appropriately limit the placement, type, size, and number of signs allowed within Old Town Placentia, and to require the proper maintenance of signs.

The purposes of these limitations and requirements are to:

1. Avoid traffic safety hazards to motorists, bicyclists, and pedestrians, caused by visual distractions and obstructions;
2. Promote the aesthetic and environmental values of the community by providing for signs that do not impair the attractiveness of the City as a place to live, work, and shop;
3. Provide for signs as an effective channel of communication, while ensuring that signs are aesthetically proportioned in relation to adjacent structures and the structures to which they are attached;
4. Safeguard and protect the public health, safety, and general welfare; and
5. Promote the pedestrian scale of the district.

A. Applicability

1. These sign regulations apply to all signs in Old Town Placentia, except that directional/instructional signs and real estate signs shall instead comply with the requirements of the City’s Zoning Code (Sign Regulations).
2. The provisions of this Chapter do not regulate the message content of a sign (sign copy), regardless of whether the message content is commercial or noncommercial.
3. Sign installation within the areas subject to this Code shall require sign permit approval in compliance with the City’s Zoning Code (Sign Regulations), unless exempted from sign permit requirements.
4. Sign Variances and Historic Sign Designation – See the Zoning Code (Variances).
5. Definitions of the specialized terms and phrases used in this section are in the Zoning Code (Sign Regulations).

B. Prohibited Signs

All sign types and sizes not expressly allowed by this Chapter shall be prohibited. Examples of prohibited signs include, but are not limited to the following:

1. Abandoned signs (includes signs on abandoned or closed businesses);
2. Animated and moving signs, including electronic message display signs, and variable intensity, blinking, or flashing signs, or signs that emit a varying intensity of light or color, except time and temperature displays (which are not considered signs);

3. Exposed cabinet/raceways behind channel letters;
4. Internally illuminated cabinet (can) signs;
5. Off-site signs (e.g., billboards, and signs mounted on vehicles);
6. Obscene signs;
7. Pole signs and other freestanding signs over six feet in height;
8. Roof signs;
9. Signs that simulate in color, size, or design, any traffic control sign or signal, or that make use of words, symbols, or characters in a manner that interferes with, misleads, or confuses pedestrian or vehicular traffic;
10. A sign burned, cut, or otherwise marked on or affixed to a rock, tree, or other natural feature;
11. A sign placed within a public right-of-way, except as provided by Table 3 (Sign Standards by Use);
12. A sign painted directly on a building, unless approved as a Creative Sign;
13. Permanent signs that advertise continuous sales, special prices, or include phone numbers are prohibited.
14. Temporary signs, including the following;
 - a. Balloons and other inflatable devices;
 - b. Flags, except official national, state, or local government, institutional or corporate flags, properly displayed; and
 - c. Pennants and streamers, except in conjunction with an athletic event, carnival, circus, or fair.

C. General Requirements for All Signs

1. Sign area and height measurement

The measurement of sign area and height shall occur in compliance with the City's Zoning Code (Sign Regulations).

2. Sign location requirements. Each sign shall be located in compliance with the following requirements, and all other applicable provisions of this Chapter.
 - a. On-premise signs required. Each sign shall be located on the same site as the subject of the sign, except as otherwise allowed by this Chapter.
 - b. Setback requirements. Each sign shall comply with the setback requirements of the applicable Subarea, except for an approved projecting sign, and except for an approved freestanding sign, which shall be set back a minimum of 5 feet from the front and side street property lines.
 - c. Placement on a building. No sign shall be placed so as to interfere with the operation of a door or window. Signs should not be located so that they cover prominent architectural features of the building.
 - d. Signs within a public right-of-way. No sign shall be allowed in the public right-of-way except for the following:
 - i. A projecting sign in compliance with Table 4 (Sign Standards by Use);
 - ii. Public signs erected by or on behalf of a governmental agency to convey public information, identify public property, post legal notices, or direct or regulate pedestrian or vehicular traffic;
 - iii. Bus stop signs installed by a public transit company;
 - iv. Informational signs of a public utility regarding its lines, pipes, poles, or other facilities; or
 - v. Emergency warning signs erected by a governmental agency, a public utility company, or a contractor doing authorized within the public right-of-way.

- e. Any sign installed or placed within the public right-of-way other than in compliance with this Section shall be forfeited to the public and be subject to confiscation.
3. Sign design. The following design criteria shall be used in reviewing the design of individual signs. Substantial conformance with each of the following design criteria shall be required before a sign permit or Building Permit can be approved.
- a. Color
Colors on signs and structural members should be harmonious with one another and relate to the dominant colors of the buildings on the site. Contrasting colors may be utilized if the overall effect of the sign is still compatible with building colors.
 - b. Design and construction
 - i. Except where otherwise permitted by elsewhere in this section, each sign shall be constructed of permanent materials and shall be permanently attached to the ground, a building, or another structure by direct attachment to a rigid wall, frame, or structure.
 - ii. Each permanent sign shall be designed by a professional (e.g., architect, building designer, landscape architect, interior designer, or others whose principal business is the design, manufacture, or sale of signs), or who are capable of producing professional results.
 - iii. Each permanent sign shall be constructed by persons whose principal business is building construction or a related trade including sign manufacturing and installation, or others capable of producing professional results. The intent is to ensure public safety, achieve signs of careful construction, neat and readable copy, and durability, to reduce maintenance costs and prevent dilapidation.
 - c. Materials and structure
 - i. Sign materials (including framing and supports) shall be representative of the type and scale of materials used on the site where the sign is located. Sign materials shall match those used on the buildings on the site and any other signs on the site.
 - ii. No sign shall include reflective material.
 - iii. Materials for permanent signs shall be durable and capable of withstanding weathering over the life of the sign with reasonable maintenance.
 - iv. The size of the structural members (e.g. columns, crossbeams, and braces) shall be proportional to the sign panel they are supporting.
 - v. The use of individual letters incorporated into the building design is encouraged, rather than a sign with background and framing other than the structure wall.
 - d. Street address
The review authority may require that a sign include the street address of the site, where it determines that public safety and emergency vehicle response would be more effectively served than if the street address were displayed solely on one or more buildings on the site.
 - e. Copy design guidelines
The City does not regulate the message content (copy) of signs; however, the following are principles of copy design and layout that can enhance the readability and attractiveness of signs. Copy design and layout consistent with these principles is encouraged, but not required.
 - i. Sign copy should relate only to the name and/or nature of the business or commercial center.
 - ii. Permanent signs that advertise continuous sales, special prices, or include phone numbers are prohibited.

- iii. Information should be conveyed briefly or by logo, symbol, or other graphic manner. The intent should be to increase the readability of the sign and thereby enhance the identity of the business.
 - iv. The area of letters or symbols should not exceed 40 percent of the sign area in commercial uses or 60 percent for residential uses.
 - v. Freestanding signs should contain the street address of the parcel or the range of addresses for a multi-tenant center.
- f. Sign lighting. Sign lighting shall be designed to minimize light and glare on surrounding rights-of-way and properties.
- i. External light sources shall be directed and shielded so that they do not produce glare off the site, on any object other than the sign.
 - ii. Sign lighting shall not blink, flash, flutter, or change light intensity, brightness, or color.
 - iii. Colored lights shall not be used at a location or in a manner so as to be confused or construed as traffic control devices.
 - iv. Neither the direct nor reflected light from primary light sources shall create hazards for pedestrians or operators of motor vehicles.
 - v. For energy conservation, light sources shall be hard-wired fluorescent or compact fluorescent lamps, or other lighting technology that is of equal or greater energy efficiency. Incandescent lamps are prohibited.

4. Sign maintenance.

- a. Each sign and supporting hardware, including temporary signs and awning signs, shall be maintained in good repair and functioning properly at all times. Any damage to a sign or its illumination, including the failure of illumination shall be repaired within a maximum of 14 days from the date of damage or failure.
- b. A repair to a sign shall be of materials and design of equal or better quality as the original sign.
- c. A sign that is not properly maintained and is dilapidated shall be deemed a public nuisance, and may be abated in compliance with the City's Zoning Code.
- d. When an existing sign is removed or replaced, all brackets, poles, and other supports that are no longer required shall be removed, and any/all damage to the exterior of the building shall be repaired/repainted to the satisfaction of the Development Services Director or his/her designee. When an existing sign is removed, the repairs must match surface type and color of building exactly.

D. Sign Standards by Use

Each sign shall comply with the standards provided by this Section and comply with the requirements in the following Table 4, except as permitted by the approval of a Creative Sign Permit described below.

E. Master Sign Program

All mixed use projects shall require a Master Sign Program, which is reviewed and approved by the decision-making body in each case. A "master sign plan" means a coordinated program of signage for new or existing commercial, office or residential which contain more than one business establishment or tenant. The Master Sign Program can permit signs that meet the intent and standards of the Sign Code and ensure that the all signs are integrated thoughtfully into the design of the structures, creating a unified architectural

statement. The Master Sign Program provides a means for defining common sign regulations for multi-tenant projects, to encourage maximum incentive and latitude in the design and display of multiple signs, and to achieve, not circumvent, the intent of this chapter.

1. Application Requirements Revisions to Master Sign Programs. A sign permit application for a master sign program shall include all information and materials required by the department, and the filing fee set by the city's Fee Resolution. Revisions to a master sign program may be approved by the Director with a standard sign permit if the intent of the original approval is not affected. Revisions that would substantially deviate from the original approval shall require the approval of a new master sign program.
2. Standards. A master sign program shall comply with the following standards:
 - a. The program shall comply with the purpose of this chapter.
 - b. The signs shall enhance the overall development, be in harmony with, and relate visually to other signs included in the master sign program, to the structures or developments they identify, and to surrounding development;
 - c. The program shall accommodate future revisions that may be required because of changes in use or tenants; and
 - d. The program shall comply with the standards of this chapter, except that flexibility is allowed with regard to sign area, number, location, or height to the extent that the master sign program will enhance the overall development and will more fully accomplish the purposes of this chapter.

F. Creative Sign Permit

1. Definition Creative Sign Permit. Applicants may apply for a Creative Sign Permit for those signs which are not listed or which exceed the provisions of this Chapter. The Creative Sign Permit is intended for signs that meet the intent and standards of the Sign Code, but may not necessarily meet the standards shown in Table 4. An applicant may request approval of a creative sign permit to authorize on-site signs that employ standards that differ from the other provisions of this chapter but comply with the intent of this Chapter.
2. Revisions to a master sign program may be approved by the Director with a standard sign permit if the intent of the original approval is not affected. Revisions that would substantially deviate from the original approval shall require the approval of a new master sign program.
3. Purpose. To encourage signs of unique design, and that exhibit a high degree of thoughtfulness, branding, imagination, inventiveness, and spirit; and to provide a process for the application of sign regulations in ways that will allow creatively designed signs that make a positive visual contribution to the overall image of the city, while mitigating the impacts of large or unusually designed signs.
4. Application and Procedure Requirements. A sign permit application for a creative sign shall include all information and materials required by the department, and the filing fee set by the city's Fee Resolution. A sign permit application for a creative sign shall be subject to review and approval by the Director of Development Services when the proposed sign is fifty square feet or less, and by the Commission when the sign is larger than fifty square feet. Notification for a sign permit for a creative sign shall be given in the same manner specified by this Zoning Ordinance for Director-approved development permits.
5. Design Criteria. In approving an application for a creative sign, the review authority shall ensure that a proposed sign meets the following design criteria.
 - a. Design Quality Criteria. The sign shall 1) constitute a substantial aesthetic improvement to the site and shall have a positive visual impact on the surrounding area; 2) be of unique design, and exhibit a high degree of thoughtfulness, imagination, inventiveness, and spirit; and 3) provide strong graphic character through the imaginative use of graphics, color, texture, quality materials, scale, and proportion.

- b. Contextual Criteria. The sign shall contain at least one of the following elements: 1) classic historic design style; 2) creative image reflecting current or historic character of the city; 3) symbols or imagery relating to the citrus packing industry; or 4) inventive representation of the use, name, or logo of the structure or business.
- c. Architectural Criteria. The sign shall: 1) utilize or enhance the architectural elements of the building; and 2) be placed in a logical location in relation to the overall composition of the building's façade and not cover any key architectural features and details of the façade.
- d. Neighborhood Impacts. The sign shall be located and designed not to cause light and glare impacts on neighboring residential uses.

23.112.070 Table 4. Sign Standards by Use

G. SIGN STANDARDS			
MULTI-FAMILY RESIDENTIAL USE			
Allowed Sign	Maximum Sign Height	Maximum No. of Signs Allowed per Parcel	Maximum Sign Area Allowed per Parcel
1. Wall or Freestanding	Wall signs: below edge of roof. Placement shall only be located below the roof or over the primary entrance. Freestanding: 48 inches	1 wall sign or freestanding sign per entrance or street frontage	12 sf each per face area; 24 sf maximum total sf for all signs.

H. SIGN STANDARDS		
NON-RESIDENTIAL USE/MIXED USE		
Allowed Sign	Placement Standards	Maximum Number and Sign Area
1. Awning	Shall be entirely on awning valance; lettering max 66% of valance height; valance height max: 18 inches.	50% of the area of the valance front. 1 sign max per each separate awning valance.
2. Marquee	To be established during project review. Allowed only for the entrance of a theatre or playhouse.	To be established during project review. 1 sign max
3. Monument	5 ft including base structure. Allowed only on a site with more than 100 ft. of continuous street frontage.	36 sf

4. Projecting or suspended	16 inches from face of building and bottom of sign shall be no closer than 8 ft above sidewalk surface below.	6 sf. No dimension greater than 3 ft. Sign shall be redwood sandblasted, hand carved or architecturally designed.
5. Wall	2 ft below parapet or eave. Individual letters 18 inches. Mounting 1-story: above 1 st floor windows. Mounting multi-story: between windows.	1 sf. Per linear foot primary business. 1 sign allowed per business frontage with pedestrian entrance. Side street or rear entrance wall sign max 50% of the primary sign area.
6. Window Permanent	Within window area	15% of total window area max.
7. Window Temporary	Within window area	25% of total window area. Allowed for display a maximum of 15 days at 1 time, up to 3 times in a 12 month period.
8. A-boards and other portable sidewalk signs are permitted	May not impede pedestrian flow.	1 per business. Signs may only be permitted while the business is open. Requires an encroachment permit if in the public right-of-way and a Creative Sign Permit
9. Building Wall Facing RR ROW	Businesses may have signage equal to or less than the allowable projecting or wall sign standards.	Building or parcel must front along Crowther Avenue and the Railroad ROW
10. Directional Signage on private property	Cannot be freestanding and shall be minimized for basic directional functions.	Included in sign permit application approval process.

I. Legal Nonconforming Signs

A legal nonconforming sign is any permanent or temporary sign that was legally established and maintained in compliance with the provisions of all applicable laws in effect at the time of original installation but that does not now comply with the provisions of this ordinance.

1. General requirements. A legal nonconforming sign shall lose its legal nonconforming status as defined herein, and brought into conformance with the provisions of this ordinance if any of the following occur:
 - a. changes including enlargement, reduction, changes in of the legal nonconforming sign;
 - b. Structural alteration for any reason;
 - c. Re-established after a business is discontinued for 60 days or more, subject to the amortization clause below; or

- d. Re-established after damage or destruction to 50 percent or more of the value of the sign, or its components, as determined by the Building Official and subject to the amortization clause below.

2. Maintenance and changes.

Sign copy and face changes, nonstructural modifications, and nonstructural maintenance (e.g., painting, rust removal) are allowed without a sign permit up to a maximum of 25 percent of the existing total area of the sign. Face changes not including copy, and any nonstructural modifications exceeding 25 percent of the existing total area of the sign, and any structural changes shall comply with all applicable standards of this Chapter.

23.112.080

A. Amortization and Existing Uses

General Explanation: The proposed amortization clause seeks to preserve private property rights in existence at the time of this zone change. Immediately following adoption of this ordinance, all legal uses, buildings or structures may continue to exist as a legal nonconforming use, building or structure. Additionally, for the five year period after the zone change is adopted, these same legal nonconforming uses, buildings or structures may be transferred or assigned.

B. Five (5) years after the effective date of this ordinance, the property may be sold or transferred and the legally nonconforming use, building, or structure may continue in the following circumstances:

- i. The business/property is transferred from a Parent to his/her Child, from a Child to his/her Parent as defined in Chapter 23.04 of Municipal Code.
- ii. The business/property is transferred from an owner to his/her employee(s) such that the Ownership does not change as defined in Chapter 23.04 of Municipal Code.

C. Notwithstanding the foregoing, five (5) years after the effective date of this ordinance, the exception set forth in subsection (B) shall only apply if:

- i. The same use in existence as of five years from effective date of this ordinance will continue to operate. If the primary use of the business/property (not accessory uses), remains unchanged, then the secondary uses may change. Secondary uses are defined in the definitions section of this chapter. Secondary uses may also be “accessory uses” as defined in Section 23.04.030 of the PMC and which means “a use incidental, appropriate, subordinate and devoted exclusively to the main use of the lot or building”; and
- ii. The building or structure is not modified or expanded; and

The use, building or structure is not abandoned or discontinued for twelve (12) months or more.

D. Except as provided in this section, all Old Town zone properties must come into compliance with the requirements of this Chapter upon its effective date.

E. In order to preserve private property rights, all legal uses, building or structures operating and/or in existence immediately prior to the effective date of this Chapter, may be continued subject to the following limitations:

- a. If at any time, the building, structure or use is abandoned or discontinued for twelve (12) months or more, the buildings, structures or uses must be brought into compliance with this ordinance.
- b. Any building, structure or use may not be altered or expanded.
- c. The business/property may be transferred from a Parent to his/her Child, from a Child to his/her Parent as defined in this Chapter.
- d. The business/property may be transferred from an owner to his/her employee(s) such that the Ownership does not change as defined in this Chapter.
- e. For a period of five (5) years following the effective date of this Ordinance, all legal uses operating and in existence immediately prior to the effective date of this ordinance, may be

<p>transferred or assigned from the owner to any other party, provided that the buyer continues to operate the same business activity. This transferred business activity must remain the primary business activity on the property, and the building footprint may not be expanded.</p> <p>f. If the primary use of the business/property (not accessory uses), remains unchanged, then accessory uses may change. An accessory use as defined in Section 23.04.030 of the PMC means “a use incidental, appropriate, subordinate and devoted exclusively to the main use of the lot or building.”</p>
<p>F. Beginning five (5) years after the effective date of this Chapter, no Old Town zoned property may be sold or transferred unless it is brought into compliance with the requirements of this chapter, unless the sale or transfer is specifically authorized in this subsection B above.</p>
<p>G. The City shall give notice to all property owners of properties within the Old Town regarding this ordinance in the following manner:</p> <ul style="list-style-type: none"> a. Within 180 days of adoption of this ordinance; b. Within 3 years after adoption of this ordinance; and c. At least 4 years after adoption of this ordinance. <p>Failure to provide any of the notices above shall not prevent the City from enforcing the requirements of this chapter.</p>
<p>H. Notwithstanding the foregoing, the property located at 102 S. Bradford is subject to the Chapter 23.06 “Placentia Historic Resources Ordinance,” the California Public Resources Code Section 21000 et. seq. “California Environmental Quality Act” as a historic resource. Due to its historical significance, conflict in complying with the historic resource provisions referenced herein, while also bringing the property into compliance with this ordinance along with the costs associated with bringing the use, building or structure into compliance with this ordinance, the property at 102 S. Bradford is exempt from Section 23.112.080 set forth herein. .</p>

23.112.090 Public Art/Public Plazas

Public Art and Public Plazas are encouraged in every development. Public art or plazas may be required as part of a development agreement for those developments that include 20 or more units or which are over 10,000 square feet. This includes new construction and remodel/rehabilitation of existing structures. Public art is encouraged to reflect the history of Old Town Placentia and the citrus growing industry.

Generally, the plans for proposed plazas or public art shall be part of the entitlement package submitted. The plazas may be located on the project site or at another location set forth in a development agreement, but must be located within the Old Town boundaries.

23.112.100 Old Town Development Community Facilities District Fee Program

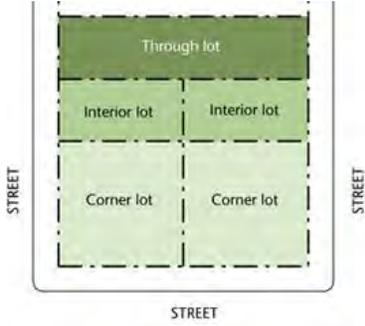
In addition to City baseline impact fees, Old Town will require additional public realm improvements and projects are subject to an Old Town Community Facilities District Fee that ensures all public sector infrastructure improvements can be provided. In addition to the Old Town Community Facilities District Fee, all projects will be required to install public infrastructure and streetscape elements up to the curb face of the public street immediately adjacent to the project, in accordance with the Public Realm Development Standards.

Definitions to be added to Chapter 23.04 of Municipal Code

Term	Definitions
Adaptive Reuse Plan	Adaptive reuse refers to a detailed plan for reusing an old site or building for a purpose other than which it was originally designed for. Adaptive reuse seeks to preserve existing buildings by retrofitting spaces for new uses while retaining much of the original features of the structure, and making use of existing infrastructure and transportation networks. Adaptive reuse plans are prepared by preservation professionals.
Antique or collectible store	An antique shop (or antiques shop) is a physical retail store specializing in the selling of antiques. Antiques are defined as a collectible object such as a piece of furniture or work of art that has a high value because of its considerable age. A collectible shop is a physical retail store specializing in the selling of objects that are suitable for collecting, typically an original work of art, sculpture, craft or antique. However, these items can range from extremely valuable objects to simple things that may only hold value to the collector. These items are may be viewed as investments by collectors and they may appreciate in value over time.
Artisan/Handcraft Shop	Retail stores selling art glass, ceramics, jewelry, and other handcrafted items. The facility can include an area for the crafting of the items being sold.
Bed & Breakfast	A guest house or small hotel offering sleeping accommodations and a morning meal. This does not include owners of single family homes renting individual rooms.
Bike Parking – Long Term	A volume of space that can accommodate locked storage of one or more bicycles or an area located inside a building where bicycles can be stored. Generally for longer term storage of bicycles.
Bike Parking – Short Term	A fixture to which one or more bicycles can be securely locked. Generally for 2 hours or less.
By Right Development	Developments that are permitted without a discretionary review process but do require Planning & Development Ad Hoc Committee review and Planning Division review to ensure compliance with zoning standards.
Child/Parent	“Child” and “Parent” shall have the same meaning as defined in California Probate Code Sections 26 and 54, respectively. In the event of any renumbering or repeal of Sections 26 and/or 54, the successor definition(s) provided pursuant to the provision shall apply.
Commercial recreation facility, indoor	A recreational facility such as a gym, that is for profit, and for which most of the recreational activities are within a closed building.

Courtyard	An open space created by a minimum of 3 sides of a courtyard building and used for private recreation in residential developments.
Courtyard Housing	Building type consisting of residences that can be arranged in several possible configurations: townhouses, townhouses over apartments, apartment over apartments, where an apartment occupies a single floor.
Cross Alley Development	A single development that incorporates multiple properties, in which a portion of the development spans the alley overhead, or the alley is vacated, or the development parcels on either side keep the alley public right of way, but incorporate the alley into the design, tying it visually into the architecture and design of the developments on either side. If the alley is not vacated, it would remain public right of way. Vehicular or pedestrian access from the alley to the new development would be permissible.
Electric Vehicle Charging Stations	<u>Level 2: 240-volt:</u> Level 2 requires charging equipment to be purchased and installed and provides about 10-20 miles of range per hour of charge. From empty, a full size battery electric car takes about 4-7 hours to recharge. <u>DC Fast Charging: 440-volt:</u> DC fast charging provides up to an 80% charge in about 30 minutes.
Entitled Project	Entitled project shall mean a project that has obtained final approval of all necessary planning and other land use approvals.
General Retail	A business or person who sells goods to an individual consumer as opposed to a wholesaler or supplier, who normally sell their goods to another business. Any retail transaction, which has a good sold, is taxable by the State Board of Equalization.
Green	Available for informal active and passive recreation. A green may be spatially defined by ground plan landscape and informal trees and/or buildings.
Hostess Bar	Hostess clubs are nightclubs where staff cater to and/or engage with customers seeking drinks and/or attentive conversation. Typically the staff will be scantily clad. These are also called “bikini bars,” “bee clubs,” and other similar descriptions.
Live/Work	Integrated residence and working space, occupied and utilized by a single household in a structure that has been designed or structurally modified to accommodate joint residential occupancy and work activity. However, such residential use shall only be allowed on the second floor or above of said live/work space. The interior residential portion shall be clearly separated and not be visible from the commercial space.
Medical Offices/ Services	An office or health facility providing health services including, without limitation, preventative and rehabilitation treatment, diagnostic services, testing and analysis. This use includes offices providing medical, dental, surgical, rehabilitation, podiatric, optometric, chiropractic and psychiatric services, and medical or dental laboratories

	incidental to these offices, but exclude inpatient services and overnight accommodation.
Mixed Use	The combination of non-residential and residential uses in the same structure or on the same site, where the residential component is located either above (vertical mixed-use) or behind or next to (horizontal mixed-use) the non-residential component.
Neighborhood Market	A retail store specializing in fresh produce and staples including bread, cereal, dairy products, and may include a deli counter. More than 75% of floor plan shall be devoted to food sales.
New Construction	New construction means any new ground up building, or any additions/renovations of more than 50% of existing ground floor building square footage, or any major remodel projects of buildings that are over 10,000 square feet, particularly if that remodel is to update the use of the property to render it more financially viable.
Nightclub	Any bar, cocktail lounge, discotheque, or similar establishment which provides entertainment including but not limited to music, dancing, and/or comedy in conjunction with alcoholic beverage sales. Includes bars, taverns, pubs, karaoke bars, and similar establishments where any food service is subordinate to the sale of alcoholic beverages.
Office Use	A place of business providing administrative business professional services such as insurance agencies, real estate offices, law offices, architectural or design offices, accounting services, travel agencies, etc. This includes government offices, and postal facilities and businesses engaged in the production of intellectual property such as advertising agencies, computer software production and programming services, educational, scientific and research organizations, media postproduction services, photography and commercial art studios, and writers and artists offices. This definition does not include “banks and financial Services.”
Ownership	Five (5) years from the effective date of this Ordinance, Ownership shall mean 51% or more interest in real property including all land, structures, and all interest in the property.
Personal Services	Personal services are any businesses where services are provided or performed through direct physical contact between patron and employee. These include but are not limited to: barbers, beauticians, aestheticians, cosmetologists, nail salons, tanning salons, massage therapists, and tattoo parlors/body modification studios. They do not include doctors, dentists, chiropractors, or other state-licensed medical professionals.
Plaza	An open area usually located near buildings and often featuring walkways, trees and shrubs, places to sit, and sometimes shops
Primary Use	Five (5) years from the effective date of this Ordinance, Primary Use shall mean use or occupancy of 70% or more of the total building area.
Retail and/or	Uses as listed as Retail/Commercial Uses in Table 1 herein.

Commercial Uses	
Studio	A place for the study or practice of an art, skill or specific fitness activity (such as dancing, singing, acting, cooking, yoga, palates, spinning, etc). Typically this is one room devoted to the activity and where there is a limited number of teachers, all teaching the same skill or activity.
Telecommunication Cell Tower	A cell tower not including building used for telecommunication businesses.
Through Lot Development	<p>A lot, other than a corner lot, having frontage on two different public streets or highways.</p>  <p>The diagram shows a rectangular block bounded by four streets. The top street is labeled 'STREET' and the bottom street is labeled 'STREET'. The left and right streets are also labeled 'STREET'. The block is divided into five lot types: a 'Through lot' at the top, two 'Interior lot's in the middle, and two 'Corner lot's at the bottom. The 'Through lot' is shaded in a darker green, while the other lots are shaded in a lighter green.</p>
Transit Oriented Development (TOD)	Transit-oriented development, or TOD, is a type of community development that includes a mixture of housing, office, retail and/or other amenities integrated into a walkable neighborhood and located within a half-mile of quality public transportation.
Wrapped Parking	A building parking design that completely conceals on all sides a parking garage that is designed for occupancy by retail, service, office, and/or residential uses, or for an all residential development.

EXECUTIVE SUMMARY

INITIAL STUDY AND NEGATIVE DECLARATION

OLD TOWN PLACENTIA REVITALIZATION PROJECT PLACENTIA, CALIFORNIA



LEAD AGENCY:

**CITY OF PLACENTIA
DEVELOPMENT SERVICES DEPARTMENT,
PLANNING DIVISION
401 EAST CHAPMAN AVENUE
PLACENTIA, CALIFORNIA 92870**

REPORT PREPARED BY:

**BLODGETT BAYLOSIS ENVIRONMENTAL PLANNING
16388 E. COLIMA ROAD, SUITE 206J
HACIENDA HEIGHTS, CALIFORNIA 91745**

MAY 22, 2017

PLCA 001

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1. INTRODUCTION

The proposed Old Town Placentia Revitalization Project involves the adoption and subsequent implementation of the Old Town Placentia Revitalization Plan document, the Old Town Placentia Revitalization Plan Development Standards (i.e. the zoning text amendment), and the Streetscape Master Plan. The Revitalization Project will also involve an amendment to the General Plan land use map to reflect the land uses contemplated within the Revitalization Plan document. The Old Town Placentia Revitalization Project applies to an L-shaped area that is located in the southwestern portion of the City of Placentia. The Planning Area is focused on Bradford Avenue and Santa Fe Avenue. Within the Planning Area are various sub-areas that include planned commercial and high-density residential uses, which are depicted in Exhibit 5. The Planning Area is just over 32 acres in size and is located north of the Burlington Northern-Santa Fe (BNSF) railroad tracks and south of Chapman Avenue. The Revitalization Project is anticipated to facilitate development, especially residential and retail, in a mixed-use setting, proximate to the newly approved Metrolink Station and parking structure in the Old Town area and the proposed Transit Oriented Development (TOD) south of the railroad along Crowther Avenue. The purpose of the Revitalization Project is to enhance the physical environment in the City's Old Town aimed at creating a lively destination to support the current economic base, create a town center for Placentia, and better connect to adjacent neighborhoods and surrounding cities. Finally, the residential development within the proposed High Density Residential zone will be "by right," pursuant to the State of California housing law. The Revitalization Project's implementation, through changes to the Zoning Code and General Plan, is critical in aiding in the realization of local and regional goals related to sustainable and infill development.

The City of Placentia is the designated *Lead Agency* for the proposed project and will be responsible for the project's environmental review. The Revitalization Project is considered to be a project under the California Environmental Quality Act (CEQA) and, as a result, the project is subject to the City's environmental review process. The project Applicant is City of Placentia, 401 East Chapman Avenue, Placentia, California 92870.

The City determined, as part of the Initial Study's preparation, that a Negative Declaration is the appropriate environmental document for the proposed project's CEQA review. The attached Initial Study and the *Notice of Intent to Adopt a Negative Declaration* will be forwarded to responsible agencies, trustee agencies, and the public for review and comment. A 20-day public review period will be provided to allow these entities and other interested parties to comment on the proposed project and the findings of the Initial Study. Questions and/or comments should be submitted to the following individual:

Joseph M. Lambert, Director of Development Services
City of Placentia, Development Services Department, Planning Division
401 East Chapman Avenue
Placentia, California 92870
714-993-8124

2. PROJECT LOCATION

The Old Town Placentia Revitalization Project applies to an L-shaped Planning Area that is located in the southwestern portion of the City of Placentia. The Planning Area is centered along Bradford Avenue south of Chapman Avenue and Santa Fe Avenue from Murray Street on the west to Alta Street on the east. The portion of the Planning Area centered on Bradford Avenue extends east to Alta Street and west to Main Street, south of Chapman Avenue. The portion of the Planning Area centered on Santa Fe Avenue extends from Murray Street on the west, Aguirre Lane (and extension of Aguirre Lane) on the north, the Burlington Northern-Santa Fe (BNSF) railroad tracks to the south, and Alta Street to the east. The Planning Area is just over 32 acres in size and is located north of the Burlington Northern-Santa Fe (BNSF) railroad tracks and south of Chapman Avenue. The Old Town Placentia Planning area is bounded by Chapman Avenue in the north, the Burlington Northern and Santa Fe Railway train tracks in the south, Alta Street in the east, and Murray Street in the west. The location of Placentia in a regional context is shown in Exhibit 1. A citywide map is provided in Exhibit 2 and a Planning Area map is provided in Exhibit 3.

3. ENVIRONMENTAL SETTING

Old Town Placentia is the City's original central business district founded in 1910 near the original Placentia train depot and the citrus packing houses. The Planning Area primarily consists of commercial uses along Santa Fe Avenue and Bradford Avenue, with residential uses primarily along the surrounding streets. The Planning Area is located north of the Burlington Northern-Santa Fe (BNSF) railroad tracks and proximate to the newly approved Metrolink Station and parking structure in the Old Town area and the recently approved Transit Oriented Development (TOD) south of the railroad along Crowther Avenue. The Metrolink Station will have two platforms, one north of the railroad and one south of the railroad. A pedestrian over-crossing bridge and a grade-separated road at Melrose Street have been completed in anticipation of the new transit station. Together with the new TOD zoning to the south, this proposed rezoning of the Old Town Placentia area represents the City's planning efforts to accommodate and capitalize on the new Metrolink Station.

The Planning Area consists of over 140 parcels made up of a diverse mix of commercial land uses, surrounded by residential neighborhoods to the west, east, and north. Many parcels are less than 5,000 square feet in size and this contributes to the small scale of the Old Town. Notable uses within the Planning Area and in the surrounding area include four community churches/church structures, the City's senior center, a fire station, and two parks. The Planning Area does not include any schools, though Kraemer Middle School and Valencia High School are located north of Chapman Avenue. California State University Fullerton is located approximately two miles to the west, in the City of Fullerton. The Planning Area is principally zoned "SF-C" Santa Fe Commercial District and there are other zones in the planning area: "C-1," "C-2," and "R-2" zones. An aerial photograph of the Planning Area is provided in Exhibit 4. The planned zoning designations for the Planning Area are shown in Exhibit 5.

EXECUTIVE SUMMARY
INITIAL STUDY & NEGATIVE DECLARATION • CITY OF PLACENTIA
OLD TOWN PLACENTIA REVITALIZATION PROJECT

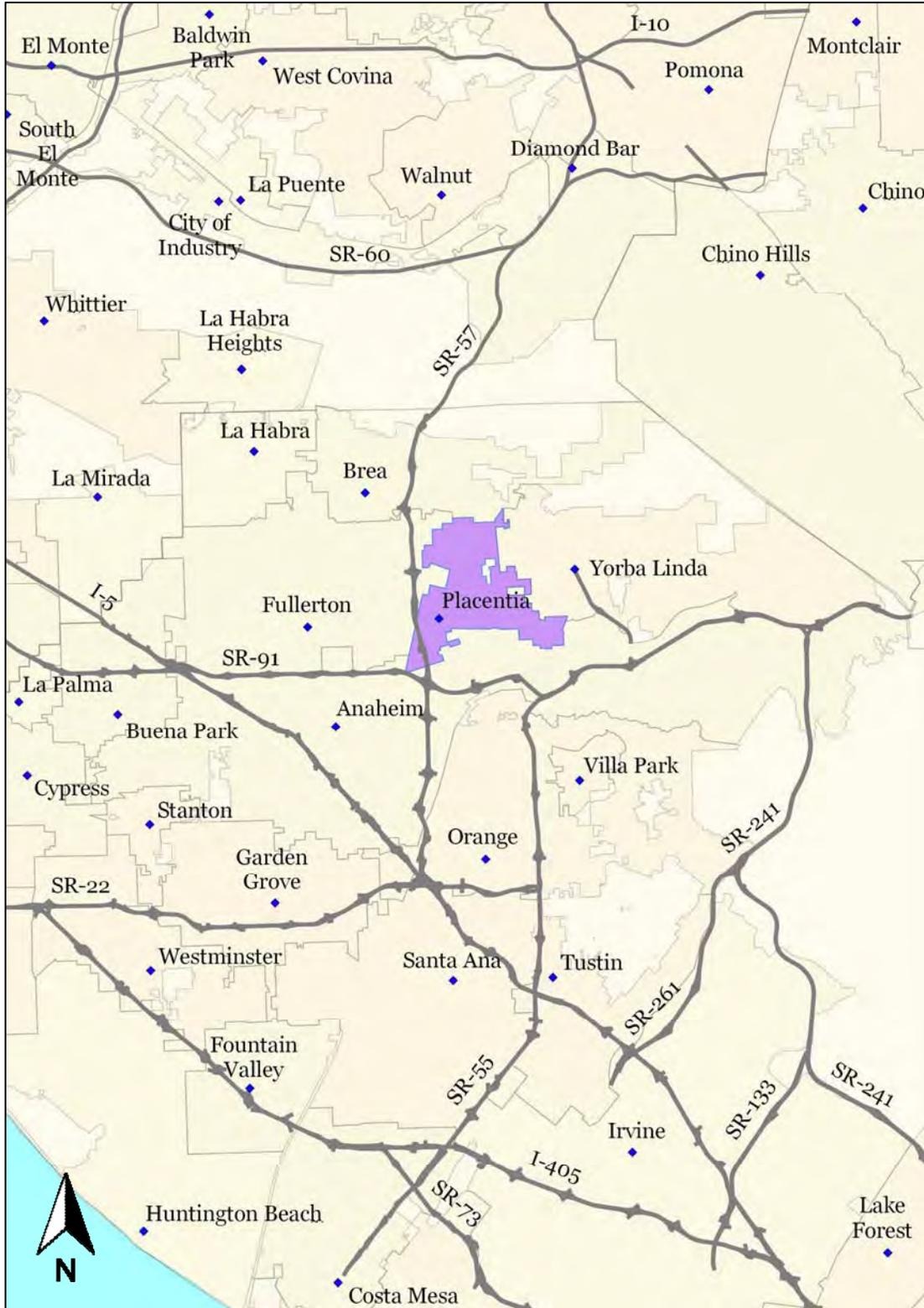


EXHIBIT 1
REGIONAL LOCATION
SOURCE: QUANTUM GIS

EXECUTIVE SUMMARY
INITIAL STUDY & NEGATIVE DECLARATION • CITY OF PLACENTIA
OLD TOWN PLACENTIA REVITALIZATION PROJECT

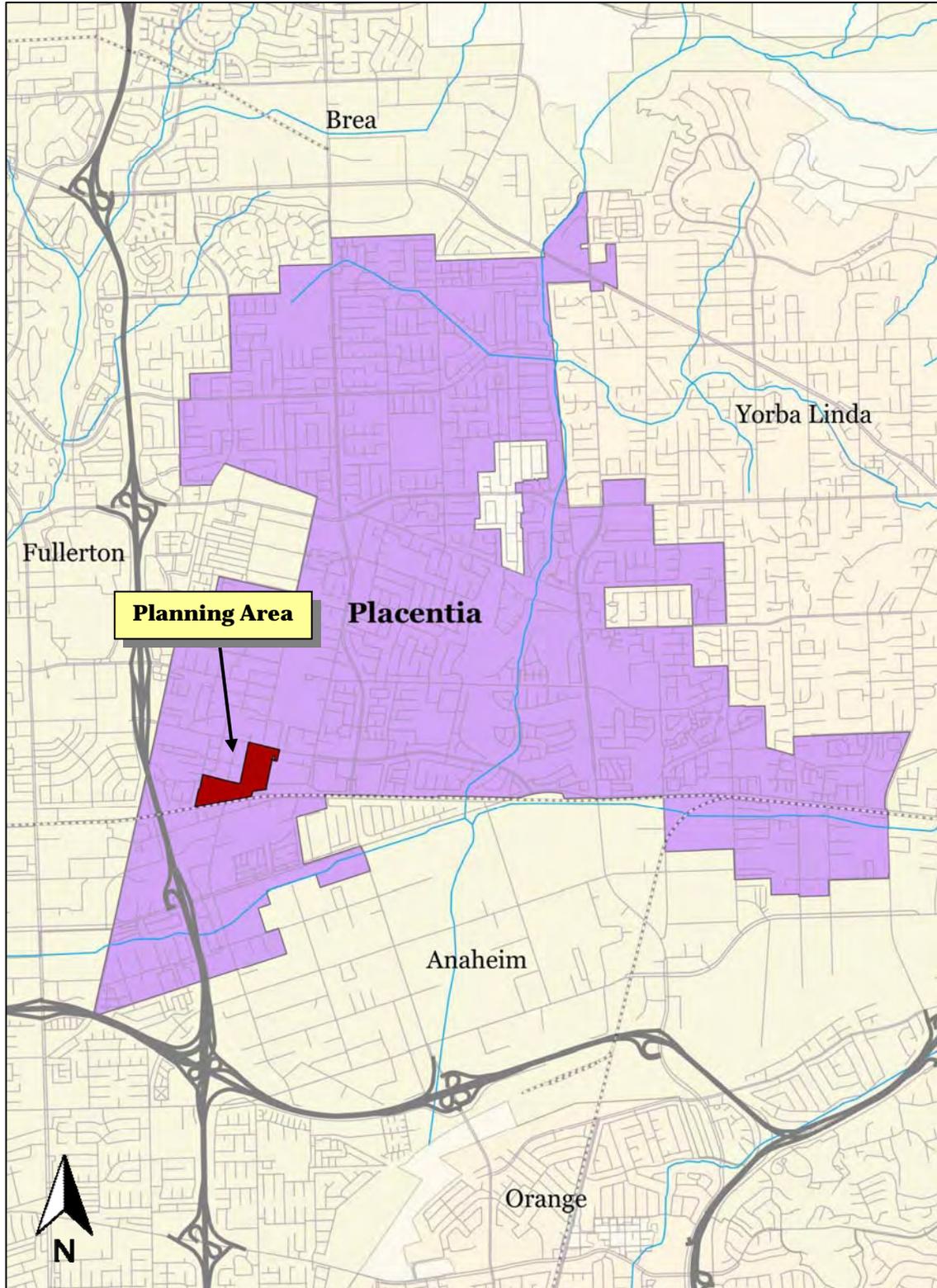


EXHIBIT 2
CITYWIDE MAP
SOURCE: QUANTUM GIS



EXHIBIT 3
PLANNING AREA
Source: Quantum GIS



EXHIBIT 4
AERIAL PHOTOGRAPH OF THE PLANNING AREA
SOURCE: GOOGLE EARTH

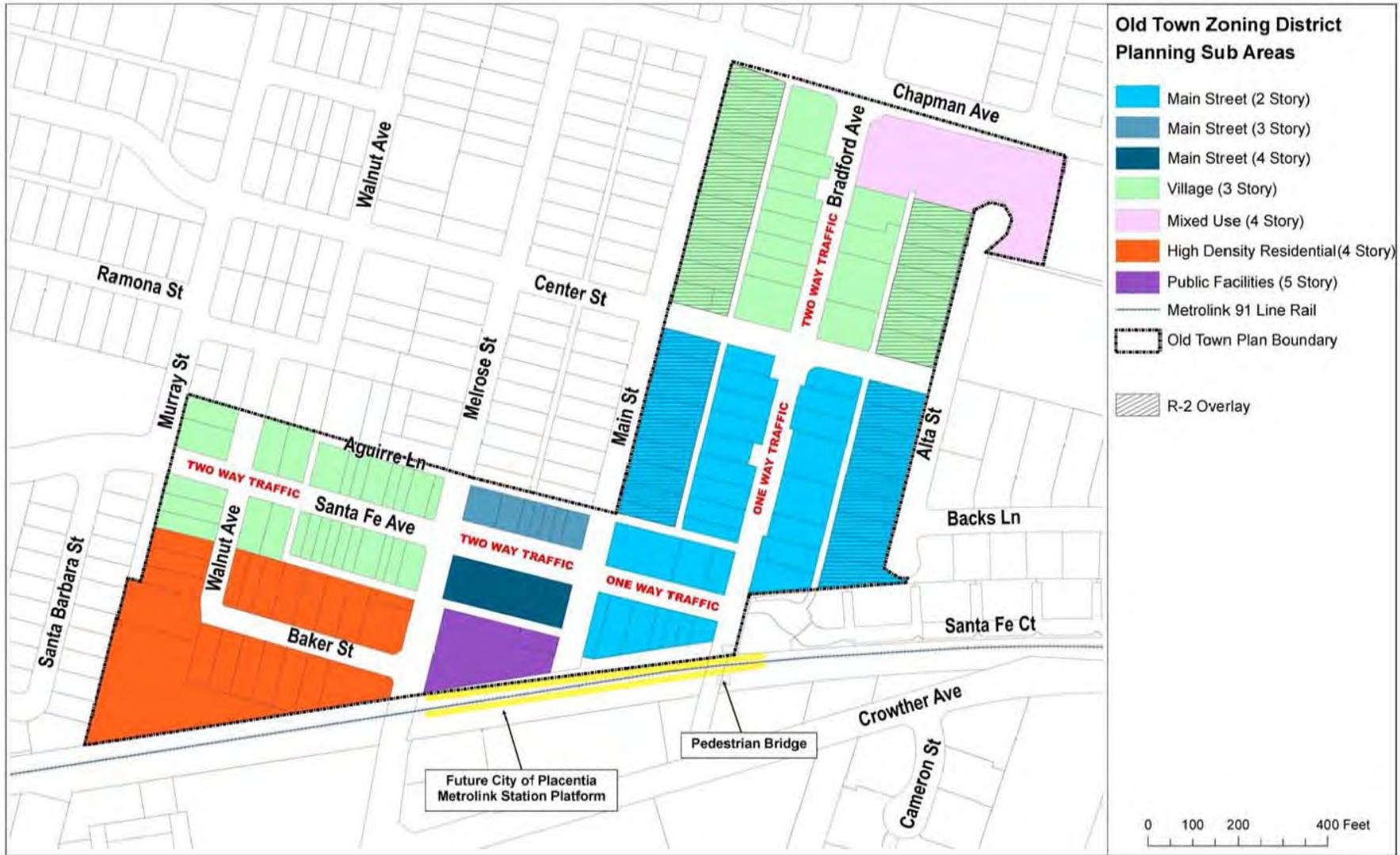


EXHIBIT 5 PROPOSED OLD TOWN ZONING DISTRICT PLANNING SUB-AREAS

Source: City of Placentia

4. PROJECT DESCRIPTION

BACKGROUND FOR PLANNING

The Old Town Placentia area suffered from disinvestment and economic decline with the widespread growth of Orange County's suburbanization from the 1960's to the 1990's. Nevertheless, existing property owners, residents, and businesses have kept the area active with a mix of small businesses, restaurants, churches, and public venues. This activity has helped this area retain its unique small scale environment which, with the new Metrolink Station, has the potential to become a vibrant commercial area. Market data indicates that population groups of all types are looking for small scale, walkable urban centers located around transit to live, work, and play. This is occurring in Orange County with new, more compact, and transit-oriented developments and historic reuse districts emerging in Anaheim, Orange, Fullerton, and Santa Ana. Coupled with the proposed Transit Oriented Development (TOD), including a new Metrolink transit station south of the railroad along Crowther Avenue, there is a unique opportunity for Old Town to see revitalization. The goals and objectives of the Old Town Revitalization Plan are expected to be implemented under a partnership between City Elected Officials, City Departments, regional agencies, local businesses, stakeholders, residents, and community organizations. The rezoning of the area and the streetscape plan are expected to be adopted by Summer of 2017 and will assist in attracting development to the area in the shorter term.

FORMAT AND PURPOSE OF THE REVITALIZATION PLAN, DEVELOPMENT STANDARDS, AND STREETScape MASTER PLAN

The City's revitalization efforts for the Old Town area center on three principal documents: the Old Town Placentia Revitalization Plan document, the Streetscape Master Plan, and the Zone Change, which sets out new and specific Development Standards for the Planning Area. The Old Town Revitalization Plan document is organized to provide a framework for private and public realm improvements in the Planning Area. The Plan is organized as follows:

- *Chapter 1: Introduction* explains the Old Town Placentia Revitalization Plan's background and purpose, identifies the Planning Area, describes citywide planning efforts, describes community outreach efforts, and outlines the organization of the Revitalization Plan document. This chapter also sets forth the administration of the Revitalization Plan.
- *Chapter 2: Vision* describes the vision for revitalization of the Planning Area. This chapter also outlines the goals and policies that establish the framework for the recommendations contained in the Revitalization Plan document.
- *Chapter 3: Mobility* details the circulation improvements for the Planning Area, including automobile, transit, pedestrian, and bicycle modes. This chapter also displays images of street cross-sections and streetscape plans for the Old Town Planning Area. Mobility recommendations within this chapter support the "Complete Streets" concept as well as other policy and design approaches, such as a circulation plan for the Planning Area.

- *Chapter 4: Infrastructure* identifies existing infrastructure and any necessary improvements to the water, sewer, storm water systems, and utility lines. This chapter also provides recommendations relevant to utilities.
- *Chapter 5: Streetscapes* describes the pedestrian realm of the Old Town Planning Area and the current lack of streetscapes. This chapter also details various recommended streetscape element improvements.
- *Chapter 6: Parking* describes the current parking conditions and the recommended parking measures and parking programs.
- *Chapter 7: Land Use* sets forth the zoning, permitted uses, and development and design standards for the Revitalization area. This chapter also details the design guidelines that will shape the character of future development in the Revitalization Project area.
- *Chapter 8: Historic Resources* describes the historic context of the Revitalization Project and identifies the existing historical resources within the Planning Area. This chapter also outlines recommendations for historic preservation.
- *Chapter 9: Business Support* outlines recommendations that will encourage small business growth and active business participation for the revitalization of the Planning Area.
- *Chapter 10: Community Events & Programming* defines the purpose of hosting community events and programming, and lists recommendations to facilitate the hosting of community events and programming.
- *Chapter 11: Maintenance* outlines recommendations for on-going maintenance of the Old Town Planning Area.
- *Chapter 12: Implementation* defines the phasing and intergovernmental coordination associated with the implementation of the Revitalization Project. This chapter also provides a table that lists the various phases of implementation, the timing of the phases, and the responsible agency for each phase.
- *Chapter 13: Sources of Funding* describes the various potential sources of funding available for the Old Town Placentia Revitalization Project.

The purpose of the Old Town Placentia Revitalization Plan document is to provide a framework and guidelines in order to enhance the physical environment in the City's Old Town aimed at creating a lively destination to support and expand the current economic base, create a town center for Placentia, and better connect to adjacent neighborhoods and surrounding cities. The Development Standards document (i.e. the zoning amendment) is intended to provide specific development standards for all future development within the Old Town Placentia Planning Area. Specific development standards may include architectural design, building height, and building setback, among many other specifications for

development within the different sub-areas of the Planning Area. Finally, the purpose of the Streetscape Master Plan is to identify conceptual designs for the roadway network within the Planning Area.

OLD TOWN PLACENTIA REVITALIZATION PLAN POLICY FRAMEWORK

The Old Town Placentia Revitalization Plan document includes a wide range of goals and policies that will establish the framework for the plan. The following table (Table 1) outlines the goals and policies included in the Revitalization Plan document which will serve to mitigate the potential impacts of new development within the Planning Area.

**Table 1
 Goals and Policies of the Old Town Placentia Revitalization Plan**

Goals	Policies
<i>Goal 1: Make Old Town Placentia a Unique Destination.</i>	●A. Celebrate the City’s heritage.
	●B. Make connections to the past.
	●C. Create a walkable village atmosphere.
	●D. Build public gathering spots.
	●E. Enhance the City’s and Old Town’s identity.
	●F. Highlight historic features.
	●G. Improve public and private spaces.
	●H. Create a diversified commercial base.
	●I. Create a shopping destination.
<i>Goal 2: Make Old Town Placentia a Pedestrian, People Priority Zone.</i>	●A. Encourage and allow compact development.
	●B. Encourage and allow outdoor dining.
	●C. Provide for small passive recreation spots.
	●D. Support inclusivity across all walks of life.
	●E. Enhance community facilities and services.
	●F. Build lively streetscapes.
	●G. Create a small-scale village environment.
	●H. Get people to stay in Old Town longer.
	●I. Limit vehicular trips.
	●J. Create a safe, inviting, pleasant, attractive, and appealing sense of place.
	●K. Tuck away parking where provided.
	●L. Avoid conflicts with motor vehicles.

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	<ul style="list-style-type: none"> ●M. Provide opportunities to create intimate walkways. ●N. Make the Old town walk-friendly for all ages and abilities.
<p><i>Goal 3: Promote Multi Modal and Sustainable Transportation.</i></p>	<ul style="list-style-type: none"> ●A. Connect surrounding suburban and commercial thoroughfares. ●B. Encourage use and increased ridership of the Metrolink System. ●C. Move away from the auto-orientation in the layout and land uses. ●D. Provide hubs for various transportation modes.
<p><i>Goal 4: Support Businesses and Provide for Development Opportunities.</i></p>	<ul style="list-style-type: none"> ●A. Enhance safety and security. ●B. Develop incentive programs. ●C. Provide business start-up opportunities. ●D. Promote the Old Town through media and outreach. ●E. Provide for infill development, reuse, and preservation. ●F. Develop a promotional campaign to encourage visits to Old Town Placentia.

Source: Old Town Placentia Revitalization Plan.

PHYSICAL CHANGES RESULTING FROM THE IMPLEMENTATION OF THE REVITALIZATION PROJECT

The Revitalization Project is anticipated to facilitate development, especially retail and residential in a mixed-use setting. The major objectives of the Revitalization Project are to identify land use options that include providing new mixed-uses and increasing housing opportunities and neighborhood-serving retail uses. The Old Town Placentia Revitalization Project will encourage transit oriented development, promote active transportation, reduce vehicles miles traveled, improve access to regional open space resources, and create community benefits. The area’s assets include a walkable layout with its short blocks across a linear street pattern.

The adoption of the proposed Revitalization Project, by itself, will not lead to any new development. The adoption of the Revitalization Project will permit new development within the Planning Area governed by the rezoning with new development standards. The potential new development permitted under the Revitalization Project may include residential and retail land uses in a mixed-use setting, proximate to the newly approved Metrolink Station and parking structure in the Old Town area and the proposed Transit Oriented Development (TOD) south of the railroad along Crowther Avenue. The major objectives of the Revitalization Project are to identify land use options that include providing new mixed-uses, increasing housing opportunities, and increasing neighborhood-serving retail uses. The Revitalization Project’s implementation is critical in aiding in the realization of local and regional goals related to sustainable and infill development.

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The project team, including City Staff and the consultants (traffic, planning, and environmental), identified the maximum amount of new development that could be realized as part of the Revitalization Project's implementation. No specific new projects are known at this time, though the development projections are critical in evaluating the potential environmental impacts including, but not limited to, land use changes, traffic, air quality, population, and public services. The potential new development is outlined below:

- 525 residential units;
- 85,000 square feet of commercial (non-retail) uses;
- 40,000 square feet of retail use; and,
- A 50-room hotel.

Based on discussions with the City, the proposed residential uses will be a combination of apartments and townhomes. Single family homes will not be permitted in the area. Currently, the Old Town area contains a mix of single-family residential use and various commercial uses. The majority of properties in the area are developed. Therefore, the majority of the proposed land uses are expected to be infill development. Within the Planning Area are various sub-areas that include planned commercial and high-density residential uses, which are depicted in Exhibit 5.

The vision for the land use and private and public land is to keep development compact by providing opportunities for infill that preserves the small-scale character of the historic core. The goal for commercial diversity includes a healthy mix of retail, restaurants, combined with outdoor gathering spaces such as public plazas. The Revitalization Project encourages rehabilitation, additions, and alterations to existing properties, while providing opportunities for new infill development. Compact development and shared parking is encouraged in all of the sub-areas; the emphasis is on the pedestrian, not the vehicle. The Revitalization Plan document provides increased height over what is currently allowed by existing zoning. Office and service uses would be permitted on the second floor of the Planning Area buildings, providing an important element to create a more full service area. Residential development could occur in areas outside of the historic core, or "Main Street" areas.

The Plan prioritizes the "commercial block" style of architecture in its historic and contemporary forms. The style can be adapted for the two-, three-, and larger four-story buildings and includes facades placed at, or close to, the right-of-way line. This style is also a form of building designed for occupancy by retail, service, or office use on the ground floor and secondary commercial or residential uses on the upper floors. New infill development, rehabilitation, additions, and building alterations need to consider the context of the Old Town block pattern and setting as well as the existing properties of historic significance or merit.

As previously mentioned, the Planning Area includes the current "SF-C" Santa Fe Commercial District zone and some surrounding parcels in the "C-1", "C-2", and "R-2" zones. The Plan proposes the area to be rezoned as "Old Town" and further divides the Old Town into five planning sub-areas to guide new development, infill, preservation, and reintegration of land uses. These sub-areas include Main Street (two- and three-story) Village, Mixed Use, High Density Residential, and Public Facilities.

Main Street (Two and Three Story Height Sub-area)

This sub-area is the core of the Old Town area and is found along both Bradford Avenue and Santa Fe Avenue from Center Street to Melrose Street and is characterized by small scale buildings with frontages that blend with the streetscapes. The focus is on historic rehabilitation and infill development to create a dense urban segment. Uses would be limited to commercial type uses that occupy smaller spaces and attract a large number of visitors. The Main Street sub-area encourages the preservation and enhancement of its unique historic character, with commercial block building forms and shopfront architecture. Allowable land uses emphasize ground floor retail, restaurant, and related uses, with opportunities for offices and other services above. No residential uses are permitted in the Main Street sub-area. The restoration and renovation of existing façades is encouraged.

Village (Three Story Height Sub-area)

This sub-area is found in two separate places: one at the north end of Bradford Avenue from Center Street to Chapman Avenue (excluding the southeast corner) and along both sides of Santa Fe Avenue from Melrose Street on the east to Murray Street on the west. The intent is to create new and infill development opportunities while preserving existing historically significant properties such as the old City Hall, Library, and Telephone Exchange building. The Village sub-area provides an opportunity for a new walkable retail and office center as an extension of the Main Street sub-area. The Village sub-area would provide for the same building form and setbacks as the Main Street sub-area but would allow three stories, with residential above. Retail and restaurants would be located on the ground floor spaces, with offices and residential permitted above. Building types are intended primarily to be a commercial block with well-defined frontages in the shopfront style similar to the Main Street zone.

Mixed-Use (Four Story Height Sub-area)

This sub-area is found at the southeastern corner of Chapman and Bradford Avenue and would be permitted four stories in height. The Mixed Use sub-area provides for a mix of concentrated urban housing types with some live-work opportunities with commercial on the ground floor. Denser, high quality, landmark development is encouraged here as this is envisioned as the gateway into the Old Town area. This sub-area could provide for urban residential building types including townhouses and courtyard housing, and live-work buildings.

High Density Residential (Four Story Height Sub-area)

At the most southwestern corner of the Plan area is the high density residential sub-area, with a four-story height limit. This area can provide opportunity for new housing development; with its residents patronizing the Old Town businesses and using the new Metrolink station for certain transport needs.

Public Facilities (Five Story Height Sub-area)

This sub-area, bounded by Melrose Street on the west, Main Street on the east, the railroad on the south and the alley on the north, is the location for the upcoming parking structure. This structure will be five stories in height and will provide 253 parking spaces and will provide parking for Old Town visitors as well as those using the Metrolink station.

R-2 Overlay Zone

This overlay zone will permit the existing uses within the area and new residential uses, pursuant to Chapter 23.15 of the City's Zoning Code. Within the R-2 Overlay zone, no new retail or commercial uses will be permitted on the ground floor.

5. DISCRETIONARY ACTIONS

A discretionary action is a decision taken by a government agency (for this project, the government agency is the City of Placentia) that calls for an exercise of judgment in deciding whether to approve a project. As part of the proposed project's implementation, the City will consider the following approvals:

- General Plan Amendment (GPA 2017-02), to amend the General Plan map from the existing land use designations to a mixed-use land use designation to allow the following: local and neighborhood-supporting mixed-use areas designed to be contextually appropriate in and compatible with the identified neighborhood or historic area. On a single site, a combination of non-residential and residential uses can occur in the same structure or on the same site, where the residential component is located either above (vertical mixed-use) or behind or next to (horizontal mixed-use) the non-residential component. Commercial retail is encouraged to be the primary use on the ground floor. Professional office and housing uses are also encouraged, particularly as adaptive reuse opportunities within existing structures. Transit orientation, walkability and pedestrian access are key considerations..
- Zone Change (ZC 2017-02), to create new development standards for the new Old Town Zone, changing the zoning from R-2, C-1, C-2, and SF-C to Old Town Zone District;
- The adoption of the Old Town Placentia Revitalization Plan document;
- The adoption of a Streetscape Master Plan; and,
- The adoption of the Negative Declaration (ND).



6. SUMMARY OF ENVIRONMENTAL ANALYSIS

This section of the attached Initial Study analyzes the potential environmental impacts that may result from the proposed project's implementation. The issue areas evaluated in the attached Initial Study include the following:

- | | |
|---|---|
| <ul style="list-style-type: none">● Aesthetics;● Agricultural & Forestry Resources;● Air Quality;● Biological Resources;● Cultural Resources;● Geology & Soils;● Greenhouse Gas Emissions;● Hazards & Hazardous Materials;● Hydrology & Water Quality;● Land Use & Planning; | <ul style="list-style-type: none">● Mineral Resources;● Noise;● Population & Housing;● Public Services;● Recreation;● Transportation & Circulation;● Tribal Cultural Resources;● Utilities; and,● Mandatory Findings of Significance. |
|---|---|

The environmental analysis included in this section reflects the Initial Study Checklist format used by the City of Placentia in its environmental review process. Under each issue area, an analysis of impacts is provided in the form of questions and answers. The analysis then provides a response to the individual questions. For the evaluation of potential impacts, questions are stated and an answer is provided according to the analysis undertaken as part of the attached Initial Study's preparation. To each question, there are four possible responses:

- *No Impact.* The proposed project *will not* have any measurable environmental impact on the environment.
- *Less Than Significant Impact.* The proposed project *may have* the potential for affecting the environment, although these impacts will be below levels or thresholds that the City of Placentia or other responsible agencies consider to be significant.
- *Less Than Significant Impact with Mitigation.* The proposed project *may have* the potential to generate impacts that will have a significant impact on the environment. However, the level of impact may be reduced to levels that are less than significant with the implementation of mitigation measures.
- *Potentially Significant Impact.* The proposed project may result in environmental impacts that are significant.

The attached Initial Study will assist the City in making a determination as to whether there is a potential for significant adverse impacts on the environment associated with the implementation of the proposed project.

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The environmental analysis provided in Section 3 of the attached Initial Study indicates that the proposed project will not result in any potentially significant impacts on the environment. For this reason, the City of Placentia determined that a Negative Declaration is the appropriate CEQA document for the proposed project. The findings of the attached Initial Study are summarized in Table 2 provided below and on the following pages.

Table 2
Summary (Initial Study Checklist)

Environmental Issues Area Examined	Potentially Significant Impact	Less Than Significant With Mitigation	Less Than Significant Impact	No Impact
Section 3.1 Aesthetics. <i>Would the project:</i>				
a) Have a substantial adverse effect on a scenic vista?			X	
b) Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway?				X
c) Substantially degrade the existing visual character or quality of the site and its surroundings?				X
d) Create a new source of substantial light or glare which would adversely affect day or nighttime views in the area?				X
Section 3.2 Agriculture and Forestry Resources. <i>Would the project:</i>				
a) Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?				X
b) Conflict with existing zoning for agricultural use, or a Williamson Act Contract?				X
c) Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code §12220[g]), timberland (as defined in Public Resources Code §4526), or timberland zoned production (as defined in Government Code §51104[g])?				X
d) Result in the loss of forest land or conversion of forest land to non-forest use?				X
e) Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland, to non-agricultural use or conversion of forest land to non-forest use?				X
Section 3.3 Air Quality. <i>Would the project:</i>				
a) Conflict with or obstruct implementation of the applicable air quality plan?			X	
b) Violate any air quality standard or contribute substantially to an existing or projected air quality violation?			X	

Table 2
Summary (Initial Study Checklist)

Environmental Issues Area Examined	Potentially Significant Impact	Less Than Significant With Mitigation	Less Than Significant Impact	No Impact
c) Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors)?			X	
d) Expose sensitive receptors to substantial pollutant concentrations?			X	
e) Create objectionable odors affecting a substantial number of people?				X
Section 3.4 Biological Resources. Would the project:				
a) Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Wildlife or U. S. Fish and Wildlife Service?				X
b) Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations or by the California Department of Fish and Wildlife or U.S. Fish and Wildlife Service?				X
c) Have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?				X
d) Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?				X
e) Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?				X
f) Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?				X
Section 3.5 Cultural Resources. Would the project:				
a) Cause a substantial adverse change in the significance of a historical resource as defined in §15064.5 of the CEQA Guidelines?			X	
b) Cause a substantial adverse change in the significance of an archaeological resource pursuant to §15064.5 of the CEQA Guidelines?			X	
c) Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?			X	
d) Disturb any human remains, including those interred outside of dedicated cemeteries?			X	

Table 2
Summary (Initial Study Checklist)

Environmental Issues Area Examined	Potentially Significant Impact	Less Than Significant With Mitigation	Less Than Significant Impact	No Impact
Section 3.6 Geology and Soils. <i>Would the project:</i>				
a) Expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving rupture of a known earthquake fault (as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault), strong seismic ground shaking, seismic-related ground failure, including liquefaction, or landslides?			X	
b) Result in substantial soil erosion or the loss of topsoil?			X	
c) Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction, or collapse?			X	
d) Be located on expansive soil, as defined in Table 18-1-B of the California Building Code (2012), creating substantial risks to life or property?				X
e) Have soils incapable of adequately supporting the use of septic tanks or alternative wastewater disposal systems where sewers are not available for the disposal of wastewater?				X
Section 3.7 Greenhouse Gas Emissions. <i>Would the project</i>				
a) Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?			X	
b) Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing emissions of greenhouse gases?				X
Section 3.8 Hazards and Hazardous Materials. <i>Would the project:</i>				
a) Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?			X	
b) Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?			X	
c) Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?				X
d) Be located on a site which is included on a list of hazardous material sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?				X
e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or a public use airport, would the project result in a safety hazard for people residing or working in the project area?				X

Table 2
Summary (Initial Study Checklist)

Environmental Issues Area Examined	Potentially Significant Impact	Less Than Significant With Mitigation	Less Than Significant Impact	No Impact
f) For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing or working in the project area?				X
g) Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?				X
h) Expose people or structures to a significant risk of loss, injury or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands?				X
Section 3.9 Hydrology and Water Quality. Would the project:				
a) Violate any water quality standards or waste discharge requirements?				X
b) Substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of pre-existing nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted)?				X
c) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, in a manner which would result in substantial erosion or siltation on- or off-site?			X	
d) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or off-site?			X	
e) Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff?			X	
f) Otherwise substantially degrade water quality?			X	
g) Place housing within a 100-year flood hazard area as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map?				X
h) Place within a 100-year flood hazard area structures which would impede or redirect flood flows?				X
i) Expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam?				X
j) Result in inundation by seiche, tsunami, or mudflow?				X
Section 3.10 Land Use and Planning. Would the project:				
a) Physically divide an established community, or otherwise result in an incompatible land use?				X

Table 2
Summary (Initial Study Checklist)

Environmental Issues Area Examined	Potentially Significant Impact	Less Than Significant With Mitigation	Less Than Significant Impact	No Impact
b) Conflict with an applicable land use plan, policy, or regulation of an agency with jurisdiction over the project (including, but not limited to the general plan, specific plan, local coastal program, or zoning ordinance) adopted for the purpose of avoiding or mitigating an environmental effect?				X
c) Conflict with any applicable habitat conservation plan or natural community conservation plan?				X
Section 3.11 Mineral Resources. <i>Would the project:</i>				
a) Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?				X
b) Result in the loss of availability of a locally-important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?				X
Section 3.12 Noise. <i>Would the project:</i>				
a) Result in exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?			X	
b) Result in the exposure of persons to or the generation of excessive groundborne noise levels?			X	
c) Result in a substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project?			X	
d) Result in a substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project?			X	
e) For a project located with an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?				X
f) For a project within the vicinity of a private airstrip, would the project expose people residing or working in the project area to excessive noise levels?				X
Section 3.13 Population and Housing. <i>Would the project:</i>				
a) Induce substantial population growth in an area, either directly (e.g., by proposing new homes and businesses) or indirectly (e.g., through extension of roads or other infrastructure)?			X	
b) Displace substantial numbers of existing housing, necessitating the construction of replacement housing elsewhere?				X
c) Displace substantial numbers of people, necessitating the construction of replacement housing elsewhere?				X

Table 2
Summary (Initial Study Checklist)

Environmental Issues Area Examined	Potentially Significant Impact	Less Than Significant With Mitigation	Less Than Significant Impact	No Impact
Section 3.14 Public Services. <i>Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, the construction of which would cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:</i>				
a) Fire protection services?			X	
b) Police protection services?			X	
c) School services?			X	
d) Other governmental services?			X	
Section 3.15 Recreation. <i>Would the project:</i>				
a) Increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?			X	
b) Include recreational facilities or require the construction or expansion of recreational facilities which might have an adverse physical effect on the environment?			X	
Section 3.16 Transportation and Circulation. <i>Would the project:</i>				
a) Conflict with an applicable plan, ordinance or policy establishing measures of effectiveness for the performance of the circulation system, taking into account all modes of transportation including mass transit and non-motorized travel and relevant components of the circulation system, including but not limited to intersections, streets, highways and freeways, pedestrian and bicycle paths, and mass transit?			X	
b) Conflict with an applicable congestion management program, including, but not limited to level of service standards and travel demand measures, or other standards established by the county congestion management agency for designated roads or highways?				X
c) Result in a change in air traffic patterns, including either an increase in traffic levels or a change in location that results in substantial safety risks?				X
d) Substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?				X
e) Result in inadequate emergency access?				X
f) Conflict with adopted policies, plans, or programs regarding public transit, bicycle, or pedestrian facilities, or otherwise decrease the performance or safety of such facilities?				X

Table 2
Summary (Initial Study Checklist)

Environmental Issues Area Examined	Potentially Significant Impact	Less Than Significant With Mitigation	Less Than Significant Impact	No Impact
Section 3.17 Tribal Cultural Resources. <i>Would the project cause a substantial adverse change in the significance of a tribal cultural resource, defined in Public Resources Code section 21074 as either a site, feature, place, cultural landscape that is geographically defined in terms of the size and scope of the landscape, sacred place, or object with cultural value to a California Native American tribe, and that is:</i>				
a) Listed or eligible for listing in the California Register of Historical Resources, or in a local register of historical resources as defined in Public Resources Code §5020.1(k)?			X	
b) A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Public Resources Code Section 5024.1.? In applying the criteria set forth in subdivision (c) of Public Resource Code Section 5024.1, the lead agency shall consider the significance of the resource to a California Native American tribe.			X	
Section 3.18 Utilities. <i>Would the project:</i>				
a) Exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board?			X	
b) Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?			X	
c) Require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?			X	
d) Have sufficient water supplies available to serve the project from existing entitlements and resources, or are new or expanded entitlements needed?			X	
e) Result in a determination by the wastewater treatment provider which serves or may serve the project that it has inadequate capacity to serve the project's projected demand in addition to the provider's existing commitments?			X	
f) Be served by a landfill with insufficient permitted capacity to accommodate the project's solid waste disposal needs?			X	
g) Comply with federal, state, and local statutes and regulations related to solid waste?				X
Section 3.19 Mandatory Findings of Significance. <i>The approval and subsequent implementation of the proposed project:</i>				
a) Will not have the potential to degrade the quality of the environment, with the implementation of the recommended standard conditions and mitigation measures included herein.				X
b) Will not have the potential to achieve short-term goals to the disadvantage of long-term environmental goals, with the implementation of the recommended standard conditions and mitigation measures referenced herein.				X

Table 2
Summary (Initial Study Checklist)

Environmental Issues Area Examined	Potentially Significant Impact	Less Than Significant With Mitigation	Less Than Significant Impact	No Impact
c) Will not have impacts that are individually limited, but cumulatively considerable, when considering planned or proposed development in the immediate vicinity, with the implementation of the recommended standard conditions and mitigation measures contained herein.				X
d) Will not have environmental effects that will adversely affect humans, either directly or indirectly, with the implementation of the recommended standard conditions and mitigation measures contained herein.				X



7. CONCLUSION

The following findings can be made regarding the Mandatory Findings of Significance set forth in Section 15065 of the CEQA Guidelines based on the results of this environmental assessment:

- The approval and subsequent implementation of the proposed project *will not* have the potential to degrade the quality of the environment.
- The approval and subsequent implementation of the proposed project *will not* have the potential to achieve short-term goals to the disadvantage of long-term environmental goals.
- The approval and subsequent implementation of the proposed project *will not* have impacts that are individually limited, but cumulatively considerable, when considering planned or proposed development in the immediate vicinity.
- The approval and subsequent implementation of the proposed project *will not* have environmental effects that will adversely affect humans, either directly or indirectly.

INITIAL STUDY & NEGATIVE DECLARATION

OLD TOWN PLACENTIA REVITALIZATION PROJECT

PLACENTIA, CALIFORNIA



PLACENTIA
Rich Heritage, Bright Future

LEAD AGENCY:

**CITY OF PLACENTIA
DEVELOPMENT SERVICES DEPARTMENT,
PLANNING DIVISION
401 EAST CHAPMAN AVENUE
PLACENTIA, CALIFORNIA 92870**

REPORT PREPARED BY:

**BLODGETT BAYLOSIS ENVIRONMENTAL PLANNING
16388 E. COLIMA ROAD, SUITE 206J
HACIENDA HEIGHTS, CALIFORNIA 91745**

MAY 22, 2017

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NEGATIVE DECLARATION

PROJECT NAME: Old Town Placentia Revitalization Project.

ADDRESS: The Old Town Placentia Revitalization Project applies to an L-shaped Planning Area that is located in the southwestern portion of the City of Placentia. The Planning Area is centered along Bradford Avenue south of Chapman Avenue and Santa Fe Avenue from Murray Street on the west to Alta Street on the east. The portion of the Planning Area centered on Bradford Avenue extends east to Alta Street and west to Main Street, south of Chapman Avenue. The portion of the Planning Area centered on Santa Fe Avenue extends from Murray Street on the west, Aguirre Lane (and extension of Aguirre Lane) on the north, the Burlington Northern-Santa Fe (BNSF) railroad tracks to the south, and Alta Street to the east. The Planning Area is just over 32 acres in size and is located north of the Burlington Northern-Santa Fe (BNSF) railroad tracks and south of Chapman Avenue in the City of Placentia. The Old Town Placentia Planning area is bounded by Chapman Avenue in the north, the Burlington Northern and Santa Fe Railway train tracks in the south, Alta Street in the east, and Murray Street in the west.

Within the Planning Area are various sub-areas that define the heights of buildings, the uses, and the architecture in each sub-area. These standards are all described in the proposed zone change, which will change the zoning of the project area from "SF-C" (Santa Fe Commercial), "C-1" (Neighborhood Commercial), "C-2" (Community Commercial), and "R-2" (Low-Medium Density Residential) to "Old Town," which further divides the Old Town into five planning sub-areas, which include Main Street (Two- Three- and Four-Story Height Sub-areas), Village (Three-Story Height Sub-area), Mixed-Use (Four-Story Height District), High-Density Residential (Four-Story Height Sub-area), and Public Facilities (Five Story Height Sub-area). The General Plan designations will also be changed to reflect the land uses contemplated within the Old Town Revitalization Plan. A portion of two zoning sub-areas will also have an R-2 Overlay, wherein the existing underlying residential uses remain conforming. These are depicted in Exhibit 2-5. Finally, the residential development within the proposed High Density Residential zone will be "by right," pursuant to the State of California housing law. An aerial photograph of the Planning Area is provided in Exhibit 2-4.

CITY AND COUNTY: Placentia, Orange County.

APPLICANT: City of Placentia, 401 East Chapman Avenue, Placentia, California 92870.

PROJECT: The proposed Old Town Placentia Revitalization Project involves the adoption and subsequent implementation of the Old Town Placentia Revitalization Plan document, the Old Town Placentia Revitalization Plan Development Standards (i.e. the zoning text amendment), and the Streetscape Master Plan. The

INITIAL STUDY & NEGATIVE DECLARATION • CITY OF PLACENTIA
OLD TOWN PLACENTIA REVITALIZATION PROJECT

Revitalization Project will also involve an amendment to the General Plan land use map to reflect the land uses contemplated within the Revitalization Plan document. The Revitalization Project is anticipated to facilitate new sustainable development, especially residential and retail, in a mixed-use setting. The Planning Area is adjacent to the recently approved Metrolink Station and parking structure in the Old Town area and the recently approved Transit Oriented Development (TOD) south of the railroad along Crowther Avenue. The purpose of the Revitalization Project is to enhance the physical environment in the City's Old Town aimed at creating a lively destination to support the current economic base, create a town center for Placentia, and better connect to adjacent neighborhoods and surrounding cities.

FINDINGS:

The City of Placentia determined that a *Negative Declaration* is the appropriate California Environmental Quality Act (CEQA) document for the proposed project. The following findings may be made based on the analysis included in the attached Initial Study:

- The proposed project *will not* have the potential to degrade the quality of the environment.
- The proposed project *will not* have the potential to achieve short-term goals to the disadvantage of long-term environmental goals.
- The proposed project *will not* have impacts that are individually limited, but cumulatively considerable, when considering planned or proposed development in the city.
- The proposed project *will not* have environmental effects that will adversely affect humans, either directly or indirectly.

Andrew C. Domiles for JOSEPH LAMMEYRT

5/22/17

Signature
City of Placentia Development Services Department

Date

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SECTION 1 INTRODUCTION

1.1 PURPOSE OF INITIAL STUDY

The proposed Old Town Placentia Revitalization Project involves the adoption and subsequent implementation of the Old Town Placentia Revitalization Plan document, the Old Town Placentia Revitalization Plan Development Standards (i.e. the zoning text amendment), and the Streetscape Master Plan. The Revitalization Project will also involve an amendment to the General Plan land use map to reflect the land uses contemplated within the Revitalization Plan document. The Old Town Placentia Revitalization Project (referred to herein-after as the “Revitalization Project”) applies to an L-shaped area that is located in the southwestern portion of the City of Placentia. The geographic area governed by the Old Town Revitalization Project (referred to herein after as the “Planning Area” applies to the same L-shaped area. The Planning Area is centered along Bradford Avenue south of Chapman Avenue and Santa Fe Avenue from Murray Street on the west to Alta Street on the east. The portion of the Planning Area centered on Bradford Avenue extends east to Alta Street and west to Main Street, south of Chapman Avenue. The portion of the Planning Area centered on Santa Fe Avenue extends from Murray Street on the west, Aguirre Lane (and extension of Aguirre Lane) on the north, the Burlington Northern-Santa Fe (BNSF) railroad tracks to the south, and Alta Street to the east. The Planning Area is just over 32 acres in size and is located north of the Burlington Northern-Santa Fe (BNSF) railroad tracks and south of Chapman Avenue in the City of Placentia. The Old Town Placentia Planning area is bounded by Chapman Avenue in the north, the Burlington Northern and Santa Fe Railway train tracks in the south, Alta Street in the east, and Murray Street in the west.

The Revitalization Project is anticipated to facilitate development through a zone change with new specific development standards for each new planning sub-area of the Old Town project area, especially residential and retail in a mixed-use setting, proximate to the newly approved Metrolink Station and parking structure in the Old Town area and the recently approved Transit Oriented Development (TOD) south of the railroad along Crowther Avenue. The purpose of the Revitalization Project is to enhance the physical environment in the City’s Old Town aimed at creating a lively destination to support the current economic base, create a town center for Placentia, and better connect to adjacent neighborhoods and surrounding cities.¹ Finally, the residential development within the proposed High Density Residential zone will be “by right,” pursuant to the State of California housing law. The Revitalization Project’s implementation, through changes to the Zoning Code and General Plan, is critical in aiding in the realization of local and regional goals related to sustainable and infill development.

The adoption and subsequent implementation of the Revitalization Project is considered to be a project under the California Environmental Quality Act (CEQA).² The City of Placentia is the designated *Lead Agency* for the proposed project and the City will be responsible for the project’s environmental review. Section 21067 of CEQA defines a Lead Agency as the public agency that has the principal responsibility for

¹ Placentia, City of. *Old Town Placentia Revitalization Plan*. May 2017.

² California, State of. *Title 14. California Code of Regulations. Chapter 3. Guidelines for the Implementation of the California Environmental Quality Act*. As Amended 1998 (CEQA Guidelines). § 15060 (b).

carrying out or approving a project that may have a significant effect on the environment.³ The project Applicant is the City of Placentia, Development Services Department, Planning Division, 401 East Chapman Avenue, Placentia, California 92870. As part of the proposed project's environmental review, the City of Placentia authorized the preparation of this Initial Study.⁴ The primary purpose of CEQA is to ensure that decision-makers and the public understand the environmental implications of a specific action or project. The purpose of this Initial Study is to ascertain whether the proposed project will have the potential for significant adverse impacts on the environment.

Pursuant to the CEQA Guidelines, additional purposes of this Initial Study include the following:

- To provide the City of Placentia with information to use as the basis for deciding whether to prepare an environmental impact report (EIR), mitigated negative declaration, or negative declaration for a project;
- To facilitate the project's environmental assessment early in the design and development of the proposed project;
- To eliminate unnecessary EIRs; and,
- To determine the nature and extent of any impacts associated the proposed project.

Although this Initial Study was prepared with consultant support, the analysis, conclusions, and findings made as part of its preparation, fully represent the independent judgment and position of the City of Placentia, in its capacity as the Lead Agency. The City also determined, as part of this Initial Study's preparation, that a Negative Declaration is the appropriate environmental document for the project's environmental review pursuant to CEQA. The implementation of the Revitalization Project will require a General Plan Amendment and a Zoning Ordinance Amendment. The revisions to both the City of Placentia General Plan and the City of Placentia Zoning Ordinance will permit certain types of land use development within the geographic area governed by the Revitalization Project. The sustainable and infill nature of this new development permitted under the Revitalization Project will help reduce vehicle miles traveled (VMTs), improve air quality, provide new housing opportunities consistent with regional planning initiatives, and promote sustainable development in Placentia's original downtown business district. Finally, the Revitalization Project includes policies and measures that will mitigate project-specific impacts when the new development occurs.

This Initial Study and the *Notice of Intent to Adopt a Negative Declaration* will be forwarded to responsible agencies, trustee agencies, and the public for review and comment. A 20-day public review period will be provided to allow these entities and other interested parties to comment on the proposed

³ California, State of. *California Public Resources Code. Division 13, Chapter 2.5. Definitions. As Amended 2001.* § 21067.

⁴ Ibid.(CEQA Guidelines) § 15050.

project and the findings of this Initial Study.⁵ Questions and/or comments should be submitted to the following contact person:

Joseph M. Lambert, Director of Development Services
City of Placentia, Development Services Department, Planning Division
401 East Chapman Avenue
Placentia, California 92870
714-993-8124

1.2 INITIAL STUDY'S ORGANIZATION

The following annotated outline summarizes the contents of this Initial Study:

- *Section 1 Introduction* provides the procedural context surrounding this Initial Study's preparation and insight into its composition.
- *Section 2 Project Description* provides an overview of the existing environment as it relates to the Planning Area and describes the proposed project's physical and operational characteristics.
- *Section 3 Environmental Analysis* includes an analysis of potential impacts associated with the proposed project's construction and the subsequent occupancy.
- *Section 4 Findings* indicates the conclusions of the environmental analysis and the mandatory findings of significance.
- *Section 5 References* identifies the sources used in the preparation of this Initial Study.

1.3 INITIAL STUDY CHECKLIST

The environmental analysis provided in Section 3 of this Initial Study indicates that the adoption and subsequent implementation of the Old Town Placentia Revitalization Project through Zone and General Plan changes will not result in any significant adverse unmitigable impacts on the environment. For this reason, the City of Placentia determined that a Negative Declaration is the appropriate CEQA document for the proposed project. The findings of this Initial Study are summarized in Table 1-1 provided below and on the following pages.

⁵ California, State of. *California Public Resources Code. Division 13, Chapter 2.5. Definitions.* as Amended 2000. *Chapter 2.6, Section 2109(b).* 2000.

**Table 1-1
 Summary (Initial Study Checklist)**

Environmental Issues Area Examined	Potentially Significant Impact	Less Than Significant With Mitigation	Less Than Significant Impact	No Impact
Section 3.1 Aesthetics. <i>Would the project:</i>				
a) Have a substantial adverse effect on a scenic vista?			X	
b) Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway?				X
c) Substantially degrade the existing visual character or quality of the site and its surroundings?				X
d) Create a new source of substantial light or glare which would adversely affect day or nighttime views in the area?				X
Section 3.2 Agriculture and Forestry Resources. <i>Would the project:</i>				
a) Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?				X
b) Conflict with existing zoning for agricultural use, or a Williamson Act Contract?				X
c) Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code §12220[g]), timberland (as defined in Public Resources Code §4526), or timberland zoned production (as defined in Government Code §51104[g])?				X
d) Result in the loss of forest land or conversion of forest land to non-forest use?				X
e) Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland, to non-agricultural use or conversion of forest land to non-forest use?				X
Section 3.3 Air Quality. <i>Would the project:</i>				
a) Conflict with or obstruct implementation of the applicable air quality plan?			X	
b) Violate any air quality standard or contribute substantially to an existing or projected air quality violation?			X	
c) Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors)?			X	
d) Expose sensitive receptors to substantial pollutant concentrations?			X	

**Table 1-1
 Summary (Initial Study Checklist)**

Environmental Issues Area Examined	Potentially Significant Impact	Less Than Significant With Mitigation	Less Than Significant Impact	No Impact
e) Create objectionable odors affecting a substantial number of people?				X
Section 3.4 Biological Resources. <i>Would the project:</i>				
a) Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Wildlife or U. S. Fish and Wildlife Service?				X
b) Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations or by the California Department of Fish and Wildlife or U.S. Fish and Wildlife Service?				X
c) Have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?				X
d) Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?				X
e) Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?				X
f) Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?				X
Section 3.5 Cultural Resources. <i>Would the project:</i>				
a) Cause a substantial adverse change in the significance of a historical resource as defined in §15064.5 of the CEQA Guidelines?			X	
b) Cause a substantial adverse change in the significance of an archaeological resource pursuant to §15064.5 of the CEQA Guidelines?			X	
c) Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?			X	
d) Disturb any human remains, including those interred outside of dedicated cemeteries?			X	
Section 3.6 Geology and Soils. <i>Would the project:</i>				
a) Expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving rupture of a known earthquake fault (as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault), strong seismic ground shaking, seismic-related ground failure, including liquefaction, or landslides?			X	

**Table 1-1
 Summary (Initial Study Checklist)**

Environmental Issues Area Examined	Potentially Significant Impact	Less Than Significant With Mitigation	Less Than Significant Impact	No Impact
b) Result in substantial soil erosion or the loss of topsoil?			X	
c) Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction, or collapse?			X	
d) Be located on expansive soil, as defined in Table 18-1-B of the California Building Code (2012), creating substantial risks to life or property?				X
e) Have soils incapable of adequately supporting the use of septic tanks or alternative wastewater disposal systems where sewers are not available for the disposal of wastewater?				X
Section 3.7 Greenhouse Gas Emissions. <i>Would the project</i>				
a) Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?			X	
b) Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing emissions of greenhouse gases?				X
Section 3.8 Hazards and Hazardous Materials. <i>Would the project:</i>				
a) Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?			X	
b) Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?			X	
c) Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?				X
d) Be located on a site which is included on a list of hazardous material sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?				X
e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or a public use airport, would the project result in a safety hazard for people residing or working in the project area?				X
f) For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing or working in the project area?				X
g) Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?				X

**Table 1-1
 Summary (Initial Study Checklist)**

Environmental Issues Area Examined	Potentially Significant Impact	Less Than Significant With Mitigation	Less Than Significant Impact	No Impact
h) Expose people or structures to a significant risk of loss, injury or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands?				X
Section 3.9 Hydrology and Water Quality. <i>Would the project:</i>				
a) Violate any water quality standards or waste discharge requirements?				X
b) Substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of pre-existing nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted)?				X
c) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, in a manner which would result in substantial erosion or siltation on- or off-site?			X	
d) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or off-site?			X	
e) Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff?			X	
f) Otherwise substantially degrade water quality?			X	
g) Place housing within a 100-year flood hazard area as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map?				X
h) Place within a 100-year flood hazard area structures which would impede or redirect flood flows?				X
i) Expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam?				X
j) Result in inundation by seiche, tsunami, or mudflow?				X
Section 3.10 Land Use and Planning. <i>Would the project:</i>				
a) Physically divide an established community, or otherwise result in an incompatible land use?				X
b) Conflict with an applicable land use plan, policy, or regulation of an agency with jurisdiction over the project (including, but not limited to the general plan, specific plan, local coastal program, or zoning ordinance) adopted for the purpose of avoiding or mitigating an environmental effect?				X

**Table 1-1
 Summary (Initial Study Checklist)**

Environmental Issues Area Examined	Potentially Significant Impact	Less Than Significant With Mitigation	Less Than Significant Impact	No Impact
c) Conflict with any applicable habitat conservation plan or natural community conservation plan?				X
Section 3.11 Mineral Resources. <i>Would the project:</i>				
a) Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?				X
b) Result in the loss of availability of a locally-important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?				X
Section 3.12 Noise. <i>Would the project:</i>				
a) Result in exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?			X	
b) Result in the exposure of persons to or the generation of excessive groundborne noise levels?			X	
c) Result in a substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project?			X	
d) Result in a substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project?			X	
e) For a project located with an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?				X
f) For a project within the vicinity of a private airstrip, would the project expose people residing or working in the project area to excessive noise levels?				X
Section 3.13 Population and Housing. <i>Would the project:</i>				
a) Induce substantial population growth in an area, either directly (e.g., by proposing new homes and businesses) or indirectly (e.g., through extension of roads or other infrastructure)?			X	
b) Displace substantial numbers of existing housing, necessitating the construction of replacement housing elsewhere?				X
c) Displace substantial numbers of people, necessitating the construction of replacement housing elsewhere?				X
Section 3.14 Public Services. <i>Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, the construction of which would cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:</i>				
a) Fire protection services?			X	

**Table 1-1
 Summary (Initial Study Checklist)**

Environmental Issues Area Examined	Potentially Significant Impact	Less Than Significant With Mitigation	Less Than Significant Impact	No Impact
b) Police protection services?			X	
c) School services?			X	
d) Other governmental services?			X	
Section 3.15 Recreation. <i>Would the project:</i>				
a) Increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?			X	
b) Include recreational facilities or require the construction or expansion of recreational facilities which might have an adverse physical effect on the environment?			X	
Section 3.16 Transportation and Circulation. <i>Would the project:</i>				
a) Conflict with an applicable plan, ordinance or policy establishing measures of effectiveness for the performance of the circulation system, taking into account all modes of transportation including mass transit and non-motorized travel and relevant components of the circulation system, including but not limited to intersections, streets, highways and freeways, pedestrian and bicycle paths, and mass transit?			X	
b) Conflict with an applicable congestion management program, including, but not limited to level of service standards and travel demand measures, or other standards established by the county congestion management agency for designated roads or highways?				X
c) Result in a change in air traffic patterns, including either an increase in traffic levels or a change in location that results in substantial safety risks?				X
d) Substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?				X
e) Result in inadequate emergency access?				X
f) Conflict with adopted policies, plans, or programs regarding public transit, bicycle, or pedestrian facilities, or otherwise decrease the performance or safety of such facilities?				X
Section 3.17 Tribal Cultural Resources. <i>Would the project cause a substantial adverse change in the significance of a tribal cultural resource, defined in Public Resources Code section 21074 as either a site, feature, place, cultural landscape that is geographically defined in terms of the size and scope of the landscape, sacred place, or object with cultural value to a California Native American tribe, and that is:</i>				
a) Listed or eligible for listing in the California Register of Historical Resources, or in a local register of historical resources as defined in Public Resources Code §5020.1(k)?			X	

**Table 1-1
 Summary (Initial Study Checklist)**

Environmental Issues Area Examined	Potentially Significant Impact	Less Than Significant With Mitigation	Less Than Significant Impact	No Impact
b) A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Public Resources Code Section 5024.1.? In applying the criteria set forth in subdivision (c) of Public Resource Code Section 5024.1, the lead agency shall consider the significance of the resource to a California Native American tribe.			X	
Section 3.18 Utilities. <i>Would the project:</i>				
a) Exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board?			X	
b) Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?			X	
c) Require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?			X	
d) Have sufficient water supplies available to serve the project from existing entitlements and resources, or are new or expanded entitlements needed?			X	
e) Result in a determination by the wastewater treatment provider which serves or may serve the project that it has inadequate capacity to serve the project's projected demand in addition to the provider's existing commitments?			X	
f) Be served by a landfill with insufficient permitted capacity to accommodate the project's solid waste disposal needs?			X	
g) Comply with federal, state, and local statutes and regulations related to solid waste?				X
Section 3.19 Mandatory Findings of Significance. <i>The approval and subsequent implementation of the proposed project:</i>				
a) Will not have the potential to degrade the quality of the environment, with the implementation of the recommended standard conditions and mitigation measures included herein.				X
b) Will not have the potential to achieve short-term goals to the disadvantage of long-term environmental goals, with the implementation of the recommended standard conditions and mitigation measures referenced herein.				X
c) Will not have impacts that are individually limited, but cumulatively considerable, when considering planned or proposed development in the immediate vicinity, with the implementation of the recommended standard conditions and mitigation measures contained herein.				X

**Table 1-1
 Summary (Initial Study Checklist)**

Environmental Issues Area Examined	Potentially Significant Impact	Less Than Significant With Mitigation	Less Than Significant Impact	No Impact
<p>d) Will not have environmental effects that will adversely affect humans, either directly or indirectly, with the implementation of the recommended standard conditions and mitigation measures contained herein.</p>				X



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SECTION 2 PROJECT DESCRIPTION

2.1 PROJECT OVERVIEW

The proposed Old Town Placentia Revitalization Project involves the adoption and subsequent implementation of the Old Town Placentia Revitalization Plan document, the Old Town Placentia Revitalization Plan Development Standards (i.e. the zoning text amendment), and the Streetscape Master Plan. The Revitalization Project will also involve an amendment to the General Plan land use map to reflect the land uses contemplated within the Revitalization Plan document. The Old Town Placentia Revitalization Project applies to an L-shaped area that is located in the southwestern portion of the City of Placentia. The Planning Area is focused on Bradford Avenue and Santa Fe Avenue. Within the Planning Area are various sub-areas that include planned commercial and high-density residential uses, which are depicted in Exhibit 2-5. An aerial photograph of the Planning Area is provided in Exhibit 2-4. The Planning Area is just over 32 acres in size and is located north of the Burlington Northern-Santa Fe (BNSF) railroad tracks and south of Chapman Avenue. The Revitalization Project is anticipated to facilitate development, especially residential and retail, in a mixed-use setting, proximate to the newly approved Metrolink Station and parking structure in the Old Town area and the proposed Transit Oriented Development (TOD) south of the railroad along Crowther Avenue. The purpose of the Revitalization Project is to enhance the physical environment in the City's Old Town aimed at creating a lively destination to support the current economic base, create a town center for Placentia, and better connect to adjacent neighborhoods and surrounding cities.⁶ Finally, the residential development within the proposed High Density Residential zone will be "by right," pursuant to the State of California housing law. The Revitalization Project's implementation, through changes to the Zoning Code and General Plan, is critical in aiding in the realization of local and regional goals related to sustainable and infill development.

2.2 PROJECT LOCATION

The Planning Area for the Old Town Placentia Revitalization Project is located within the corporate boundaries of the City of Placentia. The City of Placentia is located approximately 7.5 miles north of downtown Santa Ana and 23 miles southeast of downtown Los Angeles. The City is bounded by the cities of Brea on the north, Anaheim on the south, Yorba Linda on the east, and Fullerton on the west.⁷ The location of Placentia in a regional context is shown in Exhibit 2-1. A citywide map is provided in Exhibit 2-2. The Planning Area is illustrated in Exhibit 2-3.

The Old Town Placentia Revitalization Project applies to an L-shaped Planning Area that is located in the southwestern portion of the City of Placentia. The Planning Area is centered along Bradford Avenue south of Chapman Avenue and Santa Fe Avenue from Murray Street on the west to Alta Street on the east. The portion of the Planning Area centered on Bradford Avenue extends east to Alta Street and west to Main Street, south of Chapman Avenue. The portion of the Planning Area centered on Santa Fe Avenue extends from Murray Street on the west, Aguirre Lane (and extension of Aguirre Lane) on the north, the Burlington Northern-Santa Fe (BNSF) railroad tracks to the south, and Alta Street to the east. The Planning Area is

⁶ Placentia, City of. *Old Town Placentia Revitalization Plan*. May 2017.

⁷ Google Earth. Website Accessed April 27, 2017.

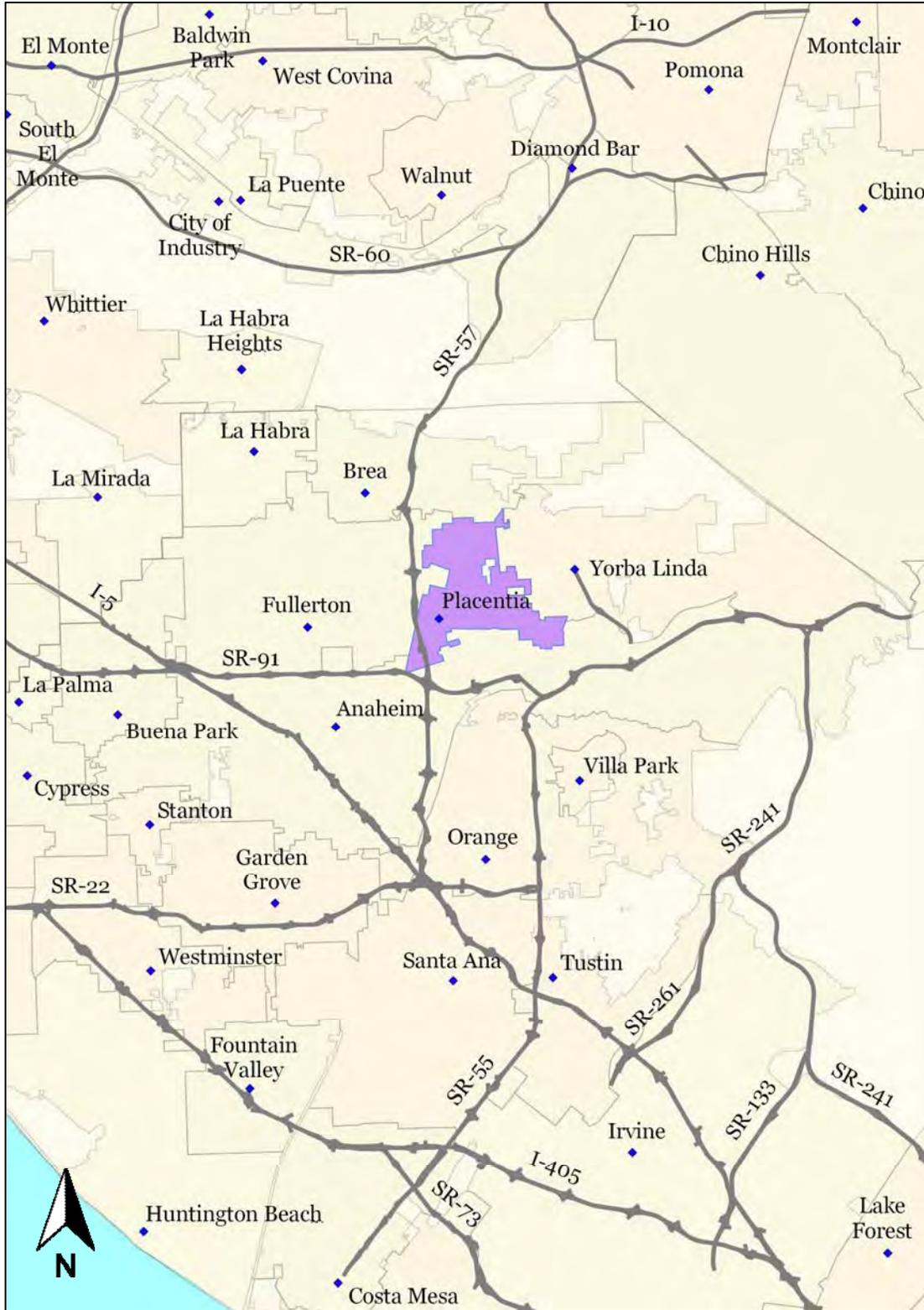


EXHIBIT 2-1
REGIONAL LOCATION

Source: Quantum GIS

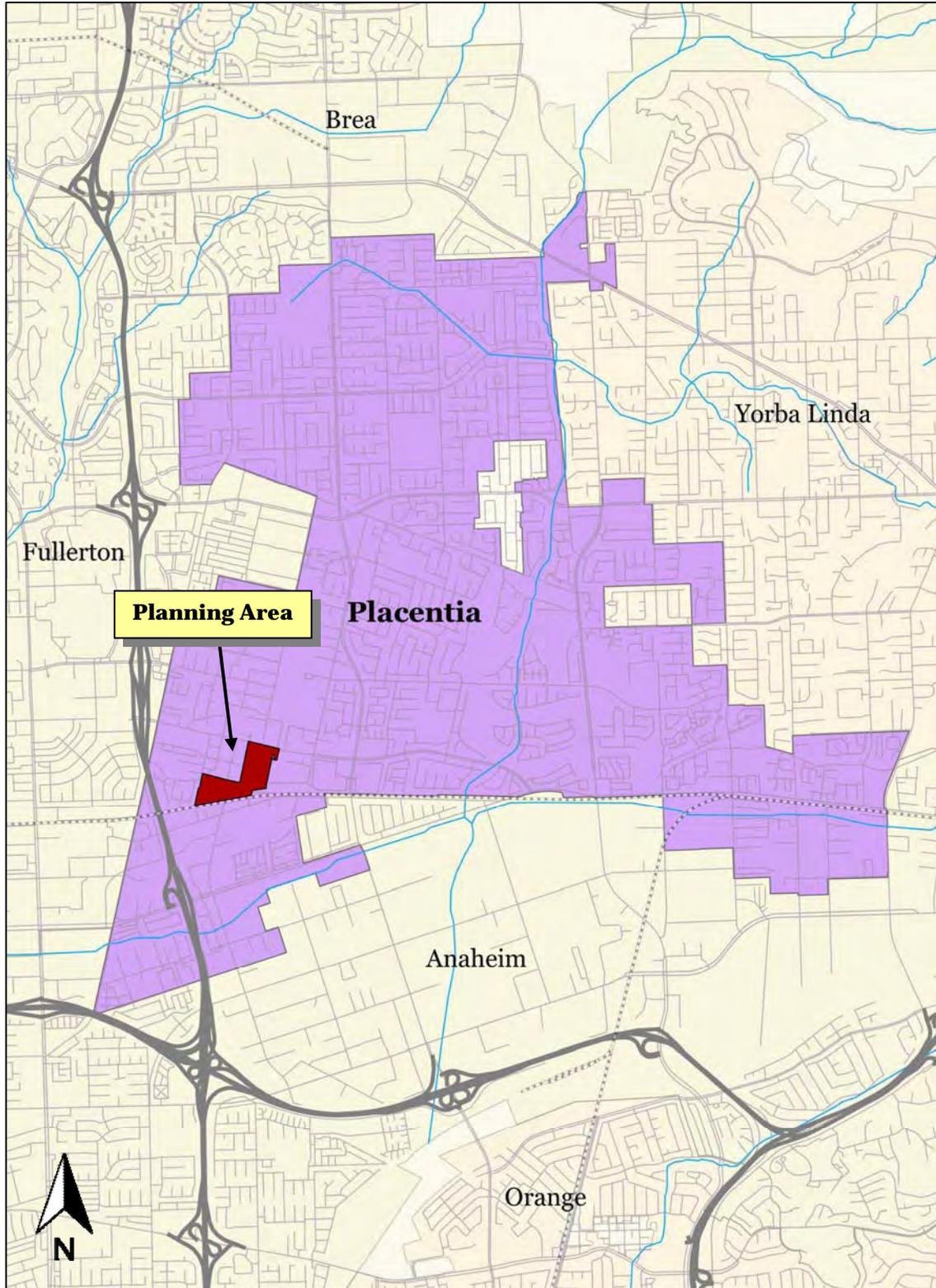


EXHIBIT 2-2
CITYWIDE MAP
Source: Quantum GIS



EXHIBIT 2-3
PLANNING AREA
Source: Quantum GIS

just over 32 acres in size and is located north of the Burlington Northern-Santa Fe (BNSF) railroad tracks and south of Chapman Avenue. The Old Town Placentia Planning area is bounded by Chapman Avenue in the north, the Burlington Northern and Santa Fe Railway train tracks in the south, Alta Street in the east, and Murray Street in the west.

2.3 ENVIRONMENTAL SETTING

Old Town Placentia is the City's original central business district founded in 1910 near the original Placentia train depot and the citrus packing houses. The Planning Area primarily consists of commercial uses along Santa Fe Avenue and Bradford Avenue, with residential uses primarily along the surrounding streets. The Planning Area is located north of the Burlington Northern-Santa Fe (BNSF) railroad tracks and proximate to the newly approved Metrolink Station and parking structure in the Old Town area and the recently approved Transit Oriented Development (TOD) south of the railroad along Crowther Avenue. The Metrolink Station will have two platforms, one north of the railroad and one south of the railroad. A pedestrian over-crossing bridge and a grade-separated road at Melrose Street have been completed in anticipation of the new transit station.⁸ Together with the new TOD zoning to the south, this proposed rezoning of the Old Town Placentia area represents the City's planning efforts to accommodate and capitalize on the new Metrolink Station.

The Planning Area consists of over 140 parcels made up of a diverse mix of commercial land uses, surrounded by residential neighborhoods to the west, east, and north. Many parcels are less than 5,000 square feet in size and this contributes to the small scale of the Old Town. Notable uses within the Planning Area and in the surrounding area include four community churches/church structures, the City's senior center, a fire station, and two parks. The Planning Area does not include any schools, though Kraemer Middle School and Valencia High School are located north of Chapman Avenue. California State University Fullerton is located approximately two miles to the west, in the City of Fullerton. The Planning Area is principally zoned "SF-C" Santa Fe Commercial District and there are other zones in the planning area: "C-1," "C-2," and "R-2" zones.⁹ An aerial photograph of the Planning Area is provided in Exhibit 2-4. The planned zoning designations for the Planning Area are shown in Exhibit 2-5.

2.4 PROJECT DESCRIPTION

BACKGROUND FOR PLANNING

The Old Town Placentia area suffered from disinvestment and economic decline with the widespread growth of Orange County's suburbanization from the 1960's to the 1990's. Nevertheless, existing property owners, residents, and businesses have kept the area active with a mix of small businesses, restaurants, churches, and public venues. This activity has helped this area retain its unique small scale environment which, with the new Metrolink Station, has the potential to become a vibrant commercial area. Market data indicates that population groups of all types are looking for small scale, walkable urban centers

⁸ Placentia, City of. *Old Town Placentia Revitalization Plan*. May 2017.

⁹ Ibid.



EXHIBIT 2-4
AERIAL PHOTOGRAPH OF THE PLANNING AREA
SOURCE: GOOGLE EARTH

located around transit to live, work, and play. This is occurring in Orange County with new, more compact, and transit-oriented developments and historic reuse districts emerging in Anaheim, Orange, Fullerton, and Santa Ana. Coupled with the proposed Transit Oriented Development (TOD), including a new Metrolink transit station south of the railroad along Crowther Avenue, there is a unique opportunity for Old Town to see revitalization. The goals and objectives of the Old Town Revitalization Plan are expected to be implemented under a partnership between City Elected Officials, City Departments, regional agencies, local businesses, stakeholders, residents, and community organizations. The rezoning of the area and the streetscape plan are expected to be adopted by Summer of 2017 and will assist in attracting development to the area in the shorter term.

FORMAT AND PURPOSE OF THE REVITALIZATION PLAN, DEVELOPMENT STANDARDS, AND STREETSCAPE MASTER PLAN

The City's revitalization efforts for the Old Town area center on three principal documents: the Old Town Placentia Revitalization Plan document, the Streetscape Master Plan, and the Zone Change, which sets out new and specific Development Standards for the Planning Area. The Old Town Revitalization Plan document is organized to provide a framework for private and public realm improvements in the Planning Area.¹⁰ The Plan is organized as follows:

- *Chapter 1: Introduction* explains the Old Town Placentia Revitalization Plan's background and purpose, identifies the Planning Area, describes citywide planning efforts, describes community outreach efforts, and outlines the organization of the Revitalization Plan document. This chapter also sets forth the administration of the Revitalization Plan.
- *Chapter 2: Vision* describes the vision for revitalization of the Planning Area. This chapter also outlines the goals and policies that establish the framework for the recommendations contained in the Revitalization Plan document.
- *Chapter 3: Mobility* details the circulation improvements for the Planning Area, including automobile, transit, pedestrian, and bicycle modes. This chapter also displays images of street cross-sections and streetscape plans for the Old Town Planning Area. Mobility recommendations within this chapter support the "Complete Streets" concept as well as other policy and design approaches, such as a circulation plan for the Planning Area.
- *Chapter 4: Infrastructure* identifies existing infrastructure and any necessary improvements to the water, sewer, storm water systems, and utility lines. This chapter also provides recommendations relevant to utilities.
- *Chapter 5: Streetscapes* describes the pedestrian realm of the Old Town Planning Area and the current lack of streetscapes. This chapter also details various recommended streetscape element improvements.

¹⁰ Placentia, City of. *Old Town Placentia Revitalization Plan*. May 2017.

- *Chapter 6: Parking* describes the current parking conditions and the recommended parking measures and parking programs.
- *Chapter 7: Land Use* sets forth the zoning, permitted uses, and development and design standards for the Revitalization area. This chapter also details the design guidelines that will shape the character of future development in the Revitalization Project area.
- *Chapter 8: Historic Resources* describes the historic context of the Revitalization Project and identifies the existing historical resources within the Planning Area. This chapter also outlines recommendations for historic preservation.
- *Chapter 9: Business Support* outlines recommendations that will encourage small business growth and active business participation for the revitalization of the Planning Area.
- *Chapter 10: Community Events & Programming* defines the purpose of hosting community events and programming, and lists recommendations to facilitate the hosting of community events and programming.
- *Chapter 11: Maintenance* outlines recommendations for on-going maintenance of the Old Town Planning Area.
- *Chapter 12: Implementation* defines the phasing and intergovernmental coordination associated with the implementation of the Revitalization Project. This chapter also provides a table that lists the various phases of implementation, the timing of the phases, and the responsible agency for each phase.
- *Chapter 13: Sources of Funding* describes the various potential sources of funding available for the Old Town Placentia Revitalization Project.

The purpose of the Old Town Placentia Revitalization Plan document is to provide a framework and guidelines in order to enhance the physical environment in the City's Old Town aimed at creating a lively destination to support and expand the current economic base, create a town center for Placentia, and better connect to adjacent neighborhoods and surrounding cities.¹¹ The Development Standards document (i.e. the zoning amendment) is intended to provide specific development standards for all future development within the Old Town Placentia Planning Area. Specific development standards may include architectural design, building height, and building setback, among many other specifications for development within the different sub-areas of the Planning Area. Finally, the purpose of the Streetscape Master Plan is to identify conceptual designs for the roadway network within the Planning Area.

OLD TOWN PLACENTIA REVITALIZATION PLAN POLICY FRAMEWORK

The Old Town Placentia Revitalization Plan document includes a wide range of goals and policies that will establish the framework for the plan. The following table (Table 2-1) outlines the goals and policies

¹¹ Placentia, City of. *Old Town Placentia Revitalization Plan*. May 2017.

included in the Revitalization Plan document which will serve to mitigate the potential impacts of new development within the Planning Area.

Table 2-1
Goals and Policies of the Old Town Placentia Revitalization Plan

Goals	Policies
<p><i>Goal 1: Make Old Town Placentia a Unique Destination.</i></p>	●A. Celebrate the City’s heritage.
	●B. Make connections to the past.
	●C. Create a walkable village atmosphere.
	●D. Build public gathering spots.
	●E. Enhance the City’s and Old Town’s identity.
	●F. Highlight historic features.
	●G. Improve public and private spaces.
	●H. Create a diversified commercial base.
	●I. Create a shopping destination.
<p><i>Goal 2: Make Old Town Placentia a Pedestrian, People Priority Zone.</i></p>	●A. Encourage and allow compact development.
	●B. Encourage and allow outdoor dining.
	●C. Provide for small passive recreation spots.
	●D. Support inclusivity across all walks of life.
	●E. Enhance community facilities and services.
	●F. Build lively streetscapes.
	●G. Create a small-scale village environment.
	●H. Get people to stay in Old Town longer.
	●I. Limit vehicular trips.
	●J. Create a safe, inviting, pleasant, attractive, and appealing sense of place.
	●K. Tuck away parking where provided.
	●L. Avoid conflicts with motor vehicles.
	●M. Provide opportunities to create intimate walkways.
●N. Make the Old town walk-friendly for all ages and abilities.	
<p><i>Goal 3: Promote Multi Modal and Sustainable Transportation.</i></p>	●A. Connect surrounding suburban and commercial thoroughfares.
	●B. Encourage use and increased ridership of the Metrolink System.

	<ul style="list-style-type: none"> ●C. Move away from the auto-orientation in the layout and land uses.
	<ul style="list-style-type: none"> ●D. Provide hubs for various transportation modes.
<p><i>Goal 4: Support Businesses and Provide for Development Opportunities.</i></p>	<ul style="list-style-type: none"> ●A. Enhance safety and security.
	<ul style="list-style-type: none"> ●B. Develop incentive programs.
	<ul style="list-style-type: none"> ●C. Provide business start-up opportunities.
	<ul style="list-style-type: none"> ●D. Promote the Old Town through media and outreach.
	<ul style="list-style-type: none"> ●E. Provide for infill development, reuse, and preservation.
	<ul style="list-style-type: none"> ●F. Develop a promotional campaign to encourage visits to Old Town Placentia.

Source: Old Town Placentia Revitalization Plan.

PHYSICAL CHANGES RESULTING FROM THE IMPLEMENTATION OF THE REVITALIZATION PROJECT

The Revitalization Project is anticipated to facilitate development, especially retail and residential in a mixed-use setting. The major objectives of the Revitalization Project are to identify land use options that include providing new mixed-uses and increasing housing opportunities and neighborhood-serving retail uses. The Old Town Placentia Revitalization Project will encourage transit oriented development, promote active transportation, reduce vehicles miles traveled, improve access to regional open space resources, and create community benefits. The area’s assets include a walkable layout with its short blocks across a linear street pattern.¹²

The adoption of the proposed Revitalization Project, by itself, will not lead to any new development. The adoption of the Revitalization Project will permit new development within the Planning Area governed by the rezoning with new development standards. The potential new development permitted under the Revitalization Project may include residential and retail land uses in a mixed-use setting, proximate to the newly approved Metrolink Station and parking structure in the Old Town area and the proposed Transit Oriented Development (TOD) south of the railroad along Crowther Avenue. The major objectives of the Revitalization Project are to identify land use options that include providing new mixed-uses, increasing housing opportunities, and increasing neighborhood-serving retail uses.¹³ The Revitalization Project’s implementation is critical in aiding in the realization of local and regional goals related to sustainable and infill development.

The project team, including City Staff and the consultants (traffic, planning, and environmental), identified the maximum amount of new development that could be realized as part of the Revitalization Project’s implementation. No specific new projects are known at this time, though the development projections are

¹² Placentia, City of. *Old Town Placentia Revitalization Plan*. May 2017.

¹³ Ibid.

critical in evaluating the potential environmental impacts including, but not limited to, land use changes, traffic, air quality, population, and public services. The potential new development is outlined below:¹⁴

- 525 residential units;
- 85,000 square feet of commercial (non-retail) uses;
- 40,000 square feet of retail use; and,
- A 50-room hotel.

Based on discussions with the City, the proposed residential uses will be a combination of apartments and townhomes. Single family homes will not be permitted in the area. Currently, the Old Town area contains a mix of single-family residential use and various commercial uses. The majority of properties in the area are developed. Therefore, the majority of the proposed land uses are expected to be infill development. Within the Planning Area are various sub-areas that include planned commercial and high-density residential uses, which are depicted in Exhibit 2-5.

The vision for the land use and private and public land is to keep development compact by providing opportunities for infill that preserves the small-scale character of the historic core. The goal for commercial diversity includes a healthy mix of retail, restaurants, combined with outdoor gathering spaces such as public plazas. The Revitalization Project encourages rehabilitation, additions, and alterations to existing properties, while providing opportunities for new infill development. Compact development and shared parking is encouraged in all of the sub-areas; the emphasis is on the pedestrian, not the vehicle. The Revitalization Plan document provides increased height over what is currently allowed by existing zoning. Office and service uses would be permitted on the second floor of the Planning Area buildings, providing an important element to create a more full service area. Residential development could occur in areas outside of the historic core, or “Main Street” areas.

The Plan prioritizes the “commercial block” style of architecture in its historic and contemporary forms. The style can be adapted for the two-, three-, and larger four-story buildings and includes facades placed at, or close to, the right-of-way line. This style is also a form of building designed for occupancy by retail, service, or office use on the ground floor and secondary commercial or residential uses on the upper floors. New infill development, rehabilitation, additions, and building alterations need to consider the context of the Old Town block pattern and setting as well as the existing properties of historic significance or merit.

As previously mentioned, the Planning Area includes the current “SF-C” Santa Fe Commercial District zone and some surrounding parcels in the “C-1”, “C-2”, and “R-2” zones. The Plan proposes the area to be rezoned as “Old Town” and further divides the Old Town into five planning sub-areas to guide new development, infill, preservation, and reintegration of land uses. These sub-areas include Main Street (two- and three-story) Village, Mixed Use, High Density Residential, and Public Facilities.

¹⁴ DKS Associates. *Old Town Revitalization Project Traffic Impact Analysis, Prepared for the City of Placentia.* May 4, 2017.

Main Street (Two and Three Story Height Sub-area)

This sub-area is the core of the Old Town area and is found along both Bradford Avenue and Santa Fe Avenue from Center Street to Melrose Street and is characterized by small scale buildings with frontages that blend with the streetscapes. The focus is on historic rehabilitation and infill development to create a dense urban segment. Uses would be limited to commercial type uses that occupy smaller spaces and attract a large number of visitors. The Main Street sub-area encourages the preservation and enhancement of its unique historic character, with commercial block building forms and shopfront architecture. Allowable land uses emphasize ground floor retail, restaurant, and related uses, with opportunities for offices and other services above. No residential uses are permitted in the Main Street sub-area. The restoration and renovation of existing façades is encouraged.

Village (Three Story Height Sub-area)

This sub-area is found in two separate places: one at the north end of Bradford Avenue from Center Street to Chapman Avenue (excluding the southeast corner) and along both sides of Santa Fe Avenue from Melrose Street on the east to Murray Street on the west. The intent is to create new and infill development opportunities while preserving existing historically significant properties such as the old City Hall, Library, and Telephone Exchange building. The Village sub-area provides an opportunity for a new walkable retail and office center as an extension of the Main Street sub-area. The Village sub-area would provide for the same building form and setbacks as the Main Street sub-area but would allow three stories, with residential above. Retail and restaurants would be located on the ground floor spaces, with offices and residential permitted above. Building types are intended primarily to be a commercial block with well-defined frontages in the shopfront style similar to the Main Street zone.

Mixed-Use (Four Story Height Sub-area)

This sub-area is found at the southeastern corner of Chapman and Bradford Avenue and would be permitted four stories in height. The Mixed Use sub-area provides for a mix of concentrated urban housing types with some live-work opportunities with commercial on the ground floor. Denser, high quality, landmark development is encouraged here as this is envisioned as the gateway into the Old Town area. This sub-area could provide for urban residential building types including townhouses and courtyard housing, and live-work buildings.

High Density Residential (Four Story Height Sub-area)

At the most southwestern corner of the Plan area is the high density residential sub-area, with a four-story height limit. This area can provide opportunity for new housing development; with its residents patronizing the Old Town businesses and using the new Metrolink station for certain transport needs.

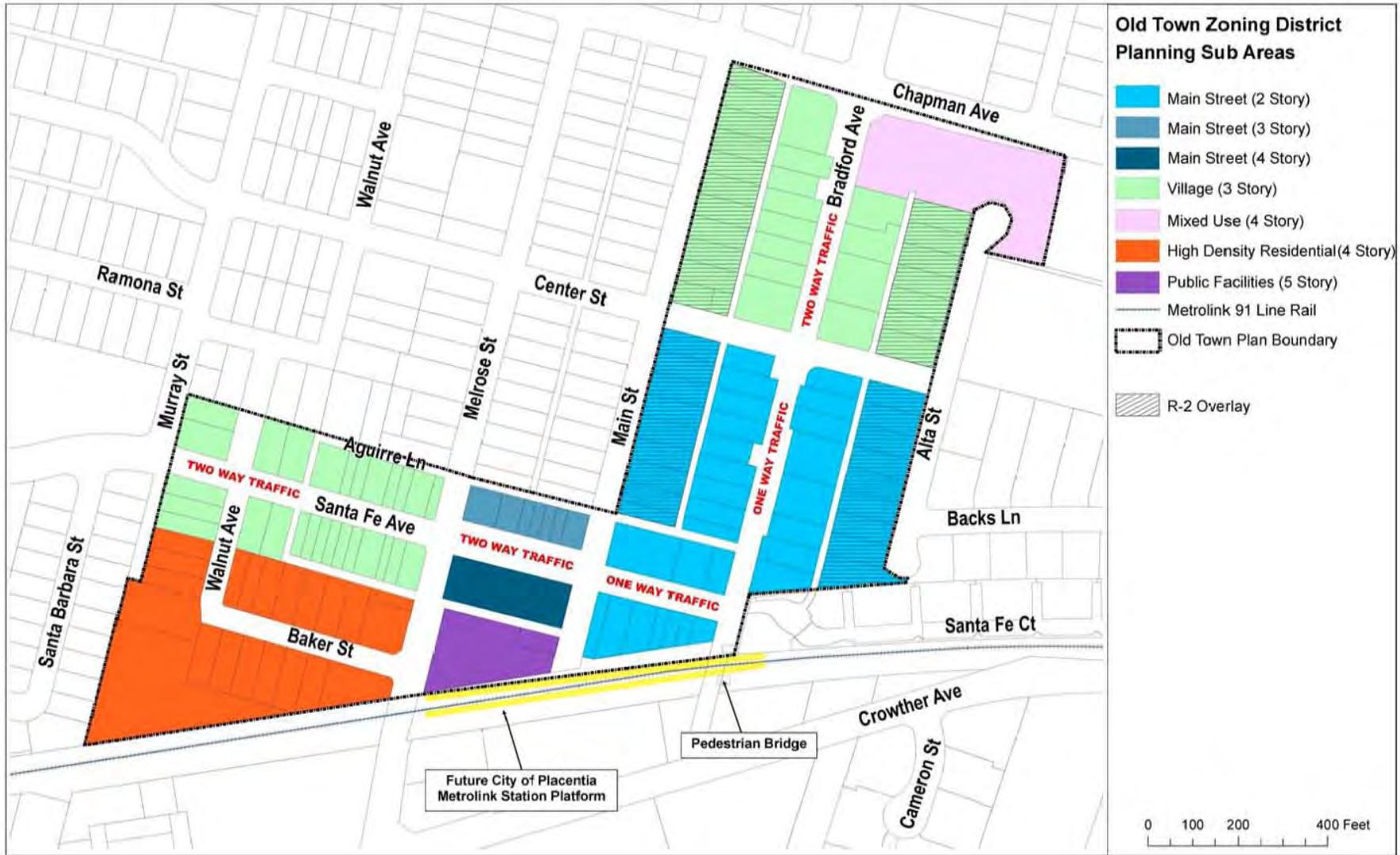


EXHIBIT 2-5
PROPOSED OLD TOWN ZONING DISTRICT PLANNING SUB-AREAS

Source: City of Placentia

Public Facilities (Five Story Height Sub-area)

This sub-area, bounded by Melrose Street on the west, Main Street on the east, the railroad on the south and the alley on the north, is the location for the upcoming parking structure. This structure will be five stories in height and will provide 253 parking spaces and will provide parking for Old Town visitors as well as those using the Metrolink station.

R-2 Overlay Zone

This overlay zone will permit the existing uses within the area and new residential uses, pursuant to Chapter 23.15 of the City's Zoning Code. Within the R-2 Overlay zone, no new retail or commercial uses will be permitted on the ground floor.

2.5 DISCRETIONARY ACTIONS

A discretionary action is a decision taken by a government agency (for this project, the government agency is the City of Placentia) that calls for an exercise of judgment in deciding whether to approve a project. As part of the proposed project's implementation, the City will consider the following approvals:

- General Plan Amendment (GPA 2017-02), to amend the General Plan map from the existing land use designations to a mixed-use land use designation to allow the following: local and neighborhood-supporting mixed-use areas designed to be contextually appropriate in and compatible with the identified neighborhood or historic area. On a single site, a combination of non-residential and residential uses can occur in the same structure or on the same site, where the residential component is located either above (vertical mixed-use) or behind or next to (horizontal mixed-use) the non-residential component. Commercial retail is encouraged to be the primary use on the ground floor. Professional office and housing uses are also encouraged, particularly as adaptive reuse opportunities within existing structures. Transit orientation, walkability and pedestrian access are key considerations..
- Zone Change (ZC 2017-02), to create new development standards for the new Old Town Zone, changing the zoning from R-2, C-1, C-2, and SF-C to Old Town Zone District;
- The adoption of the Old Town Placentia Revitalization Plan document;
- The adoption of a Streetscape Master Plan; and,
- The adoption of the Negative Declaration (ND).



SECTION 3 ENVIRONMENTAL ANALYSIS

This section of the Initial Study prepared for the proposed project analyzes the potential environmental impacts that may result from the proposed project's implementation. The issue areas evaluated in this Initial Study include the following:

- Aesthetics (Section 3.1);
- Agriculture & Forestry Resources (Section 3.2);
- Air Quality (Section 3.3);
- Biological Resources (Section 3.4);
- Cultural Resources (Section 3.5);
- Geology & Soils (Section 3.6);
- Greenhouse Gas Emissions; (Section 3.7);
- Hazards & Hazardous Materials (Section 3.8);
- Hydrology & Water Quality (Section 3.9);
- Land Use (Section 3.10);
- Mineral Resources (Section 3.11);
- Noise (Section 3.12);
- Population & Housing (Section 3.13);
- Public Services (Section 3.14);
- Recreation (Section 3.15);
- Transportation & Circulation (Section 3.16);
- Tribal Cultural Resources (Section 3.17);
- Utilities (Section 3.18); and,
- Mandatory Findings (Section 3.19).

The environmental analysis included in this section reflects the Initial Study checklist format used by the City of Placentia in its environmental review process. Under each issue area, an analysis of impacts is provided in the form of questions and answers. The analysis then provides a response to the individual questions. For the evaluation of potential impacts, questions are stated and an answer is provided according to the analysis completed as part of this Initial Study's preparation. To each question, there are four possible responses:

- *No Impact.* The proposed project will not have any measurable environmental impact on the environment.
- *Less Than Significant Impact.* The proposed project may have the potential for affecting the environment, although these impacts will be below levels or thresholds that the City of Placentia or other responsible agencies consider to be significant.
- *Less Than Significant Impact with Mitigation.* The proposed project may have the potential to generate impacts that will have a significant impact on the environment. However, the level of impact may be reduced to levels that are less than significant with the implementation of mitigation measures.
- *Potentially Significant Impact.* The proposed project may result in environmental impacts that are significant.

This Initial Study will assist the City in making a determination as to whether there is a potential for significant adverse impacts on the environment associated with the implementation of the proposed project.

3.1 AESTHETICS

3.1.1 THRESHOLDS OF SIGNIFICANCE

According to the City of Placentia, acting as Lead Agency, a project may be deemed to have a significant adverse aesthetic impact if it results in any of the following:

- An adverse effect on a scenic vista;
- Substantial damage to scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway;
- A substantial degradation of the existing visual character or quality of the site and its surroundings; or,
- A new source of substantial light or glare which would adversely affect day or nighttime views in the area.

3.1.2 ANALYSIS OF ENVIRONMENTAL IMPACTS

A. Would the project have a substantial adverse effect on a scenic vista? Less than Significant Impact.

The Planning Area is urban in nature and is currently developed with commercial and residential properties. The major physiographic features in the surrounding area include the Coyote Hills, located three miles to the northwest; the Chino Hills, located approximately four miles to the north; the Puente Hills, located approximately seven miles to the northwest; and the San Gabriel Mountains, located 19 miles to the north. The Old Town Placentia Revitalization Project applies to an L-shaped area that is located in the southwestern portion of the City of Placentia. The Planning Area is focused on Bradford Avenue and Santa Fe Avenue. The Old Town Placentia area encompasses retail, service, commercial, and residential uses. A number of buildings within the Planning Area are blighted and in need of improvement. Currently, there is no overall coherent design character or theme within the Old Town Placentia Planning Area.

The adoption and subsequent implementation of the Revitalization Project includes various goals, policies, and design standards that will enhance the visual appearance of the existing land uses and development within the Planning Area. In addition, development standards for the Planning Area will be adopted and will include guidance regarding the architectural design of new development. The following architectural styles are dictated for specific sub-areas:¹⁵

- *Main Street Sub-area - Main Street Commercial Architecture.* Multi-story facades are typically divided into base, body, and top with the ground floor taller than the shorter upper floor which is finished by a significant parapet. The ground floor has recessed entries and any expansive glass is interrupted by structural columns with transoms to allow light to penetrate deep into the interior.

¹⁵ Placentia, City of. *Old Town Placentia Revitalization Plan Development Standards*. May 2017.

Upper floor windows are smaller with vertical windows directly relating to the ground floor openings.

- *Village, Mixed-Use, and High Density Residential Sub-areas - Mission Revival Architecture.* Prominent features of the style include red clay tile roofs, use of balconies, smooth-stuccoed exterior walls usually painted white, arched openings, colorful tile work, and elaborate landscaping. The buildings frequently have courtyards.
- *High Density Residential Sub-area - Spanish Colonial Architecture.* Typical features include a low-pitched roof with little or no eave overhang, a red-tiled roof, perhaps a prominent rounded arch over a door, window or porch, a stucco wall surface, and usually an asymmetrical façade. The features elaborate molded ornament around doors and windows, polychrome tile at ingresses and wrought iron grilles and balconies.

The Revitalization Project contemplates buildings of up to five stories, or 65 feet, in height for the public Public Facilities sub-area. This sub-area will contain the public parking structure for the Metrolink Station and Old Town area. Mixed-use and high-density residential development will be more common among the Planning Area, with heights between 30 feet (two stories) and 60 feet (four stories). The majority of four-story residential buildings will be located within the Mixed Use and High Density Residential sub-areas, which will be located at the corner of Bradford and Chapman Avenues and on the southernmost portion of the Planning Area along Baker Street and Walnut Avenue. The three and four story buildings will be contiguous with the single- and two- story residential buildings, creating a unified architectural style. This will ensure a compatibility in architectural style between the different development intensities. This placement of high-density residential and mixed use buildings will mitigate aesthetic impacts by placing the majority of higher-rise building in one planned area to prevent the scattering of higher-rise buildings. Furthermore, public art and public plazas are encouraged in every development. Public art or plazas may be required as part of a development agreement for those developments that include 20 or more units or which are over 20,000 square feet. Public art is encouraged to reflect the history of Old Town Placentia and the citrus growing industry. Public art and public plazas will contribute to the aesthetic quality of the Old Town Placentia Planning Area.

The abovementioned architectural guidelines along with landscaping, street furniture, and regulated building heights will mitigate any adverse aesthetic impacts associated with new development within the Planning Area. As a result, less than significant adverse impacts will result from the implementation of the proposed project.

B. Would the project substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway? No Impact.

According to the California Department of Transportation (Caltrans), none of the arterial roadways within the Planning Area are designated scenic highways.¹⁶ In addition, the vegetation present within the Planning Area consists of species typically used for landscaping (palm trees, turf, etc.). The Planning Area is currently developed and does not contain any scenic rock outcroppings.¹⁷ Lastly, the Revitalization

¹⁶ Placentia, City of. Old Town Placentia Revitalization Development Standards. May 2017.

¹⁷ Blodgett Baylosis Environmental Planning. *Site survey*. Survey was conducted on April 20, 2017.

Project's implementation will not involve the removal of any buildings listed in the State or National Registrar (refer to Section 3.5). As a result, no impacts will occur.

C. Would the project substantially degrade the existing visual character or quality of the site and its surroundings? No Impact.

The Planning Area and the surrounding areas are developed. The main purpose of the Old Town Placentia Revitalization Project is to promote urban design elements that will enhance the appearance of the Planning Area. The adoption and subsequent implementation of the Revitalization Project will improve the Planning Area's image through the implementation of the design measures included in the Revitalization Plan document, the Development Standards, and the Streetscape Master Plan. As a result, no impacts will occur.

D. Would the project create a new source of substantial light or glare that would adversely affect day or nighttime views in the area? No Impact.

Potential sources of light and glare that may result from the adoption and subsequent implementation of the Old Town Placentia Revitalization Project include decorative lighting, security lighting, interior lighting, and vehicle headlights. The main purpose of the Revitalization Project is to promote urban design elements that will enhance the appearance of the Planning Area, including ways to control spill-over lighting and light trespass. In addition, the Development Standards list various regulations for sign lighting. These regulations include the following:¹⁸

- A. Sign lighting. Sign lighting shall be designed to minimize light and glare on surrounding rights-of-way and properties.
 - i. External light sources shall be directed and shielded so that they do not produce glare off the site, on any object other than the sign.
 - ii. Sign lighting shall not blink, flash, flutter, or change light intensity, brightness, or color.
 - iii. Colored lights shall not be used at a location or in a manner so as to be confused or construed as traffic control devices.
 - iv. Neither the direct nor reflected light from primary light sources shall create hazards for pedestrians or operators of motor vehicles.
 - v. For energy conservation, light sources shall be hard-wired fluorescent or compact fluorescent lamps, or other lighting technology that is of equal or greater energy efficiency. Incandescent lamps are prohibited.

Adherence to the development standards outlined in the Revitalization Plan will effectively mitigate light and glare and potential light trespass. As a result, the implementation of the Revitalization Plan will not result in any adverse light and glare impacts.

¹⁸ Placentia, City of. *Old Town Placentia Revitalization Plan Development Standards*. May 2017.

3.1.3 MITIGATION MEASURES

The adoption and subsequent implementation of the Old Town Placentia Revitalization Project will not lead to any significant adverse aesthetic impacts. In addition, the goals, policies, and implementation programs contained within the Revitalization Project will also further mitigate the potential impacts from new development contemplated as part of the implementation of the Old Town Placentia Revitalization Project. As a result, no additional mitigation beyond that which may be required for individual development projects is required.

3.2 AGRICULTURAL & FORESTRY RESOURCES

3.2.1 THRESHOLDS OF SIGNIFICANCE

According to the City of Placentia, acting as Lead Agency, a project may be deemed to have a significant impact on agriculture and forestry resources if it results in any of the following:

- The conversion of Prime Farmland, Unique Farmland, or Farmland of Statewide Importance to non-agricultural use;
- A conflict with existing zoning for agricultural use, or a Williamson Act Contract;
- A conflict with existing zoning for, or the rezoning of, forest land, timberland, or timberland zoned production;
- The loss of forest land or the conversion of forest land to non-forest use; or,
- Changes to the existing environment which, due to their location or nature, could result in conversion of Farmland, to non-agricultural use or conversion of forest land to non-forest use.

3.2.2 ANALYSIS OF ENVIRONMENTAL IMPACTS

A. Would the project convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use? No Impact.

No agricultural activities are located within the Planning Area and the applicable General Plan and Zoning designations do not include any agricultural land use designation within the area. In addition, the only agriculturally-designated zone within the City (*R-A Residential Agricultural District*) is located within the northernmost portion of the City and will not be affected by the proposed project, which will be located within the southwestern portion of the City. Furthermore, according to the soil maps prepared for Orange County by the United States Department of Agriculture, the Planning Area is underlain with soils of the Myford Sandy Loam Soils Association. Soils of this association are not classified as prime farmland.¹⁹ The adoption and subsequent implementation of the Revitalization Project will not involve the conversion of

¹⁹ United States Department of Agriculture, Natural Resources Conservation Service. *Web Soil Survey*.
<https://websoilsurvey.sc.egov.usda.gov/App/WebSoilSurvey.aspx>.

any agricultural land to urban uses. As a result, the adoption and subsequent implementation of the Old Town Placentia Revitalization Project will not impact any protected farmland soils.

B. Would the project conflict with existing zoning for agricultural use or a Williamson Act contract? No Impact.

No agricultural activities are located within the Planning Area or in the surrounding area. No agricultural land use designations are located within the Planning Area nor do the applicable land designations permit agricultural land uses. Furthermore, the parcels located within the land area governed by the Revitalization Project are developed with urban uses. No farming or other types of agricultural land uses are found in the Planning Area. In addition, there are no properties within the Planning Area that are subject to a Williamson Act Contract. As a result, the adoption and subsequent implementation of the Old Town Placentia Revitalization Project will not result in any impacts on existing Williamson Act contracts.

C. Would the project conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code §12220[g]), timberland (as defined in Public Resources Code §4526), or timberland zoned production (as defined in Government Code §51104[g])? No Impact.

The City of Placentia and the area governed by the Old Town Placentia Revitalization Project is located in the midst of a larger urban area and no forest lands are located within the City or within this portion of the Orange County. In addition, the City of Placentia General Plan and the Zoning Ordinance do not provide for any forest land preservation. As a result, no impacts on forest land or timber resources will result from the proposed project's implementation.

D. Would the project result in the loss of forest land or the conversion of forest land to a non-forest use? No Impact.

No forest lands are found within the City of Placentia nor do the applicable General Plan land use designations provide for any forest land protection. As a result, the adoption and subsequent implementation of the Old Town Placentia Revitalization Project will not result in any impacts related to the loss or conversion of existing forest lands. Therefore, no impacts will result from the project's implementation.

E. Would the project involve other changes in the existing environment that, due to their location or nature, may result in conversion of farmland to non-agricultural use or forest land to non-forest use? No Impact.

No agricultural activities, farmland uses, or forest uses are located or otherwise permitted in the geographic area governed by the Old Town Placentia Revitalization Project.²⁰ As a result, the adoption and subsequent implementation of the Old Town Placentia Revitalization Project will not involve the conversion of any existing farmland area to urban uses or the conversion of forest land to non-forest uses. As a result, no impacts will occur.

²⁰ Blodgett Baylosis Environmental Planning. *Site survey*. Survey was conducted on April 20, 2017.

3.2.4 MITIGATION MEASURES

The analysis of agriculture and forestry resources indicated that no impacts on these resources would occur as part of the proposed Revitalization Project's implementation. In addition, the goals, policies, and implementation programs contained within the Revitalization Project will further mitigate the potential impacts from new development contemplated as part of the implementation of the Revitalization Project. As a result, no additional mitigation beyond that which may be required for individual development projects is required.

3.3 AIR QUALITY

3.3.1 THRESHOLDS OF SIGNIFICANCE

According to the City of Placentia, acting as Lead Agency, a project may have a significant adverse environmental impact on air quality if it results in any of the following:

- A conflict with or the obstruction of the implementation of the applicable air quality plan;
- A violation of an air quality standard or a substantial contribution to an existing or projected air quality violation;
- A cumulatively considerable net increase of any criteria pollutant for which the project region is in non-attainment under an applicable Federal or State ambient air quality standard;
- The exposure of sensitive receptors to substantial pollutant concentrations; or,
- The creation of objectionable odors affecting a substantial number of people.

The South Coast Air Quality Management District (SCAQMD) has established quantitative thresholds for criteria pollutants that include the following:

- *Ozone (O₃)* is a nearly colorless gas that irritates the lungs and damages materials and vegetation. O₃ is formed by photochemical reaction. Los Angeles and the surrounding South Coast Air Basin (SCAB) are designated by the Environmental Protection Agency (EPA) and the California Air Resources Board (CARB) as an extreme ozone *non-attainment area*.²¹
- *Carbon Monoxide (CO)* is a colorless, odorless toxic gas that interferes with the transfer of oxygen to the brain that is produced by the incomplete combustion of carbon-containing fuels emitted as vehicle exhaust. The SCAB is designated as an attainment area for carbon monoxide by the EPA.
- *Nitrogen dioxide (NO₂)* is a yellowish-brown gas that, at high levels, can cause breathing difficulties. NO₂ is formed when nitric oxide (a pollutant from burning processes) combines with oxygen. Although NO₂ concentrations have not exceeded national standards since 1991, NO₂

²¹ A non-attainment area refers to a geographic area where the Environmental Protection Agency (EPA) and/or the California Air Resources Board (CARB) have determined that the air quality standards for the criteria pollutants are not being met.

emissions remain a concern because of their contribution to the formation of O₃ and particulate matter. The SCAB is designated as an attainment area for NO₂ by the EPA.

- *Sulfur dioxide* (SO₂) is a colorless, pungent gas formed primarily by the combustion of sulfur-containing fossil fuels. Health effects include acute respiratory symptoms and difficulty in breathing for children. Though SO₂ concentrations have been reduced to levels that are well below State and Federal standards, further reductions in SO₂ emissions are desirable since SO₂ is a precursor to sulfate and PM₁₀. The SCAB is designated as an attainment area for SO₂.
- *PM₁₀* refers to particulate matter less than ten microns in diameter. PM₁₀ particulates cause a greater health risk than larger-sized particles since fine particles can more easily cause respiratory irritation. The Federal standards for PM₁₀ have been met in most areas within the SCAB.
- *PM_{2.5}* refers to particulate matter less than 2.5 microns in diameter. PM_{2.5} also represents a significant health risk because particulate matter of this size may be more easily inhaled, causing respiratory irritation. The annual average concentrations of PM_{2.5} exceeded Federal standards in some areas of the SCAB. As a result, PM_{2.5} continues to be designated as non-attainment.

3.3.2 ANALYSIS OF ENVIRONMENTAL IMPACTS

A. *Would the project conflict with or obstruct implementation of the applicable air quality plan? Less than Significant Impact.*

The Planning Area governed by the Old Town Placentia Revitalization Project is located within the South Coast Air Basin (SCAB) which covers a 6,600-square-mile area within Orange County, the non-desert portions of Los Angeles County, Riverside County, and San Bernardino County. The SCAB is subject to the Final 2016 AQMP which was jointly prepared with the California Air Resources Board (CARB) and the Southern California Association of Governments (SCAG).²² Specific criteria for determining a project's conformity with the AQMP is defined in Chapter 12 of the 2012 Air Quality Management Plan (AQMP) and Section 12.3 of the SCAQMD's CEQA Air Quality Handbook:²³

- *Consistency Criteria 1* refers to a proposed project's potential for resulting in an increase in the frequency or severity of an existing air quality violation or its potential for contributing to the continuation of an existing air quality violation.
- *Consistency Criteria 2* refers to a proposed project's potential for exceeding the assumptions included in the AQMP or other regional growth projections relevant to the AQMP's implementation.

In terms of Criteria 1, construction-related activities related to the adoption and subsequent implementation of the Revitalization Project will not lead to an exceedance for AQMD daily thresholds. Although most developments will be subject to individual environmental review, all of the projects will be

²² South Coast Air Quality Management District, *Final 2012 Air Quality Plan*, Adopted February 2013.

²³ South Coast Air Quality Management District. *CEQA Air Quality Handbook*. 2012.

small in size and will not exceed air quality requirements. Developments that will be within the Planning Area will not be projects that are classified as “Regionally Significant Projects” and therefore will not have significant air quality impacts. In order to be classified as a “Regionally Significant Project” by CEQA, a project must be a residential development of more than 500 units; a commercial center or business employing more than 1,000 persons or having more than 500,000 square feet of floor area; an office building employing more than 1,000 persons or encompassing more than 250,000 square feet of floor area; a hotel consisting of more than 500 rooms; or an industrial use with more than 650,000 square feet of floor area or occupying more than 40 acres of land, or employing more than 1,000 persons.²⁴

Although the development envisioned within the Revitalization Project is extensive, the environmental analysis required for each development will account for the simultaneous construction of neighboring development, thereby minimizing the air quality impacts associated with the construction and operation of the commercial, retail, and residential developments. Short-term (construction-related) and long-term (operational) air quality assessments will be required for all new developments subject to CEQA.²⁵ The revitalization of the Planning Area would be classified as infill development, which is beneficial because it would be effective in reducing urban sprawl and the overall vehicle miles traveled (VMT) by being located on an underutilized area within a developed area.

In terms of Criteria 2, the proposed project will not lead to a significant population growth within the City. According to the population projections contained within the 2016 Regional Transportation Plan prepared by SCAG, the City of Placentia’s population in 2012 was 51,500 and the projected population for the year 2040 for the City is projected to be 58,400. As previously mentioned, each individual residential development will require an environmental analysis prior to construction which will determine whether or not the associated population increase will result in air quality impacts. The proposed project will not significantly affect any regional population, housing, and employment projections prepared for the City by the Southern California Association of Governments (SCAG) within the 2016 Regional Transportation Plan. As a result, less than significant impacts will occur.

B. Would the project violate any air quality standard or contribute substantially to an existing or projected air quality violation? Less than Significant Impact.

The potential construction emissions and long-term emissions from the build-out of the Revitalization Project are included in Appendix A. Future site-specific development proposals that deviate from the Revitalization Project would be evaluated for potential air emissions once development details have been determined and are available. Individual projects permitted as part of the Revitalization Project’s implementation will not result in significant air quality emissions. Although individual development projects have the potential to exceed SCAQMD thresholds, the objectives, policies, and implementation actions in the Revitalization Plan document would address the potential impacts. Therefore, the Revitalization Project’s implementation will not result in any significant air quality impacts.

²⁴ California, State of. *Title 14. California Code of Regulations. Chapter 3. Guidelines for the Implementation of the California Environmental Quality Act.* As Amended 1998 (CEQA Guidelines). § 15206 (2) (A-E).

²⁵ *Ibid.* § 15060 – 15065.

C. Would the project result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable Federal or State ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors)? Less than Significant Impact.

As indicated previously, the SCAB is a designated non-attainment area for ozone and particulates. The revitalization of the Old Town Placentia Planning Area would be classified as infill development, which is beneficial because it would be effective in reducing urban sprawl and in the overall vehicle miles traveled (VMT). This is because the Planning Area is located within an established urban area. Finally, the proposed Revitalization Project will not exceed these adopted projections used in the preparation of the Regional Transportation Plan. As a result, less than significant impacts related to an increase in criteria pollutants will occur.

D. Would the project expose sensitive receptors to substantial pollutant concentrations? Less than Significant Impact.

Most vehicles generate carbon monoxide (CO) as part of the tail-pipe emissions and high concentrations of CO along busy roadways and congested intersections are a concern. The areas surrounding the most congested intersections are often found to contain high levels of CO that exceed applicable standards. Typically, a hot-spot may occur near an intersection that is experiencing severe congestion (a LOS E or LOS F). The SCAQMD stated in its CEQA Handbook that a CO hot-spot would not likely develop at an intersection operating at LOS C or better. Since the Handbook was written, there have been new CO emissions controls added to vehicles and reformulated fuels are now sold in the SCAB. These new automobile emissions controls, along with the reformulated fuels, have resulted in a lowering of both ambient CO concentrations and vehicle emissions. In addition, the infill nature of the project as well as the requirements for sustainable development will promote land uses and development contemplated in the Air Quality Management Plan and the Regional Growth Management Plan.

Sensitive populations are more susceptible to the effects of air pollution than the general population. Sensitive populations (sensitive receptors) that are in proximity to localized sources of toxics and CO are of particular concern. Land uses considered sensitive receptors include residences, schools, playgrounds, childcare centers, athletic facilities, long-term health care facilities, rehabilitation centers, convalescent centers, and retirement homes. Sensitive receptors located within the Planning Area and the surrounding area include the City's senior center, two schools, and two parks. The only sensitive receptor within the Planning Area is the Placentia Senior Center, which is located on the northwest corner of Bradford Avenue and Center Street. As previously mentioned, each individual development will require an environmental analysis prior to construction which will determine whether or not the development will have any air quality impact on any nearby sensitive receptors. As a result, less than significant impacts related to the adoption and subsequent implementation of the Revitalization Project will occur.

E. Would the project create objectionable odors affecting a substantial number of people? No Impact.

The SCAQMD has identified those land uses that are typically associated with odor complaints. These uses include activities involving livestock, rendering facilities, food processing plants, chemical plants,

composting activities, refineries, landfills, and businesses involved in fiberglass molding.²⁶ The proposed project will include residential, commercial, and mixed-use development, and no generation of odors will occur. During the site survey, no odors were detected at the site. Furthermore, odor-generating uses are not permitted within the Planning Area. As a result, no odor-related impacts will occur.

3.3.3 MITIGATION MEASURES

The adoption and subsequent implementation of the Old Town Placentia Revitalization Project will not lead to any significant impacts. In addition, the goals, policies, and implementation programs contained within the Revitalization Project will also further mitigate the potential impacts from new development contemplated as part of the implementation of the Revitalization Project. As a result, no additional mitigation beyond that which may be required for individual development projects is required.

3.4 BIOLOGICAL RESOURCES

3.4.1 THRESHOLDS OF SIGNIFICANCE

According to the City of Placentia, acting as Lead Agency, a project may be deemed to have a significant adverse impact on biological resources if it results in any of the following:

- A substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Wildlife or the U.S. Fish and Wildlife Service;
- A substantial adverse effect on any riparian habitat or other sensitive natural plant community identified in local or regional plans, policies, regulations, or by the California Department of Fish and Wildlife or U.S. Fish and Wildlife Service;
- A substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act through direct removal, filling, hydrological interruption, or other means;
- A substantial interference with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory life corridors, or impede the use of native wildlife nursery sites;
- A conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance; or,
- A conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or State habitat conservation plan.

²⁶ South Coast Air Quality Management District. *CEQA Air Quality Handbook*. April 1993.

3.4.2 ANALYSIS OF ENVIRONMENTAL IMPACTS

A. *Would the project have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Wildlife or U.S. Fish and Wildlife Service? No Impact.*

The City of Placentia is an entirely urbanized city. A review of the California Department of Fish and Wildlife California Natural Biodiversity Database (CNDDDB) Bios Viewer for the Placentia Quadrangle indicated that out of a total of 47 native plant and animal species, nine are either threatened or endangered.²⁷ These species include:

- The *Coastal California Gnatcatcher* is not likely to be found on-site due to the amount of urbanization in the area and the lack of suitable habitat for the California Gnatcatcher. The absence of coastal sage scrub, the California Gnatcatcher's primary habitat, further diminishes the likelihood of encountering such birds.²⁸
- The *Least Bell's Vireo* lives in a riparian habitat, with a majority of the species living in San Diego County.²⁹ As a result, it is not likely that any least Bell's vireos will be encountered during on-site construction activities.
- The *Tricolored Blackbird* lives and breeds in freshwater marsh habitats and forages in farm fields, pastures, cattle pens, and large lawns. Furthermore, this bird species exists in densely-packed colonies, which have made the species more vulnerable to endangerment and less likely to be found in or around the Planning Area.³⁰
- The *Western Yellow-billed Cuckoo* is an insect-eating bird found in riparian woodland habitats. The likelihood of encountering a western yellow-billed cuckoo is slim due to the level of urbanization present in the surrounding areas and the lack of riparian habitat.³¹
- The *California Least Tern* generally lives along the coast. According to the U.S. Fish and Wildlife Service, Carlsbad Fish and Wildlife Office, the areas within Orange County that the avian species has been known to live include Anaheim Bay, Surfside Beach, Bolsa Chica, Huntington Beach, and Upper Newport Bay, which are all outside of the Planning Area.³²

²⁷ California Department of Fish and Wildlife. Bios Viewer. <https://map.dfg.ca.gov/bios/?tool=cnddbQuick>.

²⁸ Audubon. *California Gnatcatcher*. <http://birds.audubon.org/species/calgna>.

²⁹ California Partners in Flight Riparian Bird Conservation Plan. *Least Bell's Vireo*. http://www.prbo.org/calpif/htmldocs/species/riparian/least_bell_vireo.htm.

³⁰ National Audubon Society. *Tricolored Blackbird*. <http://www.audubon.org/field-guide/bird/tricolored-blackbird>.

³¹ US Fish and Wildlife Service. *Sacramento Fish and Wildlife Office, Public Advisory*. http://www.fws.gov/sacramento/outreach/Public-Advisories/WesternYellow-BilledCuckoo/outreach_PA_Western-Yellow-Billed-Cuckoo.htm.

³² US Fish and Wildlife Service. *California Least Tern*. https://www.fws.gov/sacramento/es_species/Accounts/Birds/es_ca-least-tern.htm.

- The *California black rail* occurs in habitats that include salt marshes, freshwater marshes, and wet meadows, which do not exist within the Planning Area. In addition, the majority of the species is found in the tidal salt marshes of the northern San Francisco Bay region.³³
- *Santa Ana sucker* is a fish species that will not be found on-site because no bodies of water are located within the Planning Area.³⁴
- The *quino checkerspot butterfly* is not likely to be found on-site since the eight known populations are located in Riverside County and San Diego County.³⁵
- *Santa Ana River woollystar* is a plant species that grows in sandy areas and requires periodic flooding along with scouring and sediment deposition to persist. Occurrences of the plant species exist primarily within San Bernardino County and Riverside County. Historical occurrences are known from Orange County, but it has been extirpated from those locations.³⁶

The adoption and subsequent implementation of the Old Town Placentia Revitalization Project will not have an impact on the aforementioned species because the Planning Area is located in the midst of an urban area and there is no suitable riparian or native habitat located within, or in the vicinity of, the Planning Area. As a result, no impacts on any candidate, sensitive, or special status species will result from proposed project's implementation.

B. Would the project have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations, or by the California Department of Fish and Wildlife or U.S. Fish and Wildlife Service? No Impact.

A review of the U.S. Fish and Wildlife Service National Wetlands Inventory, Wetlands Mapper was conducted and it was determined that there are no wetlands or riparian habitats located within the Planning Area and the surrounding area.³⁷ The nearest wetland is a lake which is located 0.8 miles southeast of the Planning Area. In addition, there are no designated "blue line streams" located within the Planning Area.³⁸ As a result, no impacts on natural or riparian habitats will result from the adoption and subsequent implementation of the Old Town Placentia Revitalization Project.

³³ U.S. Forest Service. *California Black Rail Distribution and Abundance in Relation to Habitat and Landscape Features in the San Francisco Bay Estuary*. <https://www.fs.usda.gov/treesearch-beta/pubs/31850>.

³⁴ Blodgett Baylois Environmental Planning. *Site Survey*. Survey was completed on April 20, 2017.

³⁵ Center for Biological Diversity. *Endangered Species Act Profile, Quino checkerspot butterfly*. http://www.biologicaldiversity.org/species/invertebrates/Quino_checkerspot_butterfly/endangered_species_act_profile.html.

³⁶ California Department of Fish and Wildlife. *Santa Ana River Woollystar*. <https://www.wildlife.ca.gov/Conservation/Plants/Endangered/Eriastrum-densifolium-ssp-sanctorum>.

³⁷ U.S. Fish and Wildlife Service. *Wetlands Mapper*. <http://www.fws.gov/Wetlands/data/Mapper.html>.

³⁸ A blue-line stream is any stream shown as a solid or broken blue line on 7.5 Minute Series quadrangle maps prepared by USGS. Essentially, a blue-line stream is any stream with a significant amount of water-flow for a significant part the year.

C. Would the project have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means? No Impact.

The Planning Area does not contain any natural wetland and/or riparian habitat. As previously indicated, there are no designated wetlands or riparian habitat present in the geographic area governed by the Old Town Placentia Revitalization Project. This conclusion is also supported by a review of the U.S. Fish and Wildlife Service National Wetlands Inventory, Wetlands Mapper.³⁹ As a result, the adoption and subsequent implementation of the Revitalization Project will not impact any protected wetland area or designated blue-line stream.

D. Would the project interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory life corridors, or impede the use of native wildlife nursery sites? No Impact.

No natural open space areas are located within the Planning Area or in the surrounding area that would potentially serve as an animal migration corridor. As a result, the adoption and subsequent implementation of the Revitalization Project will not result in any impacts.

E. Would the project conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance? No Impact.

No areas within the City of Placentia are included in any natural community conservation plan or other habitat conservation plan. Chapter 14.12, Trees and Shrubs, of the City's Municipal Code governs the use of all public trees and activities which may affect all public trees. The Old Town Placentia Revitalization Project also includes a focus on landscaping and tree planting with the new developments. As a result, the adoption and subsequent implementation of the Revitalization Project will not result in any impacts.

F. Would the project conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan? No Impact.

The Planning Area is currently developed and no natural habitats are found in the Planning Area. The Planning Area is not located within an area governed by a habitat conservation or community conservation plan. The nearest ecological reserve is the Coal Canyon Ecological Reserve, located 10.3 miles east of the Planning Area.⁴⁰ Since the proposed project will not conflict with any local, regional, or State habitat conservation plans, no impacts will result.

³⁹ U.S. Fish and Wildlife Service. *Wetlands Mapper*. <http://www.fws.gov/Wetlands/data/Mapper.html>

⁴⁰ California Department of Fish and Wildlife. *Ecological Reserves and Wildlife Areas in California*. <https://www.wildlife.ca.gov/lands/places-to-visit>.

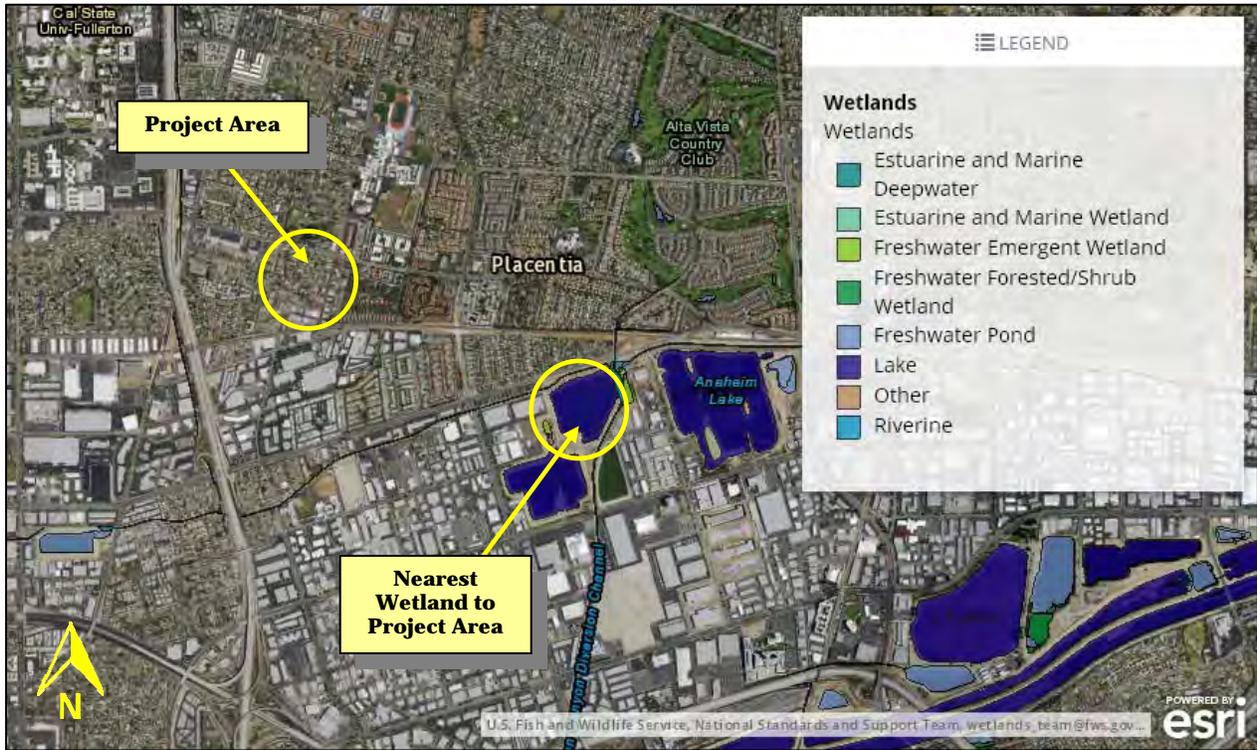


EXHIBIT 3-1
WETLANDS MAP
Source: U.S. Fish and Wildlife Service, National Wetlands Inventory

3.4.3 MITIGATION MEASURES

The analysis indicated that the proposed project would not result in any impacts on biological resources. In addition, the goals, policies, and implementation programs contained within the Revitalization Project will also further mitigate the potential impacts from new development contemplated as part of the implementation of the Old Town Placentia Revitalization Project. As a result, no additional mitigation beyond that which may be required for individual development projects is required.

3.5 CULTURAL RESOURCES

3.5.1 THRESHOLDS OF SIGNIFICANCE

According to the City of Placentia, acting as Lead Agency, a project will normally have a significant adverse impact on cultural resources if it results in any of the following:

- A substantial adverse change in the significance of a historical resource as defined in §15064.5 of the State CEQA Guidelines;
- A substantial adverse change in the significance of an archaeological resource pursuant to §15064.5 of the State CEQA Guidelines;
- The destruction of a unique paleontological resource, site, or unique geologic feature; or,
- The disturbance of any human remains, including those interred outside of formal cemeteries.

3.5.2 ANALYSIS OF ENVIRONMENTAL IMPACTS

A. Would the project cause a substantial adverse change in the significance of a historical resource as defined in §15064.5 of the State CEQA Guidelines? Less than Significant Impact.

Historic structures and sites are defined by local, State, and Federal criteria. A site or structure may be historically significant if it is locally protected through a local General Plan or historic preservation ordinance. A site or structure may be historically significant according to State or Federal criteria even if the locality does not recognize such significance. The State, through the State Historic Preservation Office (SHPO), maintains an inventory of those sites and structures that are considered to be historically significant. Finally, the U.S. Department of Interior has established specific Federal guidelines and criteria that indicate the manner in which a site, structure, or district is to be defined as having historic significance and in the determination of its eligibility for listing on the National Register of Historic Places.⁴¹ To be considered eligible for the National Register, a property's significance may be determined if the property is associated with events, activities, or developments that were important in the past, with the lives of people who were important in the past, or represents significant architectural, landscape, or engineering elements. Specific criteria include the following:

⁴¹ U. S. Department of the Interior, National Park Service. *National Register of Historic Places*. <http://focus.nps.gov/nrhp>.

- Districts, sites, buildings, structures, and objects that are associated with the lives of significant persons in the past;
- Districts, sites, buildings, structures, and objects that embody the distinctive characteristics of a type, period, or method of construction, or that represent the work of a master, or that possess high artistic values, or that represent a significant and distinguishable entity whose components may lack individual distinction; or,
- Districts, sites, buildings, structures, and objects that have yielded or may be likely to yield, information important in history or prehistory.

Ordinarily, properties that have achieved significance within the past 50 years are not considered eligible for the National Register. However, such properties *will qualify* if they are integral parts of districts that do meet the criteria or if they fall within the following categories:

- A religious property deriving primary significance from architectural or artistic distinction or historical importance;
- Districts, sites, buildings, structures, and objects that are associated with events that have made a significant contribution to the broad patterns of our history;
- A building or structure removed from its original location that is significant for architectural value, or which is the surviving structure associated with a historic person or event;
- A birthplace or grave of a historical figure of outstanding importance if there is no appropriate site or building associated with his or her productive life;
- A cemetery that derives its primary importance from graves of persons of transcendent importance, from age, from distinctive design features, or from association with historic events;
- A reconstructed building when accurately executed in a suitable environment and presented in a dignified manner as part of a restoration master plan, and when no other building or structure with the same association has survived;
- A property primarily commemorative in intent if design, age, tradition, or symbolic value has invested it with its own exceptional significance; or,
- A property achieving significance within the past 50 years if it is of exceptional importance.⁴²

Four locations within the City are recorded on the National Register of Historic Places and the list of California Historical Resources. These include the Bradford, A.S., House, the Key, George, Ranch, the Placentia Mutual Orange Association Packing House, and the site of the first Macadamia Tetraphylla

⁴² U. S. Department of the Interior, National Park Service. *National Register of Historic Places*. <http://focus.nps.gov/nrhp>.

planted in California.⁴³ These sites and structures are not located within or adjacent to the Planning Area. An inventory of potential historic resources located in the Planning Area is included in Table 3-1.

**Table 3-1
 Historic Resources in Placentia**

Resource Name	Location
Placentia Water Tower	Chapman Ave at Main St
Old Placentia Library	143 S Bradford Ave
Old City Hall	120 S Bradford Ave
Cavalry Church	102 S Bradford Ave
The Kraemer Building	238 & 240 S Bradford Ave
Telephone Exchange Building	125 S Bradford Ave
Marjie Building	149, 155, and 161 W Santa Fe Ave
The Stradley Building No. 2	167 & 173 W Santa Fe Ave

Source: Old Town Placentia Revitalization Plan.

The Revitalization Plan document recommends that a historic resource survey be conducted for the entire Planning Area within the first year of adoption to closely identify properties of significance. Infill development and alterations will need to be carefully reviewed on a case-by-case basis.

Adaptive reuse involves the conversion of a historic property for more contemporary use that may require certain modifications to the building form and features. Such modifications can be achieved without compromising the features identified as significant in earlier work. Historic rehabilitation may involve returning previously inappropriate alterations made to conform to the style and features of the historic architecture. Infill development in a historically sensitive area requires special attention to scale, massing, rhythm, and building placement. Guidance for alterations to historic properties is best found in the Secretary of the Interior’s Standards and Guidelines for Rehabilitation of Historic Properties. Changes, alterations, and demolition of potential and listed historic resources often undergo a design review process to determine the impact of the changes on the historic significance of the property. Minor changes and ongoing maintenance require minimal staff review, but major alterations and demolitions are typically reviewed by a commission.

The structures within the Planning Area are not listed on the National or State Historic Register. As indicated previously, there are four locations in the City that are recorded on the National or State Register

⁴³ U. S. Department of the Interior, National Park Service. *National Register of Historic Places*. <http://focus.nps.gov/nrhp>.
 Secondary Source: California Department of Parks and Recreation. *California Historical Resources*.
<http://ohp.parks.ca.gov/ListedResources>.

of Historic Places.⁴⁴ The Revitalization Project will be limited to the Planning Area and will not affect any existing resources listed on the National or State Registers. Redevelopment of the Planning Area may impact local culturally important buildings. Any impact to these buildings will be evaluated on a case-by-case basis once a particular development concept has been submitted to the City. As a result, less than significant impacts are associated with the proposed Revitalization Project's implementation.

B. Would the project cause a substantial adverse change in the significance of an archaeological resource pursuant to §15064.5 of the State CEQA Guidelines? Less than Significant Impact.

The greater Los Angeles Basin was previously inhabited by the Gabrielino people, named after the San Gabriel Mission. The Gabrielino tribe has lived in this region for around 7,000 years.⁴⁵ Prior to Spanish contact, approximately 5,000 Gabrielino people lived in villages throughout the Los Angeles Basin.⁴⁶ Gabrielino villages were often located near bodies of water, such as the Los Angeles River, the Rio Hondo River, the Santa Ana River, and the San Gabriel River. The closest known village to the City of Placentia is Hotuuknga, which was located in the cities of Anaheim and Yorba Linda.⁴⁷ The Revitalization Project will promote new development located in an area that is occupied by commercial and residential uses. Although the surrounding area has been subject to disturbance to accommodate the existing commercial and residential buildings, the Planning Area may be situated in an area of high archaeological significance.

Formal Native American consultation was undertaken in accordance with AB-52 and no responses were received. In the unlikely event that remains are uncovered by construction crews, all excavation and grading activities shall be halted and the Orange County Sheriff will be contacted (the Department will then contact the County Coroner). This is a standard condition under California Health and Safety Code Section 7050.5(b), which states:

"In the event of discovery or recognition of any human remains in any location other than a dedicated cemetery, there shall be no further excavation or disturbance of the site or any nearby area reasonably suspected to overlie adjacent remains until the coroner of the county in which the human remains are discovered has determined, in accordance with Chapter 10 (commencing with (b) Section 27460) of Part 3 of Division 2 of Title 3 of the Government Code, that the remains are not subject to the provisions of Section 27491 of the Government Code or any other related provisions of law concerning investigation of the circumstances, manner and cause of any death, and the recommendations concerning the treatment and disposition of the human remains have been made to the person responsible for the excavation, or to his or her authorized representative, in the manner provided in Section 5097.98 of the Public Resources Code. The coroner shall make his or her determination within two working days from the time the person responsible for the excavation, or his or her authorized representative, notifies the coroner of the discovery or recognition of the human remains. If the coroner determines that the remains are not subject to his or her authority and if the coroner recognizes the human remains to be those of a Native American, or has reason to believe that

⁴⁴ National Park Service U.S. Department of the Interior. *National Register of Historic Places, Title List Display.* <http://nrhp.focus.nps.gov/natreghome.do>.

⁴⁵ Tongva People of Sunland-Tujunga. *Introduction.* http://www.lausd.k12.ca.us/Verdugo_HS/classes/multimedia/intro.html.

⁴⁶ Rancho Santa Ana Botanical Garden. *Tongva Village Site.* <http://www.rsabg.org/tongva-village-site1>.

⁴⁷ TongvaPeople.com. *Villages.* <http://tongvapeople.com/villages.html>.

they are those of a Native American, he or she shall contact, by telephone within 24 hours, the Native American Heritage Commission.”

In addition, Title 14; Chapter 3; Article 5; Section 15064.5 of CEQA will apply in terms of the identification of significant archaeological resources and their salvage. Therefore, the potential impacts are considered to be less than significant.

C. Would the project directly or indirectly destroy a unique paleontological resource or site or unique geologic feature? Less than Significant Impact.

No paleontological resources or geologic features are anticipated to be encountered during the construction phase of any new development due to the age of the soil and the limited amount of excavation that will be required to implement the project. The soils that underlie the Planning Area are alluvial in nature. Alluvial deposits are typically quaternary in age (from two million years ago to the present day) and span the two most recent geologic epochs, the Pleistocene and the Holocene.⁴⁸ Given the degree of previous disturbance on-site and the depth to natural soils under the alluvial soils near the surface, the likelihood of encountering paleontological resources is slim. As a result, the potential impacts will be less than significant.

D. Would the project disturb any human remains, including those interred outside of dedicated cemeteries? Less than Significant Impact.

There are no dedicated cemeteries located within the City of Placentia. The nearest cemetery to the Planning Area is the Anaheim Cemetery located approximately 2.05 miles to the southwest in the City of Anaheim.⁴⁹ In the unlikely event that a human burial is encountered during the construction of any new development within the Old Town Placentia Planning Area, all construction activities shall be halted and the Placentia Police Department will be contacted (the department will then contact the County Coroner). In the event of an accidental discovery, Title 14; Chapter 3; Article 5; Section 15064.5 of CEQA will apply in terms of the identification of significant archaeological resources and their salvage. As a result, the impacts are considered to be less than significant.

3.5.3 MITIGATION MEASURES

The adoption and subsequent implementation of the Old Town Placentia Revitalization Project will not lead to any significant impacts on cultural resources. In addition, the goals, policies, and implementation programs contained within the Revitalization Project will also further mitigate the potential impacts from new development contemplated as part of the implementation of the Revitalization Project. Finally, the adherence to those regulation and protocols that govern the discovery of cultural resources during grading and excavation will further reduce potential impacts. As a result, no additional mitigation beyond the standard conditions identified previously will be required.

⁴⁸ United States Geological Survey. *What is the Quaternary?* http://geomaps.wr.usgs.gov/sfgeo/quaternary/stories/what_is.html.

⁴⁹ Google Earth. Website accessed May 4, 2017.

3.6 GEOLOGY & SOILS

3.6.1 THRESHOLDS OF SIGNIFICANCE

According to the City of Placentia, acting as Lead Agency, a project may be deemed to have a significant adverse impact on the environment if it results in the following:

- The exposure of people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving rupture of a known earthquake fault (as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault), ground shaking, liquefaction, or landslides;
- Substantial soil erosion resulting in the loss of topsoil;
- The exposure of people or structures to potential substantial adverse effects, including location on a geologic unit or a soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction, or collapse;
- Locating a project on an expansive soil, as defined in the California Building Code, creating substantial risks to life or property; or,
- Locating a project in, or exposing people to potential impacts, including soils incapable of adequately supporting the use of septic tanks or alternative wastewater disposal systems where sewers are not available for the disposal of wastewater.

3.6.2 ANALYSIS OF ENVIRONMENTAL IMPACTS

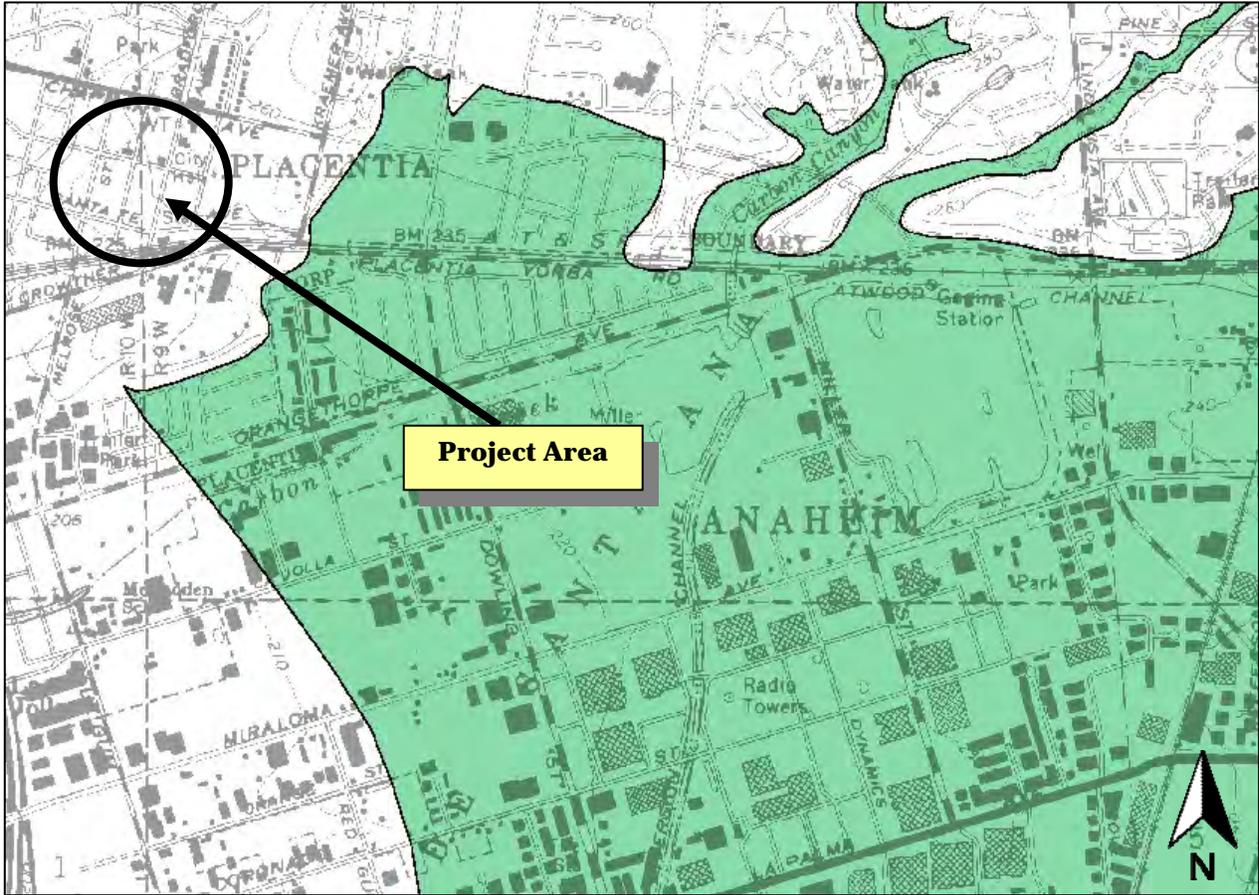
A. *Would the project expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving rupture of a known earthquake fault (as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault), ground-shaking, liquefaction, or landslides? Less than Significant Impact.*

The City of Placentia is located in a seismically active region. Earthquakes from several active and potentially active faults in the Southern California region could affect the Planning Area. The biggest threats to both the City and the Planning Area are the El Modeno and Peralta Hills Faults, located approximately two miles to the south, and the Whittier Fault, located approximately 4.3 miles to the north.⁵⁰ In 1972, the Alquist-Priolo Earthquake Zoning Act was passed in response to the damage sustained in the 1971 San Fernando Earthquake. The Alquist-Priolo Earthquake Fault Zoning Act's main purpose is to prevent the construction of buildings used for human occupancy on the surface trace of active

⁵⁰ California Institute of Technology, Southern California Earthquake Data Center. *Significant Earthquakes and Faults, Historical Earthquakes and Significant Faults in Southern California*. <http://scedc.caltech.edu/significant>.



EXHIBIT 3-2
FAULTS IN THE PROJECT AREA
SOURCE: SOUTHERN CALIFORNIA EARTHQUAKE DATA CENTER



MAP EXPLANATION

Zones of Required Investigation:

Liquefaction
Areas where historic occurrence of liquefaction, or local geological, geotechnical and groundwater conditions indicate a potential for permanent ground displacements such that mitigation as defined in Public Resources Code Section 2693(c) would be required.

Earthquake-Induced Landslides
Areas where previous occurrence of landslide movement, or local topographic, geological, geotechnical and subsurface water conditions indicate a potential for permanent ground displacements such that mitigation as defined in Public Resources Code Section 2693(c) would be required.

EXHIBIT 3-3
LIQUEFACTION RISK
SOURCE: CALIFORNIA GEOLOGICAL SURVEY

faults.⁵¹ The City of Placentia was not included in any Alquist-Priolo Special Studies Zone.⁵² However, the Old Town Placentia area, like all development in the City, would be subject to ground shaking. All new development within the State and within the Planning Area would be required to comply with the California Building Code development standards, which identify specific requirements to seismicity.⁵³ Liquefaction is the process by which the ground soil loses strength due to an increase in water pressure following seismic activity. According to the California Department of Conservation, the Planning Area for the Old Town Placentia Revitalization Project is not located in a liquefaction zone. As a result, the impacts are considered to be less than significant.

B. Would the project expose people or structures to potential substantial adverse effects, including substantial soil erosion or the loss of topsoil? Less than Significant Impact.

According to the soil maps prepared for Orange County by the United States Department of Agriculture, the Planning Area is underlain with soils of the Myford Sandy Loam Soils Association. Soils of the Myford association have an erosion hazard; however, the majority of the Planning Area and the surrounding area is currently developed, which has reduced the area's soil erosion risk.⁵⁴ The Planning Area is level and limited excavation will be required for the structural supports, building foundations, and utility lines of any new development. In addition, the project applicant for each development must adhere to the construction best management practices (BMPs) identified by the City and the Water Quality Management Plan (WQMP), which will decrease any potential erosion impacts.

Operational BMPs would include elements such as modular wetlands, bioswales, stormwater retention basins, grate inlet filters, etc. as a means to reduce stormwater runoff and erosion. Furthermore, the surface grades within the parking and internal roadways of new developments will be designed to facilitate drainage into the nearest roadway curb and gutters. As a result, the impacts are expected to be less than significant.

C. Would the project expose people or structures to potential substantial adverse effects, including location on a geologic unit or a soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction, or collapse? Less than Significant Impact.

Recent studies completed by the California Geological Survey (CGS) Seismic Hazard Zones Mapping Program indicate the Planning Area is not located within an area that is subject to potential slope failure. The Planning Area is located within an area that is not subject to potential liquefaction risk. The Myford Sandy Loam Soils underlie the Planning Area and this soils association is suitable for future development as is evident of the existing development found within and around the Planning Area. The design of future

⁵¹ California Department of Conservation. *What is the Alquist-Priolo Act.* <http://www.conservation.ca.gov/cgs/rghm/ap/Pages/main.aspx>.

⁵² California Department of Conservation. *Table 4, Cities and Counties Affected by Alquist-Priolo Earthquake Fault Zones as of January 2010.*

⁵³ California. State of. California Building Standards Commission. *California Building Code, Part 2, Volume 2, Chapter 16.*

⁵⁴ United States Department of Agriculture, Natural Resources Conservation Service. *Web Soil Survey.* <https://websoilsurvey.sc.egov.usda.gov/App/WebSoilSurvey.aspx>.

projects will be required to conform to the current building code seismic requirements. As a result, the adoption and subsequent implementation of the Old Town Placentia Revitalization Project will not result in any significant impacts.

D. Would the project result in or expose people to potential impacts, including location on expansive soil, as defined in Uniform Building Code (2012), creating substantial risks to life or property? No Impact.

The soils that underlie the Planning Area belong to the Myford Sandy Loam Soils Association. These soils do not represent a constraint to development according to the United States Department of Agriculture (USDA).⁵⁵ Shrinking and swelling is influenced by the amount of clay present in the underlying soils.⁵⁶ According to the United States Department of Agriculture, clay is present in the composition of Myford Soils Association beyond a depth of 12 inches.⁵⁷ However, all new structural improvements would be required to comply with the most current California Building Code requirements. As a result, no impacts related to expansive soils are anticipated.

E. Would the project result in or expose people to potential impacts, including soils incapable of adequately supporting the use of septic tanks or alternative wastewater disposal systems where sewers are not available for the disposal of wastewater? No Impact.

No septic tanks will be used as part of any future development. As a result, no impacts associated with the use of septic tanks will occur as part of the proposed project's implementation.

3.6.3 MITIGATION MEASURES

The adoption and subsequent implementation of the Old Town Placentia Revitalization Project will not lead to any significant impacts related to geology and soils. In addition, the goals, policies, and implementation programs contained within the Revitalization Project will also further mitigate the potential impacts from new development contemplated as part of the implementation of the Revitalization Project. As a result, no additional mitigation beyond that which may be required for individual development projects is required.

3.7 GREENHOUSE GAS EMISSIONS

3.7.1 THRESHOLDS OF SIGNIFICANCE

A project may be deemed to have a significant adverse impact on greenhouse gas emissions if it results in any of the following:

⁵⁵ United States Department of Agriculture, Soil Conservation Service. *Report and General Soil Map, Orange County, California*. Rev. 1969.

⁵⁶ Natural Resources Conservation Service Arizona. *Soil Properties Shrink/Swell Potential*. http://www.nrcs.usda.gov/wps/portal/nrcs/detailfull/az/soils/?cid=nrcs144p2_065083.

⁵⁷ United States Department of Agriculture. *Myford Series*. https://soilseries.sc.egov.usda.gov/OSD_Docs/M/MYFORD.html.

- The generation of greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment; and,
- The potential for conflict with an applicable plan, policy, or regulation adopted for the purpose of reducing emissions of greenhouse gases.

3.7.2 ENVIRONMENTAL ANALYSIS

A. Would the project generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment? Less than Significant Impact.

The State of California requires CEQA documents to include an evaluation of greenhouse gas (GHG) emissions or gases that trap heat in the atmosphere. GHG are emitted by both natural processes and human activities. Examples of GHG that are produced both by natural processes and human activities include carbon dioxide (CO₂), methane (CH₄), and nitrous oxide (N₂O). The accumulation of GHG in the atmosphere regulates the earth's temperature. Without these natural GHG, the Earth's surface would be about 61°F cooler. However, emissions from fossil fuel combustion have elevated the concentrations of GHG in the atmosphere to above natural levels.

GHG differ from criteria or toxic air pollutants in that the GHG emissions do not cause direct adverse human health effects. Rather, the direct environmental effect of GHG emissions is the increase in global temperatures, which in turn has numerous impacts on the environment and humans. Some examples of observed changes include shrinking glaciers, thawing permafrost, late freezing, early break-up of ice on rivers and lakes, a lengthened growing season, shifts in plant and animal ranges, and earlier flowering of trees.

Table 3-2 summarizes annual greenhouse gas (CO₂E) emissions from build-out of the proposed development that would be part of the Old Town Revitalization Project.⁵⁸ Carbon dioxide equivalent, or CO₂E, is a term that is used for describing different greenhouses gases in a common and collective unit. As indicated in Table 3-2, the CO₂E total for the project is 41,370.25 pounds per day or 18.77 MTCO₂E per day. This translates into an annual emission of 6,851.05 MTCO₂E. As indicated in Table 3-2, the great majority of the GHG emissions will be generated from mobile sources (Long-Term Mobile Emissions). For this reason, the project's use of trip reduction incentives (the use alternative forms of transportation, the installation of electric vehicle charging stations and bicycle racks, and other TDM measures) will be important. The project is also an infill development that will be placed amongst an urban and underutilized area.

⁵⁸ The CalEEMod Air Quality Worksheets are provided in Appendix A.

**Table 3-2
 Greenhouse Gas Emissions Inventory**

Source	GHG Emissions (Lbs/Day)			
	CO ₂	CH ₄	N ₂ O	CO ₂ E
Long-term Area Emissions	78.03	0.08	--	79.93
Long-term Energy Emissions	3,262.61	0.06	0.06	3,282.00
Long-term Mobile Emissions	37,951.97	2.25	--	38,008.32
Total Long-term Emissions	41,292.61	2.39	0.06	41,370.25

Source: CalEEMod V.2016.V. 3.1

Although the development envisioned within the Revitalization Project is extensive, the environmental analysis required for each development will account for the simultaneous construction of neighboring development, thereby minimizing the greenhouse gas impacts associated with the construction and operation of the commercial, retail, and residential developments. Short-term (construction-related) and long-term (operational) greenhouse gas emission assessments will be required for all new developments subject to CEQA.⁵⁹ Although individual development projects may have the potential to exceed SCAQMD thresholds, the objectives, policies, and implementation actions in the Revitalization Project would address the potential impacts. The adoption and subsequent implementation of the Revitalization Project will not result in a significant amount of additional greenhouse gases. As a result, less than significant impacts are anticipated.

B. Would the project conflict with an applicable plan, policy, or regulation adopted for the purpose of reducing emissions of greenhouse gasses? No Impact.

The City of Placentia is in the process of updating its General Plan, which was adopted in the 1980's. The General Plan will establish new objectives, policies, and implementation actions to reduce greenhouse gases by encouraging the use of alternative energy sources, reducing vehicle miles traveled (VMT), conserving parks/open space, developing public education programs emphasizing green building practices and promoting innovative approaches to reduce harmful impacts to the atmosphere. The objectives, policies, and implementation actions within the Revitalization Plan document will further reduce greenhouse gas emissions since the project focuses on the revitalization of an underutilized area, which will become a pedestrian-oriented area with a diverse mix of uses.

Although the development envisioned within the Revitalization Project is extensive, the environmental analysis required for each development will account for the simultaneous construction of neighboring development, thereby minimizing the greenhouse gas impacts associated with the construction and operation of the commercial, retail, and residential developments. Short-term (construction-related) and long-term (operational) greenhouse gas emission assessments will be required for all new developments subject to CEQA.⁶⁰ Although individual development projects may have the potential to exceed SCAQMD

⁵⁹ California, State of. *Title 14. California Code of Regulations. Chapter 3. Guidelines for the Implementation of the California Environmental Quality Act.* As Amended 1998 (CEQA Guidelines). § 15060 – 15065.

⁶⁰ Ibid.

thresholds, the objectives, policies, and implementation actions in the Revitalization Project would further address the potential impacts since the project focuses on the revitalization of an underutilized area, which will become a pedestrian-oriented area with a diverse mix of uses.

Furthermore, there will also be a regional benefit in terms of a reduction in VMT because it is classified as an infill project that is consistent with the regional and State sustainable growth objectives identified in the State's Strategic Growth Council (SGC).⁶¹ The proposed project will provide employment opportunities for local residents, local shopping and dining establishments, and residential uses, thereby reducing VMT and reducing trip length for vehicle trips in the surrounding area. As a result, no potential conflict with an applicable greenhouse gas policy plan, policy, or regulation will occur.

3.7.3 MITIGATION MEASURES

The adoption and subsequent implementation of the Old Town Placentia Revitalization Project will not lead to any significant impacts related to greenhouse gas emissions. In addition, the goals, policies, and implementation programs that will be contained within the Revitalization Project will also further mitigate the potential impacts from new development contemplated as part of the implementation of the Revitalization Project. As a result, no additional mitigation beyond that which may be required for individual development projects is required.

3.8 HAZARDS & HAZARDOUS MATERIALS

3.8.1 THRESHOLDS OF SIGNIFICANCE

According to the City of Placentia, acting as Lead Agency, a project may be deemed to have a significant adverse impact on risk of upset and human health if it results in any of the following:

- The creation of a significant hazard to the public or the environment through the routine transport, use or disposal of hazardous materials;
- The creation of a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment;
- The generation of hazardous emissions or the handling of hazardous or acutely hazardous materials, substances or waste within one-quarter mile of an existing or proposed school;

⁶¹ California Strategic Growth Council. <http://www.sgc.ca.gov/Initiatives/infill-development.html>. Promoting and enabling sustainable infill development is a principal objective of the SGC because of its consistency with the State Planning Priorities and because infill furthers many of the goals of all of the Council's member agencies. Focusing growth toward infill areas takes development pressure off conservation lands and working lands; it increases transit rider-ship and reduces vehicle trips; it requires less per capita energy and water use than less space-efficient development; it improves public health by promoting active transportation and active lifestyles; and it provides a more equitable mix of housing choices, among other benefits. Thus, the SGC has been investigating actions that can be taken to improve the ability of local governments and private developers to successfully plan and build good infill projects.

- Locating the project on a site that is included on a list of hazardous material sites compiled pursuant to Government Code Section 65962.5 resulting in a significant hazard to the public or the environment;
- Locating the project within an area governed by an airport land use plan, or where such a plan has not been adopted, within two miles of a public airport or a public use airport;
- Locating the project in the vicinity of a private airstrip that would result in a safety hazard for people residing or working in the Planning Area;
- The impairment of the implementation of, or physical interference with, an adopted emergency response plan or emergency evacuation plan; or,
- The exposure of people or structures to a significant risk of loss, injury, or death involving wild land fire, including where wild lands are adjacent to urbanized areas or where residences are intermixed with wild lands.

3.8.2 ANALYSIS OF ENVIRONMENTAL IMPACTS

A. Would the project create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials? Less than Significant Impact.

The proposed project involves the revitalization of the Old Town Placentia area, which will involve the construction and operation of commercial, retail, residential, and mixed-use uses. The use of hazardous materials for the new developments will largely consist of those commonly found in a commercial setting used in routine maintenance and cleaning. All future tenants will need to comply with all Federal and State regulations regarding hazardous materials. A database search was conducted of the California Department of Toxic Substances Control (DTSC) Envirostor database and the Environmental Protection Agency (EPA) Envirofacts database to locate any potentially contaminated sites within the Old Town Planning Area. One auto body location was identified through the EPA Envirofacts database.⁶² The identified site is a small waste generator and will not create a significant environmental impact to the larger Old Town Placentia Planning Area.

The Planning Area is currently developed with a variety of commercial and residential land uses. Depending on the age of the buildings that would be demolished as part of any future development within the Planning Area, potential asbestos containing materials (ACMs) and lead-based paint (LBP) may be present within the buildings. If any of the buildings to be demolished are determined to have been constructed prior to 1975, a ACM/LBP survey will be required to be completed prior to the building demolition to assess the occurrence of these hazardous materials, pursuant to Federal and State regulations. Furthermore, if the building is discovered to contain ACMs, LBPs, or other hazardous substances, the project contractors will adhere to all requirements governing the handling, removal, and disposal of the hazardous substances that may be encountered during demolition and land clearance activities. Any contamination encountered during the demolition, grading, and/or site preparation

⁶² California Department of Toxic Substances Control. *Envirostor*. <https://www.envirostor.dtsc.ca.gov/public>. Secondary Source: United States Environmental Protection Agency (EPA). *Envirofacts*. <https://www3.epa.gov/enviro>.

activities must also be removed and disposed of in accordance with applicable laws prior to the issuance of any building permit. This is a standard condition that is required as part of any development within the City. Adherence to Federal and State regulations governing the handling, transport, and disposal of lead based paint and asbestos containing materials will reduce potential impacts to levels that are less than significant.

B. Would the project create a significant hazard to the public or the environment, or result in reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment? Less than Significant Impact.

Future development arising as part of the Old Town Placentia Revitalization Project's implementation will not result in any significant adverse impact related to hazardous materials. As previously mentioned, the use of hazardous materials for the new development will largely consist of those commonly found in a commercial setting used in routine maintenance and cleaning. In addition, the auto body site that was identified through the EPA Envirofacts database is a small waste generator and will not create a significant environmental impact to the larger Old Town Planning Area. Furthermore, if any buildings are discovered to contain ACMs, LBPs, or other hazardous substances, the project contractors will adhere to all Federal and State requirements governing the handling, removal, and disposal of the hazardous substances that may be encountered during demolition and land clearance activities. As a result, less than significant impacts are anticipated with the proposed project's implementation.

C. Would the project emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school? No impact.

Kraemer Middle School and Valencia High School are located within one-quarter mile of the Planning Area. However, hazardous chemicals and materials used on-site will be limited to common household maintenance and cleaning products. Due to the nature of the proposed use, no hazardous materials will be emitted. As a result, no impacts are anticipated with the proposed project's implementation.

D. Would the project be located on a site, which is included on a list of hazardous material sites compiled pursuant to Government Code Section 65962.5, and, as a result, would it create a significant hazard to the public or the environment? No Impact.

Government Code Section 65962.5 refers to the Hazardous Waste and Substances Site List, commonly known as the Cortese List, maintained by the California Department of Toxic Substances Control. The Cortese list contains hazardous waste and substance sites including public drinking water wells with detectable levels of contamination, sites with known underground storage tanks (USTs) having a reportable release, solid waste disposal facilities from which there is a known migration, hazardous substance sites selected for remedial action, historic Cortese sites, and sites with known toxic material identified through the abandoned site assessment program. A search of the Envirostor Hazardous Waste

and Substances Site “Cortese” List database did not identify any Cortese sites within the Planning Area or the entire City.⁶³ Therefore, no impacts will occur.

E. For a project located within an airport land use plan, or where such a plan has not been adopted, within two miles of a public airport or a public use airport, would the project result in a safety hazard for people residing or working in the project area? No Impact.

The Planning Area is not located within two miles of an operational public airport. The nearest public use airport is the Fullerton Municipal Airport and it is located approximately 5.75 miles west of the Planning Area. The nearest major airport is located in Long Beach approximately 12.7 miles to the south.⁶⁴ As a result, no impacts will occur.

F. For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing or working in the project area? No Impact.

The Planning Area is not located within two miles of a private airstrip.⁶⁵ The nearest private airport to the Planning Area is a heliport that is located 0.6 miles southwest of the Planning Area. As a result, no impacts will occur.

G. Would the project impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan? No Impact.

At no time will any local roadways be completely closed to traffic during the construction of any new development. Each new development will be evaluated to determine if a construction plan is necessary so the proposed development does not impair or interfere with any emergency response or evacuation plan. The construction plan must ensure that all construction staging occurs on-site. The construction plan must also identify specific provisions for the regulation of construction vehicle ingress and egress to the site as a means to provide continued through-access. As a result, no impacts are associated with the proposed project's implementation.

H. Would the project expose people or structures to a significant risk of loss, injury, or death involving wild lands fire, including where wild lands are adjacent to urbanized areas or where residences are intermixed with wild lands? No Impact.

The Planning Area is urbanized and the properties surrounding the site are developed. There are no areas of native vegetation found within the Planning Area or in the surrounding area that could provide a fuel source for a wildfire. As a result, there are no impacts associated with potential wildfires from off-site locations.

⁶³ California Department of Toxic Substances Control. *Hazardous Waste and Substances Site Cortese List*. http://www.dtsc.ca.gov/SiteCleanup/Cortese_List.cfm.

⁶⁴ Toll Free Airline. *Orange County Public and Private Airports, California*. <http://www.tollfreeairline.com/california/orange.htm>.

⁶⁵ Ibid.

3.8.3 MITIGATION MEASURES

The adoption and subsequent implementation of the Old Town Placentia Revitalization Project will not lead to any significant impacts related to hazards and hazardous materials. In addition, the goals, policies, and implementation programs contained within the Revitalization Project will also further mitigate the potential impacts from new development contemplated as part of the implementation of the Revitalization Project. As a result, no additional mitigation beyond that which may be required for individual development projects is required.

3.9 HYDROLOGY & WATER QUALITY

3.9.1 THRESHOLDS OF SIGNIFICANCE

According to the City of Placentia, acting as Lead Agency, a project may be deemed to have a significant adverse environmental impact on water resources or water quality if it results in any of the following:

- A violation of any water quality standards or waste discharge requirements;
- A substantial depletion of groundwater supplies or interference with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level;
- A substantial alteration of the existing drainage pattern of the site or area through the alteration of the course of a stream or river in a manner that would result in substantial erosion or siltation on or off-site;
- A substantial alteration of the existing drainage pattern of the site or area, including the alteration of the course of a stream or river, in a manner that would result in flooding on or off-site;
- The creation or contribution of water runoff that would exceed the capacity of existing or planned storm water drainage systems or the generation of substantial additional sources of polluted runoff;
- The substantial degradation of water quality;
- The placement of housing within a 100-year flood hazard area as mapped on a Federal Flood Hazard Boundary, Flood Insurance Rate Map, or other flood hazard delineation map;
- The placement of structures within 100-year flood hazard areas that would impede or redirect flood flows;
- The exposure of people or structures to a significant risk of flooding as a result of dam or levee failure; or,
- The exposure of a project to inundation by seiche, tsunami, or mudflow.

3.9.2 ANALYSIS OF ENVIRONMENTAL IMPACTS

A. Would the project violate any water quality standards or waste discharge requirements? No Impact.

As part of the new development supported by the Revitalization Project, certain improvements will be installed that will affect the amount of potential storm water runoff. The major source of potential water pollution is related to sheet runoff capturing surface pollutants that are then conveyed into the local storm water system that is composed of gutters, drains, catch basins, and pipes. This storm water infrastructure collects the rainwater runoff and ultimately deposits everything it gathers, including contaminants and debris, into the ocean. All development will be required to comply with all pertinent stormwater discharge requirements. In addition, future development must adhere to the construction best management practices (BMPs) identified by the City and the Water Quality Management Plan (WQMP). The future development will also be required to implement operational BMPs such as modular wetlands, bioswales, stormwater retention basins, grate inlet filters, etc. as a means to reduce stormwater runoff and filter out potential contaminants. The adoption and subsequent implementation of the Revitalization Project will not involve any significant impacts since all new development will be required to comply with the aforementioned requirements. As a result, no impacts are anticipated with the proposed project's implementation.

B. Would the project substantially deplete groundwater supplies or interfere substantially with groundwater recharge in such a way that would cause a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of a pre-existing nearby well would drop to a level which would not support existing land uses or planned uses for which permits have been granted)? No Impact.

Grading related activities are not anticipated to encounter and deplete groundwater supplies. A search was conducted through the Regional Water Quality Control Board's on-line database Geotracker to identify the presence of any natural underground water wells within the Planning Area. The search yielded no results.⁶⁶ In addition, all new development will be connected to the City's water lines and will not deplete groundwater supplies through the consumption of the water. All new development will be required to meet the City's Water Efficiency Landscape Ordinance to reduce the burden placed on the City's water resources.⁶⁷ As a result, no impacts are anticipated with the proposed project's implementation.

C. Would the project substantially alter the existing drainage pattern of the site or area, including the alteration of the course of a stream or river, in a manner, which would result in substantial erosion or siltation on- or off-site? Less than Significant Impact.

Although the impervious surfaces (asphalt, building slabs, etc.) that will be constructed with new development will result in the generation of stormwater runoff, the Planning Area will be properly drained and is not expected to result in erosion or siltation on- or off-site. In the absence of mitigation, the new impervious surfaces (buildings, internal driveways, parking areas, etc.) that would be constructed may result in the generation of pollutants. The Planning Area will be graded so that stormwater runoff will be

⁶⁶ Geotracker GAMA. <http://geotracker.waterboards.ca.gov/gama/gamamap/public/default.asp>.

⁶⁷ Placentia, City of. *Municipal Code, Chapter 23.77, Water Efficient Landscape Ordinance.*

directed to the curbs and gutters on the local roadways. Furthermore, there are no streams, rivers, or other bodies of water located within, or adjacent to the Planning Area. The proposed project will be restricted to the Planning Area and will not alter the course of any waterways.⁶⁸ In addition, no natural drainage or riparian areas remain within the Planning Area due to the past development. As a result, less than significant impacts are anticipated.

D. Would the project substantially alter the existing drainage pattern of the site or area, including the alteration of the course of a stream or river, in a manner, which would result in flooding on-or off-site? Less than Significant Impact.

As indicated previously, the impervious surfaces (asphalt, building slabs, etc.) that will be constructed will result in the generation of stormwater runoff. However, the Planning Area will be properly drained and is not expected to result in flooding on-or off-site. The Planning Area will be graded so that stormwater runoff will be directed to the curbs and gutters on the local roadways. As indicated in the previous section, the proposed project will be restricted to the Planning Area and will not alter the course of any waterways. As a result, less than significant impacts are anticipated.

E. Would the project create or contribute runoff water that would exceed the capacity of existing or planned storm water drainage systems or provide substantial additional sources of polluted runoff? Less than Significant Impact.

Future development in the City would increase impervious areas and overall levels of activity. As a result, impacts to stormwater quality would occur. The pollutants that would be expected with future development projects include pollutants typically found in stormwater runoff. Without mitigation, future development would be expected to increase pollutant loadings, including hydrocarbons, fertilizers, pesticides, trash, and sediment. Future development would be required to include both Structural and Non-Structural BMPs, and to comply with the SUSMP. New development projects would also be required to meet pertinent water quality standards and implement mitigation (as necessary) to reduce impacts to levels that are less than significant. As a result, less than significant impacts are anticipated with the proposed project's implementation.

F. Would the project otherwise substantially degrade water quality? Less than Significant Impact.

The City of Placentia will require the preparation of a SUSMP. The preparation of the SUSMP will ensure that local water quality remains undisturbed once the project is operational. As a result, less than significant impacts are anticipated with the proposed project's implementation.

⁶⁸ Google Earth. Website accessed May 8, 2017.

G. Would the project place housing within a 100-year flood hazard area as mapped on a Federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map? No Impact.

According to maps obtained at the Federal Emergency Management System Map Service Center, the Planning Area is not located within a 100-year flood plain (refer to Exhibit 3-4).⁶⁹ As a result, no impacts related to flood flows are associated with the proposed project's implementation.

H. Would the project place within a 100-year flood hazard area, structures that would impede or redirect flood flows? No Impact.

As indicated previously, the Planning Area is not located within a designated 100-year flood hazard area as defined by FEMA.⁷⁰ As a result, the proposed project will not involve the placement of any structures that would impede or redirect potential floodwater flows since the site is not located within a 100-year flood hazard area. Therefore, no flood-related impacts are anticipated to occur with the implementation of the Old Town Placentia Revitalization Project.

I. Would the project expose people or structures to a significant risk of flooding as a result of dam or levee failure? No Impact.

The City of Placentia is within the dam inundation area of the Carbon Canyon Dam and the Prado Dam. If an inundation event should occur as a result of dam failure, floodwaters may potentially reach the SR-91 Freeway in the southern portion of the City. The Planning Area is currently developed and new developments would not increase the exposure of people or structures to a significant flooding risk. As a result, no impacts are anticipated with the proposed project's implementation.

J. Would the project result in inundation by seiche, tsunami, or mudflow? No Impact.

The Planning Area is located approximately 15 miles inland from the Pacific Ocean and would not be exposed to the effects of a tsunami. There are no surface water bodies in the immediate area of the Planning Area that would result in a potential seiche hazards. The Planning Area would not be subject to mudflows since the Planning Area is currently developed and generally level. As a result, no impacts related to seiche, tsunami, or mudflows will result from the implementation of the proposed project.

3.9.3 MITIGATION MEASURES

The adoption and subsequent implementation of the Old Town Placentia Revitalization Project will not lead to any significant impacts related to hydrology and water quality. In addition, the goals, policies, and implementation programs contained within the Revitalization Project will also further mitigate the potential impacts from new development contemplated as part of the implementation of the Revitalization Project. As a result, no additional mitigation beyond that which may be required for individual development projects is required.

⁶⁹ Federal Emergency Management Agency. *FEMA's National Flood Hazard Layer*. <https://msc.fema.gov/portal>.

⁷⁰ Federal Emergency Management Agency. *FEMA and ESRI Flood Insurance Rate Mapping*. 2010.

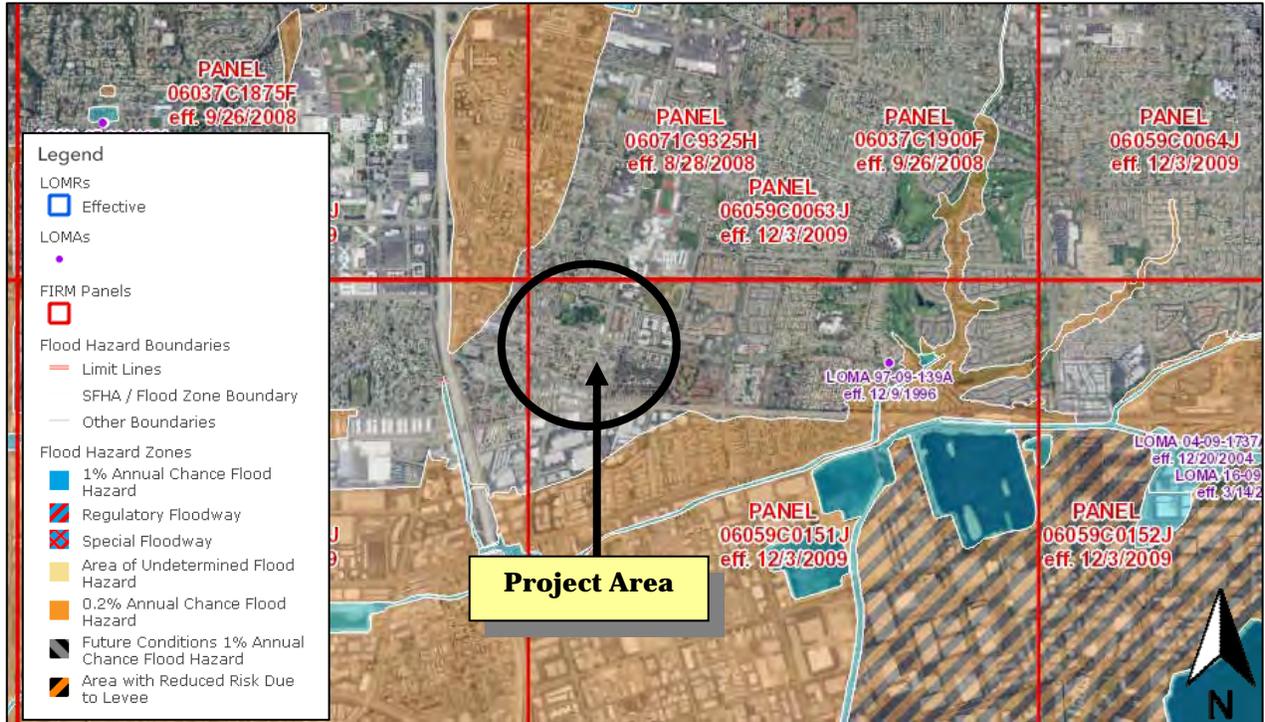


EXHIBIT 3-4
FEMA FLOOD MAP
Source: Federal Emergency Management Agency

3.10 LAND USE & PLANNING

3.10.1 THRESHOLDS OF SIGNIFICANCE

According to the City of Placentia, acting as Lead Agency, a project may be deemed to have a significant impact on land use and development if it results in any of the following:

- The disruption or division of the physical arrangement of an established community;
- A conflict with an applicable land use plan, policy or regulation of the agency with jurisdiction over the project; or,
- A conflict with any applicable conservation plan or natural community conservation plan.

3.10.2 ANALYSIS OF ENVIRONMENTAL IMPACTS

A. Would the project physically divide or disrupt an established community or otherwise result in an incompatible land use? No Impact.

The Revitalization Project is anticipated to facilitate development, especially residential and retail in a mixed-use setting, proximate to the newly approved Metrolink Station and parking structure in the Old Town area and the recently approved Transit Oriented Development (TOD) south of the railroad along Crowther Avenue. The purpose of the Revitalization Project is to enhance the physical environment in the City's Old Town aimed at creating a lively destination to support the current economic base, create a town center for Placentia, and better connect to adjacent neighborhoods and surrounding cities. The Revitalization Project's implementation is critical in aiding in the realization of local and regional goals related to sustainable and infill development.

California State law requires specific plans (the Revitalization Plan document is essentially a specific plan) to be internally consistent with the jurisdiction's general plan. The City is currently in the process of updating the City's General Plan adopted in the 1980's to provide a regulatory framework for development throughout the City. The General Plan land use map is being updated concurrently with the Revitalization Project to reflect the vision for the Old Town area within the Revitalization Plan document. With proposed General Plan Amendment and the update to the General Plan land use map, the Revitalization Project is intended to be consistent with the City's General Plan, Zoning Code, Municipal Codes, and regional planning initiatives particularly for climate action, sustainability, multimodal transportation, complete streets, and livability. The current adopted General Plan designations for the Planning Area include Commercial, High Density Residential, and Medium Density Residential (refer to Exhibit 3-6). The General Plan Map will be amended to reflect the land use designations identified in the Old Town Placentia Revitalization Plan document. The land uses and development promoted in the Revitalization Project are generally consistent with the land use and development policies that are included in the Placentia General Plan.

A zoning text amendment, referred to as the Old Town Placentia Revitalization Development Standards herein, is being concurrently processed with the Revitalization Project. The Planning Area currently

includes the “SF-C” (Santa Fe Commercial) zone and some surrounding parcels in the “C-1” (Neighborhood Commercial), “C-2” (Community Commercial), and “R-2” (Low-Medium Density Residential) zones (refer to Exhibit 3-5). The area will be rezoned as “Old Town” and further divides the Old Town into five planning sub-areas to guide new development, infill, preservation, and reintegration of land uses. These sub-areas were drawn around distinctive areas or locations in the Planning Area, with distinctive standards (refer to Exhibit 3-7). These five planning sub-areas include Main Street (Two- Three- and Four-Story Height Sub-areas), Village (Three-Story Height Sub-area), Mixed-Use (Four-Story Height District), High-Density Residential (Four-Story Height Sub-area), and Public Facilities (Five Story Height Sub-area). Each sub-area will permit different uses and will employ designated architectural styles, setbacks, and other designated development standards outlined within the Old Town Placentia Revitalization Development Standards.⁷¹ The project team, including City Staff and the consultants (traffic, planning, and environmental) identified the maximum amount of new development that could be realized as part of the Revitalization Project’s implementation. No specific new projects are known at this time though the development projections are critical in evaluating the potential environmental impacts including, but not limited to land use changes, traffic, air quality, population, and public services.

Based on discussions with the City, the proposed residential uses will be a combination of apartments and townhomes. Currently, the Old Town Planning Area contains a mix of residential uses and various commercial uses. Since the majority of properties located within the Planning Area are developed, the majority of the proposed land uses are expected to be infill development.⁷² The Revitalization Project is intended to expand the purpose and reach of the “Santa Fe Commercial District” zoning code provisions, creating a new zone called “Old Town,” with revised development standards to accommodate the goals of the community and the Revitalization Plan. The Revitalization Plan has also been prepared to be consistent with the Transit Oriented Development Packing House District plan and development codes for the area to the immediate south of the Old Town, separated by the Burlington Northern-Santa Fe (BNSF) railroad tracks. The proposed Revitalization Project’s implementation will not physically divide or disrupt an established community since the existing residential neighborhoods will be maintained.

B. Would the project conflict with an applicable land use plan, policy, or regulation of an agency with jurisdiction over the project (including but not limited to, a general plan, specific plan, local coastal program, or zoning ordinance) adopted for the purpose of avoiding or mitigating an environmental effect? No Impact.

The current General Plan designations for the Planning Area include Commercial, High Density Residential, and Medium Density Residential (refer to Exhibit 3-6). The Planning Area currently includes the “SF-C” (Santa Fe Commercial) zone and some surrounding parcels in the “C-1” (Neighborhood Commercial), “C-2” (Community Commercial), and “R-2” (Low-Medium Density Residential) zones. The existing Zoning designations for the Planning Area are shown in Exhibit 3-5.

⁷¹ Placentia, City of. *Old Town Placentia Revitalization Plan Development Standards*. May 2017.

⁷² Ibid.

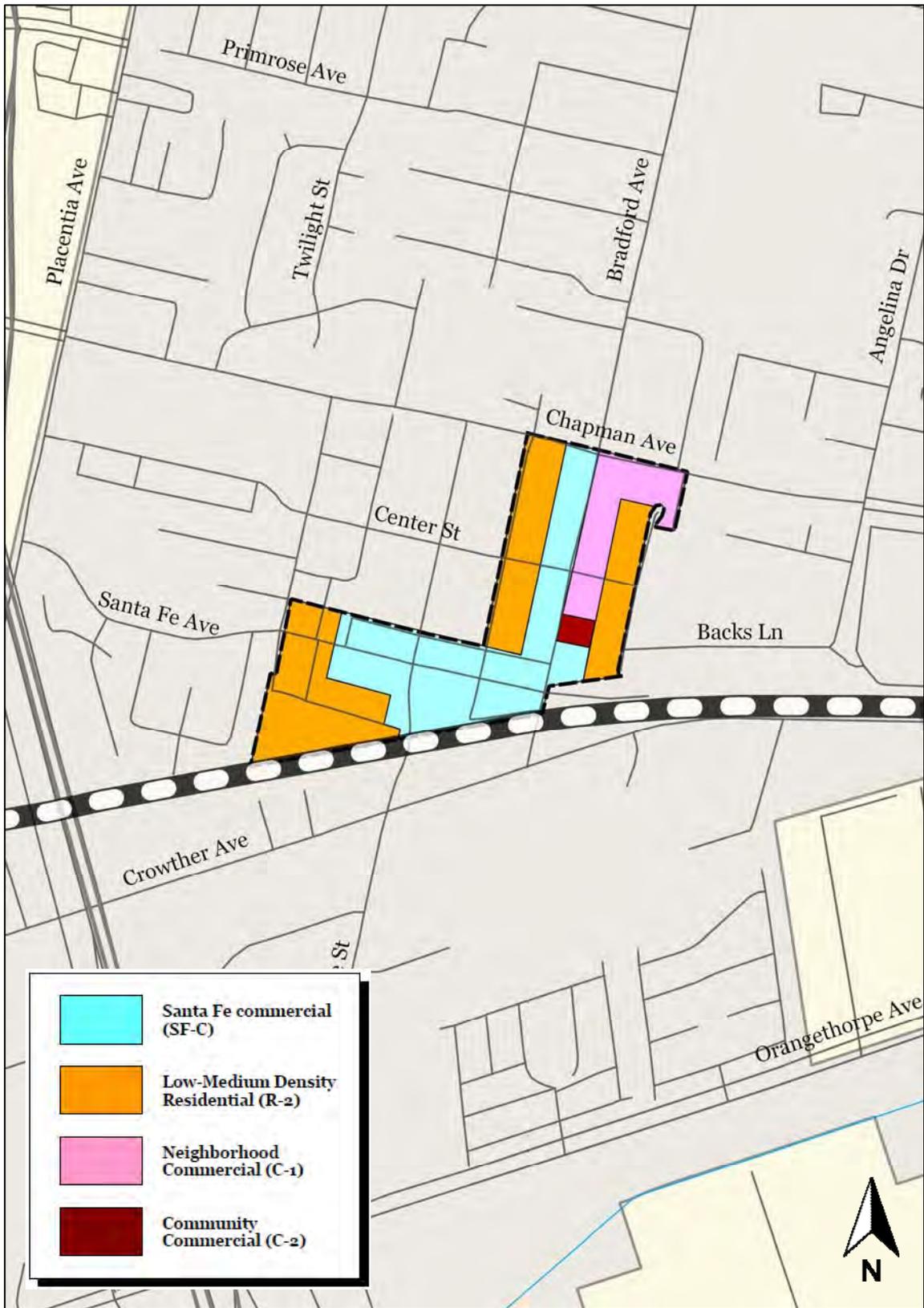


EXHIBIT 3-5
EXISTING ZONING MAP FOR THE PLANNING AREA

Source: QGIS

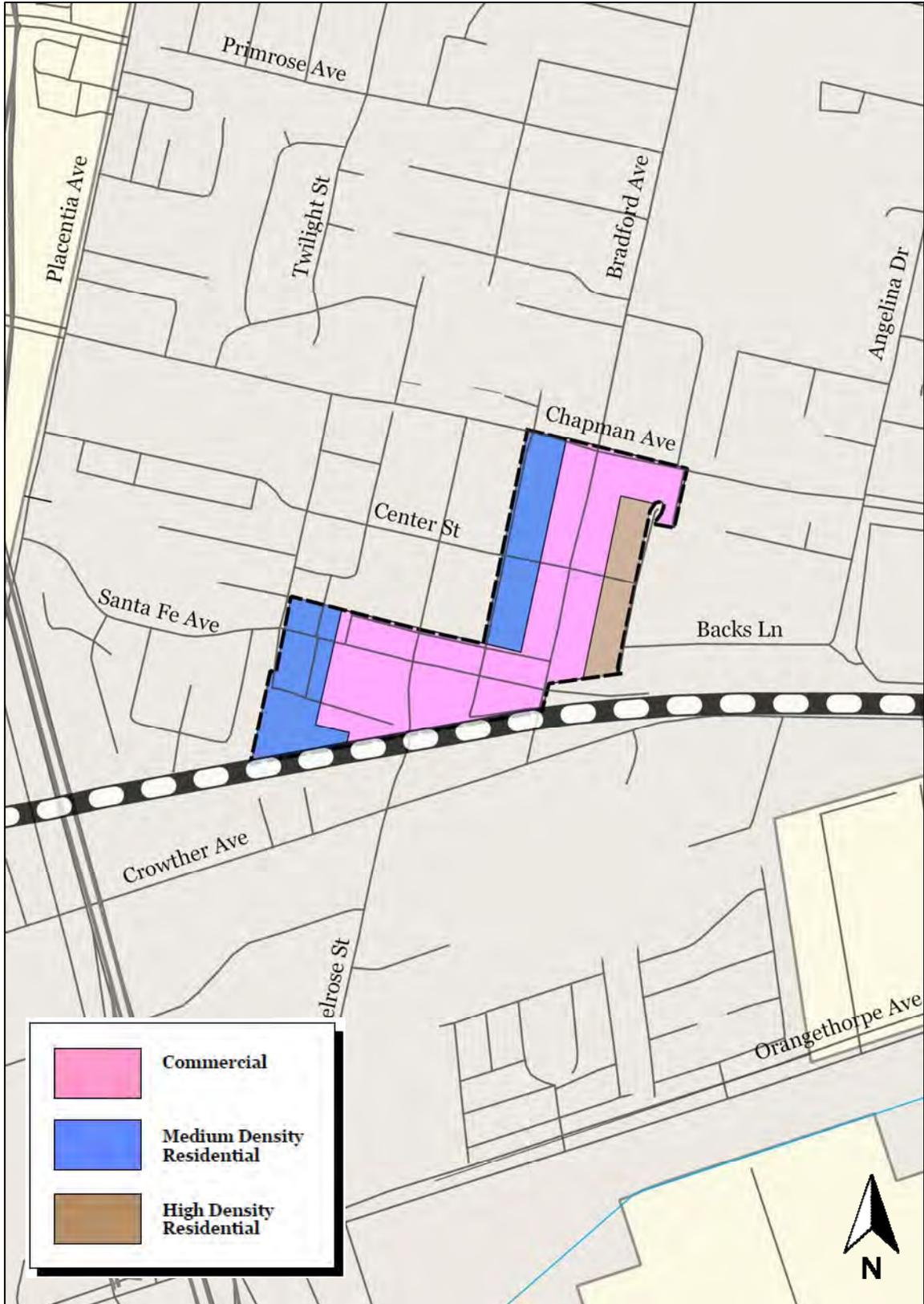


EXHIBIT 3-6
EXISTING GENERAL PLAN MAP FOR THE PLANNING AREA

Source: QGIS

A zoning text amendment, referred to as the Old Town Placentia Revitalization Development Standards herein, is being concurrently processed with adoption of the Revitalization Project. An amendment to the City's General Plan land use map is also being processed concurrently with the adoption of the Revitalization Project to reflect the land use designations envisioned within the Revitalization Plan document. With proposed zoning text and General Plan land use map amendments, the Revitalization Project is intended to be consistent with the City's General Plan, Zoning Code, Municipal Codes, and regional planning initiatives particularly for climate action, sustainability, multimodal transportation, complete streets, and livability.

The Plan proposes the area to be rezoned as "Old Town" and further divides the Old Town into five planning sub-areas to guide new development, infill, preservation, and reintegration of land uses. These sub-areas were drawn around distinctive areas or locations in the Plan area, with distinctive standards (refer to Exhibit 3-7). These five planning sub-areas include Main Street (Two- Three- and Four-Story Height Sub-areas), Village (Three-Story Height Sub-area), Mixed-Use (Four-Story Height District), High-Density Residential (Four-Story Height Sub-area), and Public Facilities (Five Story Height Sub-area).

The major objectives of the Revitalization Project are to identify land use options that include providing new mixed-uses, and increasing housing opportunities and neighborhood-serving retail uses. Furthermore, there will also be a regional benefit in terms of a reduction in vehicle miles traveled (VMT) because it is classified as an infill project that is consistent with the regional and State sustainable growth objectives identified in the State's Strategic Growth Council (SGC).⁷³ The proposed project will provide employment opportunities for local residents, local shopping and dining establishments, and residential uses, thereby reducing VMT and reducing trip length for vehicle trips in the surrounding area. As a result, no impacts related to the adoption and subsequent implementation of the Revitalization Project will occur.

C. Will the project conflict with any applicable habitat conservation plan or natural community conservation plan? No Impact.

The Planning Area is currently developed and no natural habitats are found on the within the area. The Planning Area is not located within an area governed by a habitat conservation or community conservation plan. The nearest ecological reserve is the Coal Canyon Ecological Reserve, located 10.3 miles east of the Planning Area.⁷⁴ The Revitalization Project will be restricted to the Planning Area will not encroach upon the ecological reserve. As a result, no impacts will occur.

⁷³ California Strategic Growth Council. <http://www.sgc.ca.gov/Initiatives/infill-development.html>. Promoting and enabling sustainable infill development is a principal objective of the SGC because of its consistency with the State Planning Priorities and because infill furthers many of the goals of all of the Council's member agencies. Focusing growth toward infill areas takes development pressure off conservation lands and working lands; it increases transit rider-ship and reduces vehicle trips; it requires less per capita energy and water use than less space-efficient development; it improves public health by promoting active transportation and active lifestyles; and it provides a more equitable mix of housing choices, among other benefits. Thus, the SGC has been investigating actions that can be taken to improve the ability of local governments and private developers to successfully plan and build good infill projects.

⁷⁴ California Department of Fish and Wildlife. *Ecological Reserves and Wildlife Areas in California*. <https://www.wildlife.ca.gov/lands/places-to-visit>.

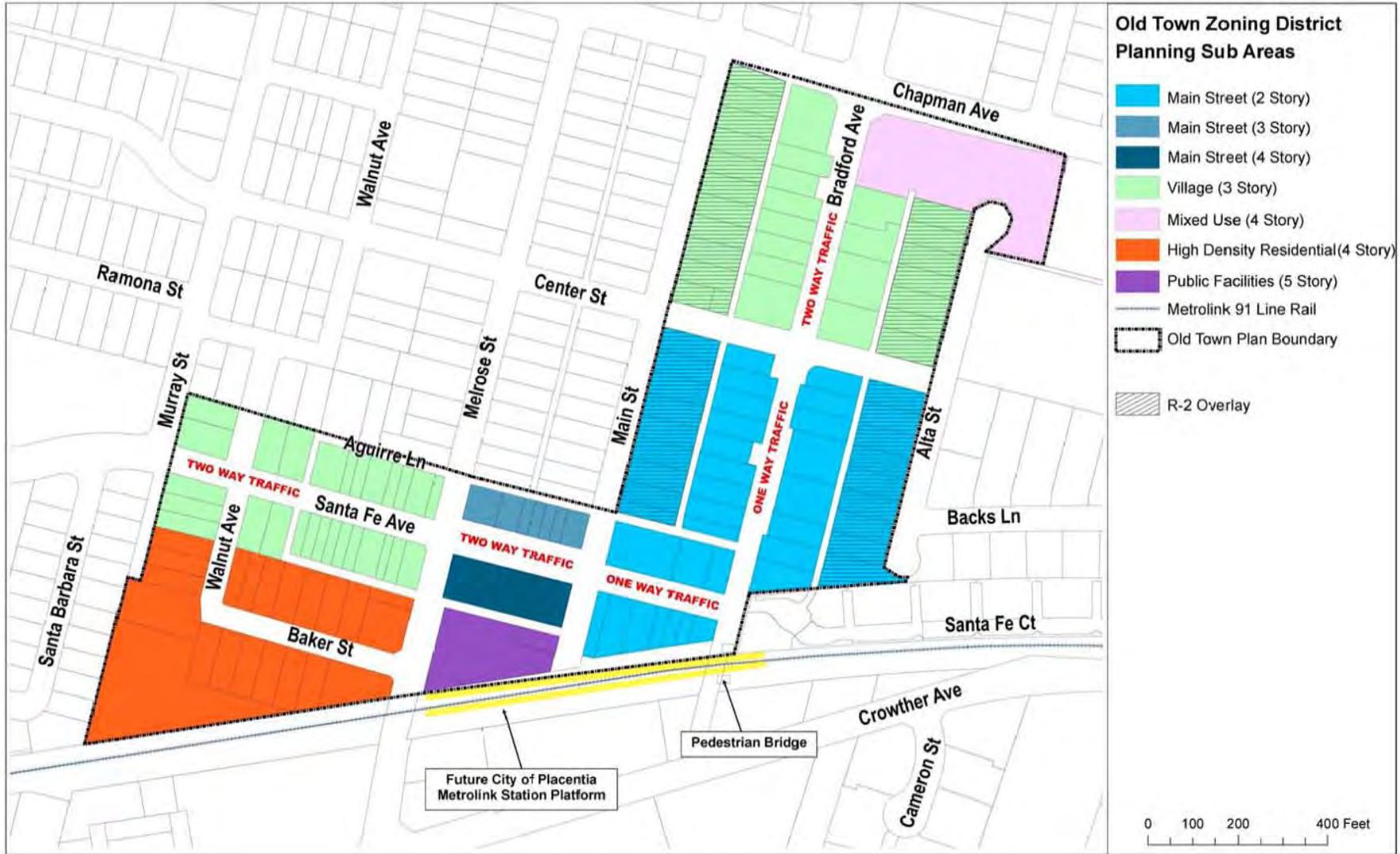


EXHIBIT 3-7
PROPOSED OLD TOWN ZONING DISTRICT PLANNING SUB-AREAS

Source: City of Placentia

3.10.3 MITIGATION MEASURES

The adoption and subsequent implementation of the Old Town Placentia Revitalization Project will not lead to any significant impacts. An amendment to the General Plan land use map and a zoning text amendment are being concurrently processed with the Revitalization Project. With the General Plan and zoning code amendments, the Revitalization Plan is intended to be consistent with the City's General Plan, Zoning Code, Municipal Codes, and regional planning initiatives particularly for climate action, sustainability, multimodal transportation, complete streets, and livability. As a result, no additional mitigation beyond that which may be required for individual development projects is required.

3.11 MINERAL RESOURCES

3.11.1 THRESHOLDS OF SIGNIFICANCE

According to the City of Placentia, acting as Lead Agency, a project may be deemed to have a significant adverse impact on energy and mineral resources if it results in any of the following:

- The loss of availability of a known mineral resource that would be of value to the region and the residents of the state; or,
- The loss of availability of a locally important mineral resource recovery site delineated on a local general plan, specific plan, or other land use plan.

3.11.2 ANALYSIS OF ENVIRONMENTAL IMPACTS

A. *Would the project result in the loss of availability of a known mineral resource that would be of value to the region and the residents or the state? No Impact.*

The Planning Area is not located in a Significant Mineral Aggregate Resource Area (SMARA) nor is it located in an area with active mineral extraction activities. A review of California Division of Oil, Gas, and Geothermal Resources Well Finder indicates that there are no wells located within the Planning Area.⁷⁵ The nearest wells to the Planning Area are located approximately 285 feet east of the northern Alta Street terminus, and approximately 100 feet south of Santa Fe Avenue, within Santa Fe Park.⁷⁶ The first well mentioned is currently plugged, and the second well is buried.⁷⁷

In addition, according to the Generalized Mineral Land Classification of Orange County, the Planning Area is located in Mineral Resource Zone (MRZ) boundary number three (MRZ-3). Areas located in MRZ-3 are classified as areas where the significance of mineral deposits cannot be determined from the available

⁷⁵ California, State of. Department of Conservation. *California Oil, Gas, and Geothermal Resources Well Finder*. <http://www.conservation.ca.gov/dog/Pages/Wellfinder.aspx>.

⁷⁶ Google Earth. Website accessed May 9, 2017.

⁷⁷ California, State of. Department of Conservation. *Well Details*. <https://secure.conservation.ca.gov/WellSearch/Details?api=05901179>.

data.⁷⁸ Although the Planning Area is located in MRZ-3, the implementation of the proposed project will not interfere with any active mineral resource extractions. There are a total of five active mineral resource areas in Orange County. These areas include the Santa Ana River Resource Area, the Lower Santiago Creek Resource Area, the Upper Santiago Creek Resource Area, the Arroyo Trabuco Resource Area, and the San Juan Creek Resource Area.⁷⁹ None of these resource areas are located near the Planning Area. As a result, no impacts to mineral resources will occur.

B. Would the project result in the loss of availability of a locally important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan? No Impact.

As previously mentioned, no mineral, oil, or energy extraction and/or generation activities are located within the Planning Area or in the vicinity. Moreover, the proposed project will not interfere with any well-extraction activities. Therefore, no impacts will result from the implementation of the proposed project.

3.11.3 MITIGATION MEASURES

The adoption and subsequent implementation of the Old Town Placentia Revitalization Project will not lead to any impacts related to mineral resources. In addition, the goals, policies, and implementation programs contained within the Revitalization Project will also further mitigate the potential impacts from new development contemplated as part of the implementation of the Revitalization Project. As a result, no additional mitigation beyond that which may be required for individual development projects is required.

3.12 NOISE

3.12.1 THRESHOLDS OF SIGNIFICANCE

According to the City of Placentia, acting as Lead Agency, a project may be deemed to have a significant impact on the environment if it results in any of the following:

- The exposure of persons to, or the generation of, noise levels in excess of standards established in the local general plan, noise ordinance or applicable standards of other agencies;
- The exposure of people to, or generation of, excessive ground-borne noise levels;
- A substantial permanent increase in ambient noise levels in the vicinity of the project above levels existing without the project;
- A substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project;

⁷⁸ California, State of. Department of Conservation. *Generalized Mineral Land Classification of Orange County, California*. ftp://ftp.consrv.ca.gov/pub/dmg/pubs/ofr/OFR_94-15/OFR_94-15_Plate_1.pdf

⁷⁹ California, State of. Department of Conservation. *Update of Mineral Land Classification of Portland Cement Concrete Aggregate in Ventura, Los Angeles, and Orange Counties, California, Part III: Orange County*. Report dated 1994. ftp://ftp.consrv.ca.gov/pub/dmg/pubs/ofr/OFR_94-15/OFR_94-15_Text.pdf

- Locating within an area governed by an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or private use airport, where the project would expose people to excessive noise levels; or,
- Locating within the vicinity of a private airstrip that would result in the exposure of people residing or working in the Planning Area to excessive noise levels.

3.12.2 ANALYSIS OF ENVIRONMENTAL IMPACTS

A. *Would the project result in exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies? Less than Significant Impact.*

Noise levels may be described using a number of methods designed to evaluate the “loudness” of a particular noise. The most commonly used unit for measuring the level of sound is the decibel (dB). Zero on the decibel scale represents the lowest limit of sound that can be heard by humans. The eardrum may rupture at 140 dB. An increase of between 3.0 dB and 5.0 dB is the ambient noise level considered to represent the threshold for human sensitivity. Noise levels associated with everyday activities are illustrated in Exhibit 3-8. The City of Placentia Municipal Code has established the following noise control standards for residential and commercial development:⁸⁰

- *All Residential Property (Zone 1): 55 dBA between 7:00 AM and 10:00 PM and 50 dBA between 10:00 PM and 7:00 AM; and,*
- *All Commercial Property (Zone 2): 65 dBA anytime.*

According to the City of Placentia municipal code, City noise standards are not to be exceeded:

- For a cumulative period of time more than 30 minutes in any hour; or,
- By five dBA for a cumulative period of more than 15 minutes in any hour; or,
- By 10 dBA for a cumulative period of more than five minutes in any hour; or,
- By 15 dBA for a cumulative period of more than one minute in any hour; or,
- By 20 dBA for any period of time.

Since the proposed project involves the development of both commercial and residential zones, the City’s noise ordinance states that the noise standard applicable to the affected property shall apply. As a result, noise emanating from commercial properties should not exceed 55 dbA when adjacent to residential properties.

⁸⁰ Placentia, City of. *Municipal Code, Title 23 Zoning, Section 23.76.050 Exterior Noise Standards.*

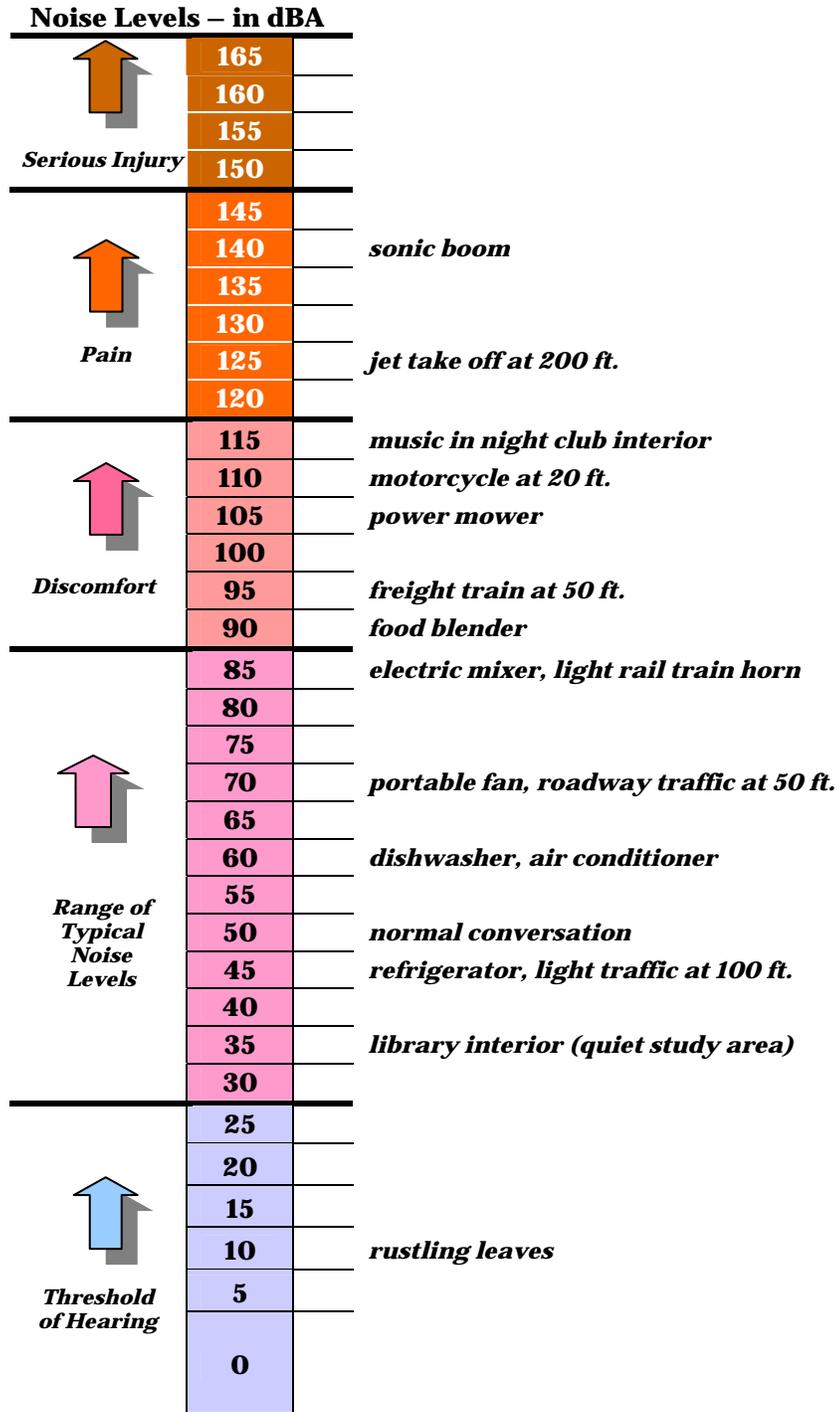


EXHIBIT 3-8 TYPICAL NOISE SOURCES AND LOUDNESS SCALE

Source: Blodgett Baylosis Environmental Planning

Noise sources associated with grading, construction, and the maintenance of real property are not subject to the abovementioned provisions. However, the City limits the use of power construction tools or equipment to specific time periods.⁸¹ All grading is only permitted between the hours of 7:00 AM and 7:00 PM Monday through Friday, and between the hours of 9:00 AM and 6:00 PM on Saturday, and are prohibited on Sundays and on all Federal holidays, unless performing emergency work. Permitted hours for construction activities and the maintenance of real property are outlined within the City of Placentia municipal code and listed below in Table 3-3.

Table 3-3
Permitted Hours for Construction Activities and the Maintenance of Real Property within the City of Placentia

	Monday-Friday	Saturday	Sunday and Holidays
Initial Construction	7:00 AM to 7:00 PM	9:00 AM to 6:00 PM	Prohibited
Remodeling, Repair Work	7:00 AM to 7:00 PM	9:00 AM to 6:00 PM	10:00 AM to 5:00 PM
Maintenance of Real Property	7:00 AM to 7:00 PM	9:00 AM to 6:00 PM	10:00 AM to 5:00 PM

Source: City of Placentia Municipal Code

Noise monitoring was conducted using a Sper Scientific digital sound level meter Model 840029. A total of 400 noise measurements were taken at four different areas within the Old Town Placentia Planning Area: the Calvary Church parking lot (102 Bradford Avenue), the southwest corner of Bradford Avenue and Santa Fe Avenue, the northeast corner of Santa Fe Avenue and Aguirre Lane, and the cul-de-sac at the eastern terminus of Baker Street. The measurements were taken in between the hours of 11:00 AM and 12:00 PM on May 10, 2017. The average noise levels at the first measurement location (Calvary Church parking lot) were 56.3 dBA; the average noise levels at the second measurement location (southwest corner of Bradford Avenue and Santa Fe Avenue) were 56.4 dBA; the average noise levels at the third measurement location (northeast corner of Santa Fe Avenue and Aguirre Lane) were 57.2 dBA; finally, the average noise levels at the fourth measurement location (the cul-de-sac at the eastern terminus of Baker Street) were 53.5 dBA.⁸² The results of the four noise measurement surveys are graphically depicted in Exhibit 3-9.

A change in traffic noise levels of between 3.0 dBA and 5.0 dBA is generally considered to be the limit where the change in the ambient noise levels may be perceived by persons with normal hearing. The streetscape plan, building design, and other development standards will be effective in attenuating any increased traffic noise. In addition, the future land uses and development will be required to comply with the City's noise control requirements.

⁸¹ Placentia, City of. *Municipal Code, Title 23 Zoning, Section 23.81.170 Grading, Construction and Maintenance of Real Property.*

⁸² Blodgett Baylosis Environmental Planning. *Second Site Survey.* Survey was completed on May 10, 2017.

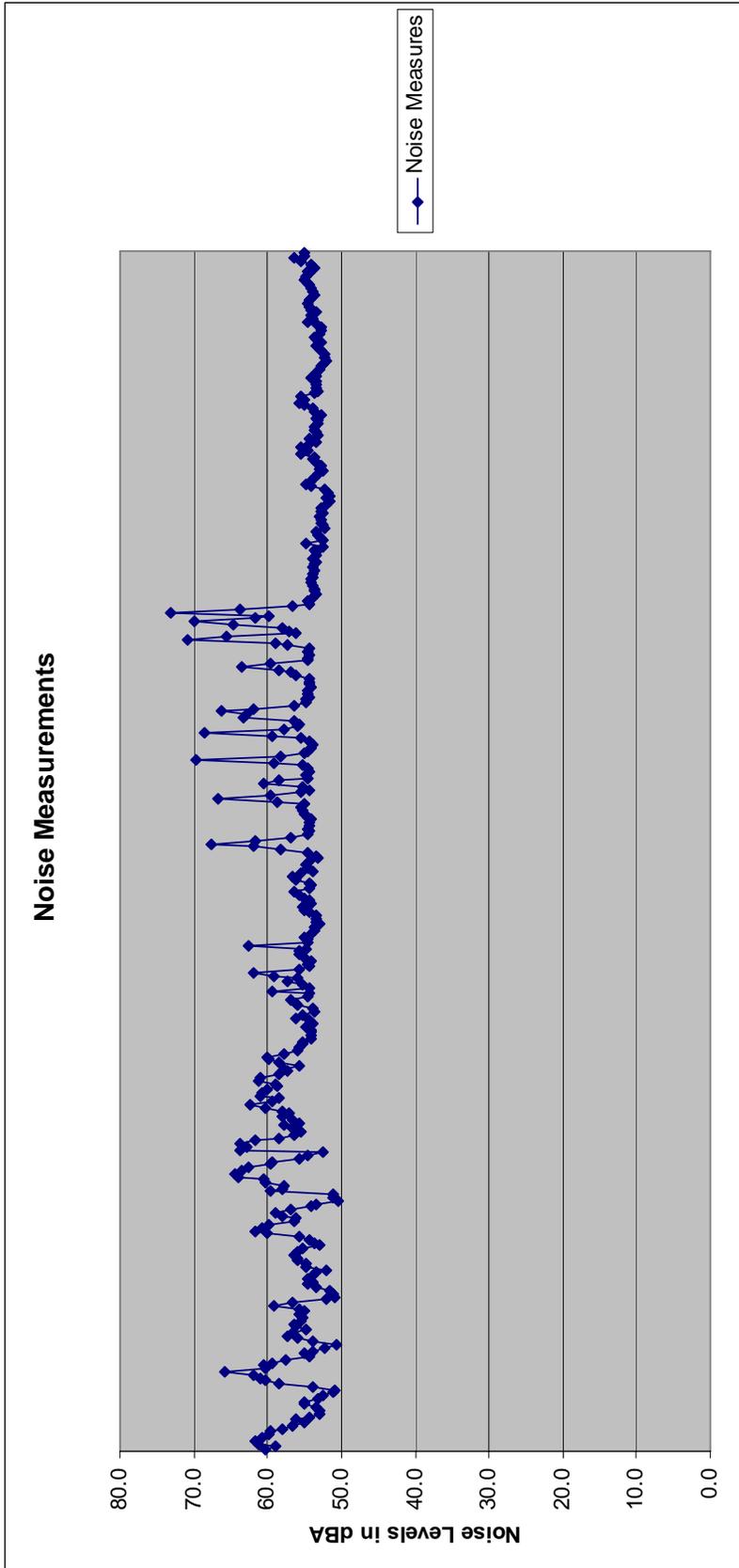


EXHIBIT 3-9
NOISE MEASUREMENT RESULTS
Source: Blodgett Baylosis Environmental Planning

The current noise environment within the Planning Area is dominated by traffic noise emanating from nearby arterial roadways and the railroad track. The levels of noise emanating from local traffic are dependent on various factors. For example, the speed of the traffic is more influential to noise levels than the volume of traffic. Although traffic levels will increase in the Planning Area, various development standards will result in traffic calming. Traffic calming involves the incorporation of physical design and other measures to deliberately reduce traffic speeds. Physical design elements may include speed bumps, the narrowing of roads, and the installation of raised pedestrian crossings. As a result, less than significant impacts will occur.

B. Would the project result in exposure of people to or generation of excessive ground-borne noise levels? Less than Significant Impact.

As indicated in the previous section, a change in traffic noise levels of between 3.0 dBA and 5.0 dBA is generally considered to be the limit where the change in the ambient noise levels may be perceived by persons with normal hearing. It typically requires a doubling of traffic volumes to register a perceptible increase in traffic noise. The current noise environment within the Planning Area is dominated by traffic noise emanating from nearby roadways and the neighboring railroad track. The levels of noise emanating from local traffic are dependent on various factors. For example, the speed of the traffic is more influential to noise levels than the volume of traffic. Although traffic levels will increase in the Planning Area, various development standards will result in traffic calming. Traffic calming involves the incorporation of physical design and other measures to deliberately reduce traffic speeds. Physical design elements may include speed bumps, the narrowing of roads, and the installation of raised pedestrian crossings. Traffic calming will be effective in reducing any potential ground-borne noise levels. As a result, less than significant impacts will occur.

C. Would the project result in a substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project? Less than Significant Impact.

A change in traffic noise levels of between 3.0 dBA and 5.0 dBA is generally considered to be the limit where the change in the ambient noise levels may be perceived by persons with normal hearing. The streetscape plan, building design, and other development standards will be effective in attenuating any increased traffic noise. In addition, the future land uses and development will be required to comply with the City's noise control requirements. The current noise environment within the Planning Area is dominated by traffic noise emanating from nearby arterial roadways and the railroad track. The levels of noise emanating from local traffic are dependent on various factors. For example, the speed of the traffic is more influential to noise levels than the volume of traffic. Although traffic levels will increase in the Planning Area, various development standards will result in traffic calming. Traffic calming involves the incorporation of physical design and other measures to deliberately reduce traffic speeds. Physical design elements may include speed bumps, the narrowing of roads, and the installation of raised pedestrian crossings. As a result, less than significant impacts will occur.

D. Would the project result in a substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project? Less than Significant Impact.

Noise levels associated with various types of construction equipment are summarized in Exhibit 3-10. The noise levels are those that would be expected at a distance of 50 feet from the noise source. Composite construction noise is best characterized in a study prepared by Bolt, Beranek, and Newman. In the study, the noisiest phases of construction are anticipated to be 89 dBA as measured at a distance of 50 feet from the construction activity. In later phases during building erection, noise levels are typically reduced from these values and the physical structures further break up line-of-sight noise. However, as a worst-case scenario, the 89 dBA value was used as an average noise level for the construction activities at 50 feet from the noise sources. Typical noise levels from various types of construction equipment are shown in Exhibit 3-10.

Two types of noise impacts could occur during the construction phase: the transport of workers and equipment to the construction site, which would incrementally increase noise levels along access roadways. Composite construction noise is best characterized by Bolt, Beranek, and Newman.⁸³ In this study, the noisiest phases of construction for development is presented as 89 dBA as measured at a distance of 50 feet from the construction effort. As previously mentioned, noise sources associated with grading, construction, and the maintenance of real property are not subject to the abovementioned provisions. However, the City limits the use of power construction tools or equipment to specific time periods.⁸⁴ As a result, the impacts related to the adoption and subsequent implementation of the Revitalization Project will be less than significant.

E. For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels? No Impact.

The Planning Area is not located within two miles of an operational public airport. The nearest public use airport is the Fullerton Municipal Airport and it is located approximately 5.75 miles west of the Planning Area. The nearest major airport is located in Long Beach approximately 12.7 miles to the south.⁸⁵ As a result, no significant adverse impacts related to the exposure of persons to aircraft noise from a public use airport are anticipated.

⁸³ USEPA, Protective Noise Levels. 1971

⁸⁴ Placentia, City of. *Municipal Code, Title 23 Zoning, Section 23.81.170 Grading, Construction and Maintenance of Real Property.*

⁸⁵ Toll Free Airline. *Orange County Public and Private Airports, California.* <http://www.tollfreeairline.com/california/orange.htm>.

Typical noise levels 50-ft. from source

70 80 90 100

Equipment Powered by Internal Combustion Engines	Earth Moving Equipment	Compactors (Rollers)					
		Front Loaders					
		Backhoes					
		Tractors					
		Scrapers, Graders					
		Pavers					
		Trucks					
	Materials Handling Equipment	Concrete Mixers					
		Concrete Pumps					
		Cranes (Movable)					
		Cranes (Derrick)					
	Stationary Equipment	Pumps					
		Generators					
Compressors							
Impact Equipment	Pneumatic Wrenches						
	Jack Hammers						
	Pile Drivers						
Other Equipment	Vibrators						
	Saws						

EXHIBIT 3-10
TYPICAL CONSTRUCTION NOISE LEVELS

Source: Blodgett Baylosis Environmental Planning

F. Within the vicinity of a private airstrip, would the project expose people residing or working in the project area to excessive noise levels? No Impact.

The Planning Area is not located within two miles of a private airstrip.⁸⁶ As a result, no noise impacts related to the exposure of persons to aircraft noise from a private airstrip will result from the proposed project.

3.12.3 MITIGATION MEASURES

The adoption and subsequent implementation of the Old Town Placentia Revitalization Project will not lead to any significant impacts. The goals, policies, and implementation programs contained within the Revitalization Project will also further mitigate the potential impacts from new development contemplated as part of the implementation of the Revitalization Project. As a result, no additional mitigation beyond that which may be required for individual development projects is required.

3.13 POPULATION & HOUSING

3.13.1 THRESHOLDS OF SIGNIFICANCE

According to the City of Placentia, acting as Lead Agency, a project may be deemed to have a significant impact on housing and population if it results in any of the following:

- A substantial growth in the population within an area, either directly or indirectly related to a project;
- The displacement of a substantial number of existing housing units, necessitating the construction of replacement housing; or,
- The displacement of substantial numbers of people, necessitating the construction of replacement housing.

3.13.2 ANALYSIS OF ENVIRONMENTAL IMPACTS

A. Would the project induce substantial population growth in an area, either directly or indirectly (e.g., through projects in an undeveloped area or extension of major infrastructure)? Less than Significant Impact.

Based on discussions with the City, the proposed residential uses will be a combination of apartments and townhomes. Currently, the Old Town area contains a mix of single-family residential use and various commercial uses. As we understand, the majority of properties in the area are developed. Therefore, the majority of the proposed land uses are expected to be infill development. No specific new projects are known at this time.

⁸⁶ Toll Free Airline. *Orange County Public and Private Airports, California.* <http://www.tollfreeairline.com/california/orange.htm>.

Although the residential development envisioned within the Revitalization Project may be extensive, environmental analysis will be required for all new residential developments subject to CEQA.⁸⁷ The environmental analysis for each residential development will account for the population growth anticipated for the specific development and that of any neighboring development that may be simultaneously constructed. Although there will be significant population growth, the commercial development contemplated within the Revitalization Project will be able to support the population growth. Furthermore, the revitalization of the Planning Area would be classified as infill development, which is beneficial because it would be effective in reducing urban sprawl and the overall vehicle miles traveled (VMT) by being located on an underutilized area within a developed area. Finally, the residential development within the proposed High Density Residential zone will be “by right,” pursuant to the State of California housing law, thereby allowing the streamlining of City approval for legally-complying residential projects. As a result, less than significant impacts will occur.

B. Would the project displace substantial numbers of existing housing, necessitating the construction of replacement housing elsewhere? No Impact.

Based on the development standards in the zone change, the proposed residential uses will be a combination of apartments and townhomes. Currently, the Old Town area contains a mix of residential uses and various commercial uses. The majority of properties in the area are developed. Therefore, the majority of the proposed land uses are expected to be infill development. Although the implementation of the Revitalization Project may result in the reuse of existing residential structures, the potential new development will result in the construction of up to 525 residential units. Therefore, a substantial number of existing housing will not be displaced and no adverse impacts will result.

C. Would the project displace substantial numbers of people, necessitating the construction of replacement housing elsewhere? No Impact.

Based on the proposed development standards in the zone changes, the proposed residential uses will be a combination of apartments and townhomes. Currently, the Old Town area contains a mix of residential uses and various commercial uses. As we understand, the majority of properties in the area are developed. Therefore, the majority of the proposed land uses are expected to be infill development. No specific new projects are known at this time, though the potential new development involves the construction of 525 residential units. As a result, a substantial number of people will not be displaced and no adverse impacts will result.

3.13.3 MITIGATION MEASURES

Although the residential development envisioned within the Revitalization Project may be extensive, environmental analysis will be required for all new residential developments subject to CEQA.⁸⁸ The environmental analysis for each residential development will account for the population growth anticipated for the specific development and that of any neighboring development that may be

⁸⁷ California, State of. *Title 14. California Code of Regulations. Chapter 3. Guidelines for the Implementation of the California Environmental Quality Act.* As Amended 1998 (CEQA Guidelines). § 15060 – 15065.

⁸⁸ Ibid.

simultaneously constructed. Furthermore, the revitalization of the Planning Area would be classified as infill development, which is beneficial because it would be effective in reducing urban sprawl and the overall vehicle miles traveled (VMT) by being located on an underutilized area within a developed area. As a result, no additional mitigation beyond that which may be required for individual development projects is required.

3.14 PUBLIC SERVICES

3.14.1 THRESHOLDS OF SIGNIFICANCE

According to the City of Placentia, acting as Lead Agency, a project may be deemed to have a significant adverse impact on public services if it results in any of the following:

- A substantial adverse physical impact associated with the provision of new or physically altered governmental facilities, the construction of which would cause significant environmental impact in order to maintain acceptable service ratios, response times, or other performance objectives relative to fire department services;
- A substantial adverse physical impact associated with the provision of new or physically altered governmental facilities, the construction of which would cause significant environmental impact in order to maintain acceptable service ratios, response times, or other performance objectives relative to law enforcement services;
- A substantial adverse physical impact associated with the provision of new or physically altered governmental facilities, the construction of which would cause significant environmental impact in order to maintain acceptable service ratios, response times, or other performance objectives relative to school services; or,
- A substantial adverse physical impact associated with the provision of new or physically altered governmental facilities, the construction of which would cause significant environmental impact in order to maintain acceptable service ratios, or other performance objectives relative to other government services.

3.14.2 ANALYSIS OF ENVIRONMENTAL IMPACTS

- A. *Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, the construction of which would cause significant environmental impacts in order to maintain acceptable service ratios, response times, or other performance objectives relative to fire department services? Less than Significant Impact.*

The Director of the Orange county Fire Authority provides fire services under a joint powers agreement (JPA) to the City of Placentia. Battalion 2 provides firefighting services at two fire stations in Placentia. Fire Station 34 is located at 1530 North Valencia Avenue; Fire Station 35 is located at 120 South Bradford Avenue. The residential, commercial, and mixed-use development, once occupied, will be periodically inspected by the Orange County Fire Department. In addition, the Fire Department will review the

development plans to ascertain the nature and extent of any additional measures that may be required to meet any Fire Code requirements. The Fire Department currently reviews all new development plans, and future development will be required to conform to all fire protection and prevention requirements, including, but not limited to, building setbacks, emergency access, fire hydrants, interior sprinklers, etc. All new development will be state-of-the-art and will include modern fire suppression equipment and technology. The adoption and subsequent implementation of the Old Town Placentia Revitalization Project will not lead to any significant impacts because additional fire staff will not be needed now or in the near future. As a result, less than significant impacts will result.

B. Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, the construction of which would cause significant environmental impacts in order to maintain acceptable service ratios, response times, or other performance objectives relative to police services? Less than Significant Impact.

Law enforcement services in Placentia are provided by the Placentia Police Department. The Police Department is located in the Civic Center. As part of the Police Department's annual review, demand shall be evaluated and resources allocated as necessary. The adoption and subsequent implementation of the Old Town Placentia Revitalization Project will not lead to any significant impacts because additional police staff will not be needed now or in the near future. As a result, less than significant impacts will result.

C. Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, the construction of which would cause significant environmental impacts in order to maintain acceptable service ratios, or other performance objectives relative to school services? Less than Significant Impact.

The Planning Area is located within the service boundaries of the Placentia-Yorba Linda Unified School District. New development will be required to pay all pertinent development fees to the school district. Although the new residential development will result in an incremental increase of school-age students residing within the City, additional school services will not be needed now or in the near future. As a result, less than significant impacts will result.

D. Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, the construction of which would cause significant environmental impacts in order to maintain acceptable service ratios, response times or other performance objectives relative to other governmental services? Less than Significant Impact.

As previously mentioned, although the new commercial and residential development will result in an incremental increase in demand of governmental services, additional governmental services will not be needed now or in the near future. As a result, less than significant impacts will result.

3.14.3 MITIGATION MEASURES

Although the new commercial and residential development will result in an incremental increase in demand of public services, no significant demand for public services will occur within the immediate

future. As a result, no additional mitigation beyond that which may be required for individual development projects is required.

3.15 RECREATION

3.15.1 THRESHOLDS OF SIGNIFICANCE

According to the City of Placentia, acting as Lead Agency, a project may be deemed to have a significant adverse impact on the environment if it results in any of the following:

- The use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated; or,
- The construction or expansion of recreational facilities, which might have an adverse physical effect on the environment.

3.15.2 ANALYSIS OF ENVIRONMENTAL IMPACTS

A. Would the project increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated? Less than Significant Impact.

The City of Placentia operates 16 parks throughout the City. Although the new residential development will result in an incremental increase in demand of park facilities, additional recreational facilities will not be needed now or in the near future. Furthermore, no established park facilities will be displaced by future development that is contemplated as part of the Revitalization Project's implementation. As a result, less than significant impacts will result.

B. Would the project affect existing recreational facilities or require the construction or expansion of recreational facilities that might have an adverse physical effect on the environment? Less than Significant Impact.

As previously mentioned, although the new residential development will result in an incremental increase in demand of park facilities, additional recreational facilities will not be needed now or in the near future. Furthermore, no established park facilities will be displaced by future development that is contemplated as part of the Revitalization Project's implementation. As a result, less than significant impacts are anticipated.

3.15.3 MITIGATION MEASURES

Although the new residential development will result in an incremental increase in demand of recreational facilities, there will be no significant demand for recreational services within the near future. As a result, no additional mitigation beyond that which may be required for individual development projects is required.

3.16 TRANSPORTATION & CIRCULATION

3.16.1 THRESHOLDS OF SIGNIFICANCE

According to the City of Placentia, acting as Lead Agency, a project will normally have a significant adverse impact on traffic and circulation if it results in any of the following:

- A conflict with an applicable plan, ordinance, or policy establishing measures of effectiveness for the performance of the circulation system, taking into account all modes of transportation including mass transit and non-motorized travel and relevant components of the circulation system, including but not limited to, intersections, streets, highways and freeways, pedestrian and bicycle paths, and mass transit;
- A conflict with an applicable congestion management program, including but not limited to, level of service standards and travel demand measures, or other standards established by the County congestion management agency for designated roads or highways;
- Results in a change in air traffic patterns, including either an increase in traffic levels or a change in the location that result in substantial safety risks;
- Substantially increases hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment);
- Results in inadequate emergency access; or,
- A conflict with adopted policies, plans, or programs regarding public transit, bicycle, or pedestrian facilities, or otherwise decrease the performance or safety of such facilities.

3.16.2 ANALYSIS OF ENVIRONMENTAL IMPACTS

A. Would the project cause a conflict with an applicable plan, ordinance, or policy establishing measures of effectiveness for the performance of the circulation system, taking into account all modes of transportation including mass transit and non-motorized travel and relevant components of the circulation system, including but not limited to, intersections, streets, highways and freeways, pedestrian and bicycle paths, and mass transit)? Less than Significant Impact.

The Old Revitalization Project is bounded by Alta Street in the east, the Burlington Northern and Santa Fe Railway train tracks in the south, Chapman Avenue in the north and Murray Street in the west. The revitalization project consists of adding 525 residential units, 85,000 square feet of commercial use, 40,000 square feet of retail use, and a 50-room hotel to the existing area. The Traffic Impact Analysis (TIA) has been prepared consistent with the policies of the City of Placentia guidelines, discussions with the City staff, and methodologies from the Institute of Transportation Engineers (ITE) manuals.

The Revitalization Plan document lists various design options considered for one-way circulation patterns within the Planning Area. The preferred circulation design (Option #3) for the Old Town Planning Area proposes one-way traffic southbound on Bradford Avenue from Center Street to Santa Fe Avenue, and westbound on Santa Fe Avenue from Bradford Avenue to Main Street.

Based on discussions with the City, the analysis was conducted at the study intersections for the following scenarios as part of the TIA:

- 1) Existing (2017) Conditions;
- 2) Existing (2017) Plus Project Conditions;
- 3) Project Buildout Year (2037) Without Project Conditions; and,
- 4) Project Buildout Year (2037) With Project Conditions, including a new one way circulation pattern along Bradford Avenue and Santa Fe Avenue, as indicated above.

Regional access to the project area is provided by State Route 57 (SR-57). The project's traffic related impacts were evaluated at following eight intersections:

- 1) Chapman Avenue/SR-57 Southbound Ramps;
- 2) Chapman Avenue/SR-57 Northbound Ramps;
- 3) Chapman Avenue/Placentia Avenue;
- 4) Chapman Avenue/Murray Street;
- 5) Chapman Avenue/Melrose Street;
- 6) Chapman Avenue/Bradford Avenue;
- 7) Santa Fe Street/Melrose Street (All-Way Stop); and,
- 8) Center Street/Bradford Avenue (All-Way Stop)

Exhibit 3-11 illustrates the Planning Area's location and study intersections.

City of Placentia General Plan guidelines and the Orange County Congestion Management Program (CMP) require that the analysis of signalized intersections be performed using the Intersection Capacity Utilization (ICU) methodology. The assessment of intersection conditions addresses Levels of Service (LOS), in terms of volume-to-capacity (V/C) ratio under the ICU analysis for signalized intersections. Unsignalized intersections and intersections under jurisdiction of Caltrans require the analysis to be performed using the Highway Capacity Manual (HCM) methodology. The assessment of intersection conditions addresses LOS in terms of control delay for HCM analysis.

The Traffix Version 8.0 software package was used to determine intersection LOS based on ICU methodology and HCM methodology for the study intersections.

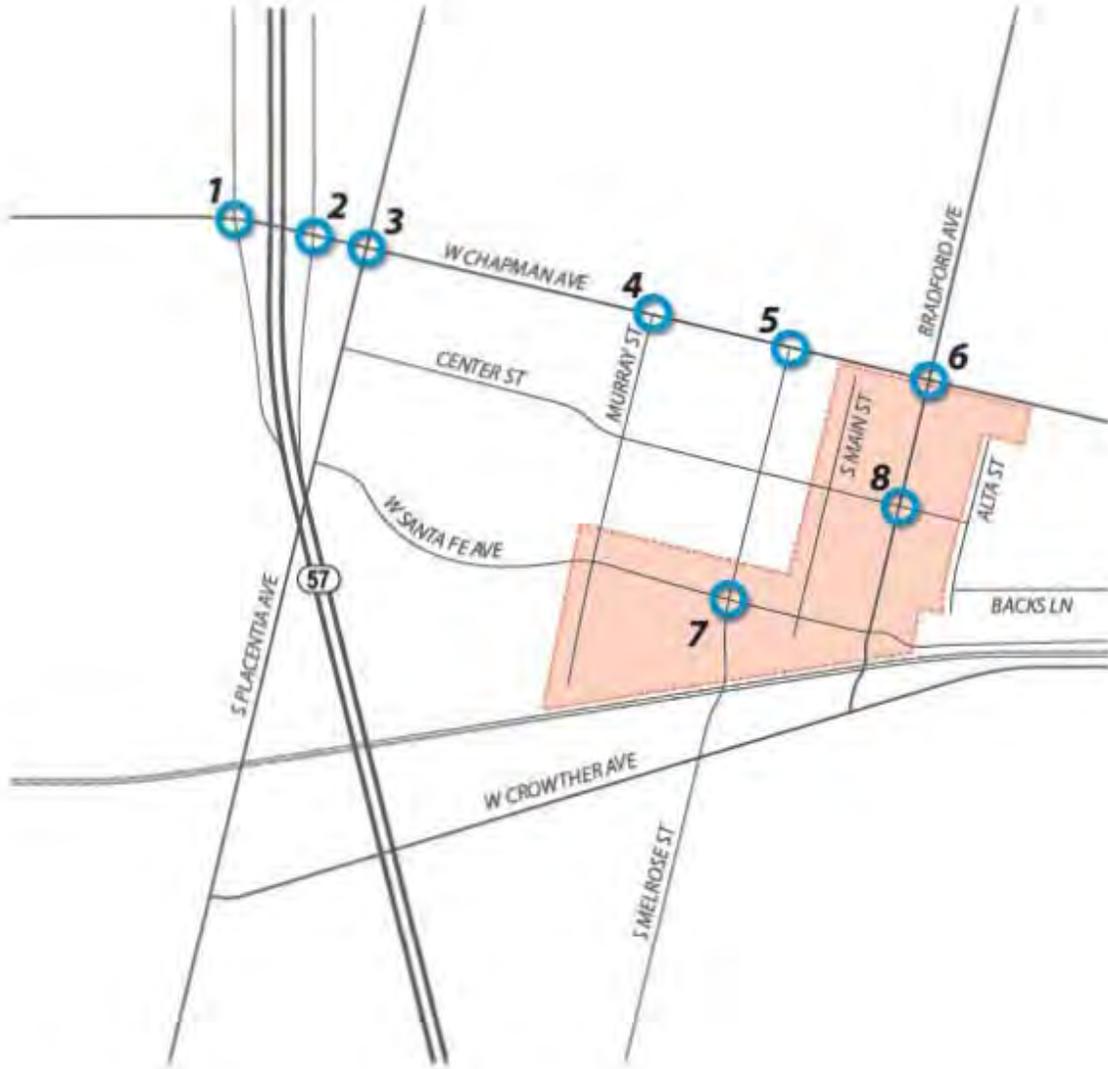
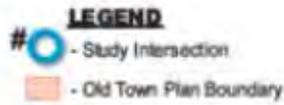


EXHIBIT 3-11
PLANNING AREA AND STUDY INTERSECTIONS
Source: DKS Associates

Brief LOS definitions along with the corresponding volume to capacity ratio for the ICU methodology are shown in Table 3-4.

Table 3-4
Level of Service ICU/VC Definitions

Level of Service	V/C Ratio
A	0.00-0.60
B	0.61-0.70
C	0.71-0.80
D	0.81-0.90
E	0.91-1.00
F	1.01 or greater

Source: Orange County Congestion Management Plan, 2015

The corresponding control delays for the HCM methodology for signalized and unsignalized intersections are shown in Tables 3-9 and 3-10, respectively.

Table 3-5
Level of Service Definitions for Signalized Intersections
Based on HCM Delay

Level of Service	Delay per Vehicle (in seconds)
A	≤ 0-10
B	> 10-20
C	> 20-35
D	> 35-55
E	> 55-80
F	> 80

Source: Highway Capacity Manual, Transportation Research Board, Special Report No. 209, Washington, D.C., 2000.

Table 3-6
Level of Service HCM Definitions

Level of Service	Delay per Vehicle (in seconds)
A	≤ 0-10
B	> 10-15
C	> 15-25
D	> 25-35
E	> 35-50
F	> 50

Source: Highway Capacity Manual, Transportation Research Board, Special Report No. 209, Washington, D.C., 2000.

The degree of congestion at an intersection is described by the level-of-service, which ranges from LOS A to LOS F, with LOS A representing free-flow conditions with little delay and LOS F representing over-saturated traffic flow throughout the peak hour. Table 3-7 provides a description of each specific LOS grade (LOS A through LOS F).

Table 3-7
Level of Service Descriptions

LOS	Description
A	No approach phase is fully utilized by traffic, and no vehicle waits longer than one red indication. Typically, the approach appears quite open, turns are made easily, and nearly all drivers find freedom of operation.
B	This service level represents stable operation, where an occasional approach phase is fully utilized and a substantial number are nearing full use. Many drivers begin to feel restricted within platoons of vehicles.
C	This level still represents stable operating conditions. Occasionally drivers may have to wait through more than one red signal indication, and backups may develop behind turning vehicles. Most drivers feel somewhat restricted, but not objectionably so.
D	This level encompasses a zone of increasing restriction approaching instability at the intersection. Delays to approaching vehicles may be substantial during short peaks within the peak period; however, enough cycles with lower demand occur to permit periodic clearance of developing queues, thus preventing excessive backups.
E	Capacity occurs at the upper end of this service level. It represents the most vehicles that any particular intersection approach can accommodate. Full utilization of every signal cycle is seldom attained no matter how great the demand.
F	This level describes forced flow operations at low speeds, where volumes exceed capacity. These conditions usually result from queues of vehicles backing up from a restriction downstream. Speeds are reduced substantially, and stoppages may occur for short or long periods of time due to the congestion. In the extreme case, both speed and volume can drop to zero.

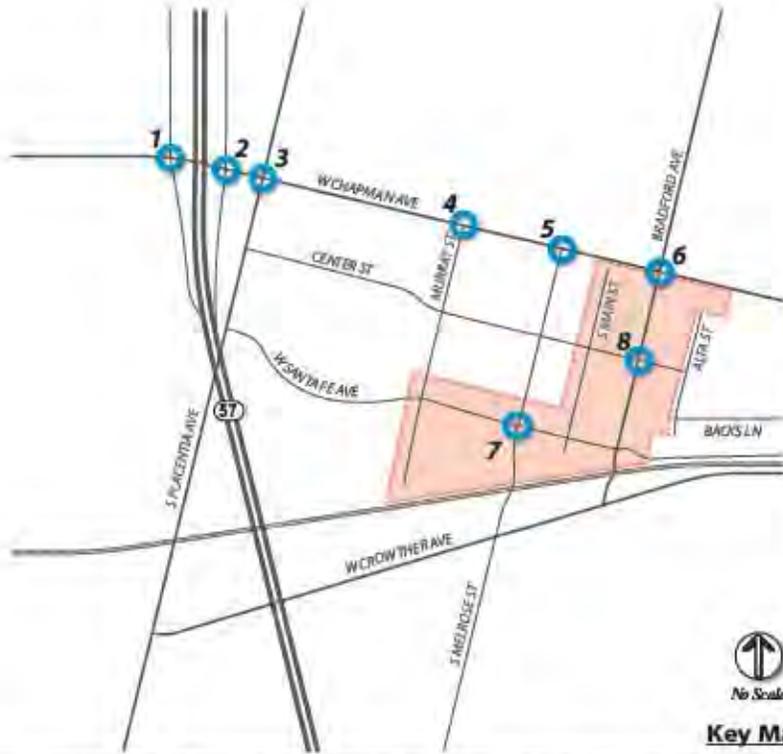
Source: Highway Capacity Manual, Transportation Research Board, Special Report No. 209, Washington, D.C., 2000.

Based on the City of Placentia’s Circulation Element, the acceptable level of service for all study intersections is LOS D or better. Therefore, any intersection operating at a LOS E or F will be considered deficient. An intersection is considered to be significantly impacted if the project-related increase in the V/C ratio equals or exceeds 0.01, if it is operating at LOS E or LOS F. For intersections under the jurisdiction of Caltrans, the significant impact criteria is based on the *Caltrans Guide for the Preparation of Traffic Impact Studies* document. Caltrans maintains a target LOS at the transition between LOS C and LOS D using the HCM methodology. Key roadways in the study area are summarized in Table 3-8 along with their existing characteristics. As shown, all surrounding roadways within the study area network are under the jurisdiction of City of Placentia, with the exception of the SR-57 Freeway.

**Table 3-8
 Existing Characteristics of Roadways in the Planning Area**

Roadway	Roadway Jurisdiction	Roadway Classification	Cross-Section	Posted Speed	Pedestrian Facilities	Bicycle Facilities	Transit Facilities
Chapman Avenue	City of Placentia	Primary Arterial	4 Lanes	35/40 mph	Sidewalks	None	OCTA
Placentia Avenue	City of Placentia	Secondary Arterial	4 Lanes	40 mph	Sidewalks	None	OCTA
Murray Street	City of Placentia	Local Street	2 Lanes	25 mph	Sidewalks	None	None
Melrose Street	City of Placentia	Secondary Arterial	2 Lanes	25 mph	Sidewalks	None	None
Bradford Avenue	City of Placentia	Secondary Arterial	2 Lanes	25 mph	Sidewalks	None	OCTA
Center Street	City of Placentia	Local Street	2 Lanes	25 mph	Sidewalks	None	None
Santa Fe Avenue	City of Placentia	Local Street	2/4 Lanes	25/35 mph	Sidewalks	None	None
SR-57 Freeway	Caltrans	Freeway	10 Lanes	60 mph	None	None	None

Exhibit 3-12 illustrates the existing roadway conditions for the study area roadways. The number of through traffic lanes and the existing intersection controls are identified. Existing traffic volumes at all study intersections were collected on Wednesday, April 26, 2017. The peak hours were determined by combining the four highest adjacent 15 minute periods during the AM peak period (7:00-9:00 AM) and the PM peak period (4:00-6:00 PM) at the intersections. Exhibit 3-13 illustrates the existing AM and PM peak hour traffic volumes at the study intersections. The actual counts are provided in Appendix A of the DKS Traffic Study. The existing LOS have been evaluated at the study intersections based on the ICU methodology for signalized intersections and the 2000 HCM methodology for all-way stop controlled and Caltrans intersections.



1. E Chapman Ave. @ SR-57 SB Ramp



N/S - Permitted
 EW - Protected

2. E Chapman Ave. @ SR-57 NB Ramp



N/S - Permitted
 EW - Protected

3. E Chapman Ave. @ Placentia Ave.



N/S - Protected
 EW - Protected

4. W Chapman Ave. @ Murray St.



N/S - Permitted
 EW - Permitted

5. W Chapman Ave. @ S Melrose St.



N/S - Permitted
 EW - Permitted

6. E Chapman Ave. @ S Bradford Ave.



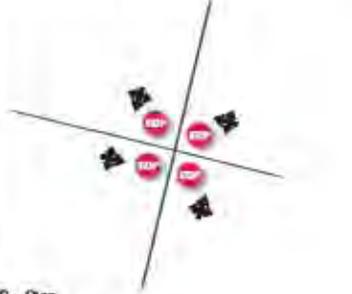
N/S - Permitted
 EW - Protected

7. W Santa Fe Ave. @ S Melrose St.



N/S - Stop
 EW - Stop

8. Center St. @ S Bradford Ave.



N/S - Stop
 EW - Stop

- LEGEND**
- # - Study Intersection
 - 🚦 - Traffic Signal
 - 🛑 - Stop Sign
 - 🚚 - Not Train Diverted Boundary
 - RTO - Right Turn Overlap
 - ← - Lane Configuration

EXHIBIT 3-12 EXISTING LANE CONFIGURATIONS

Source: DKS Associates

INITIAL STUDY & NEGATIVE DECLARATION • CITY OF PLACENTIA
 OLD TOWN PLACENTIA REVITALIZATION PROJECT

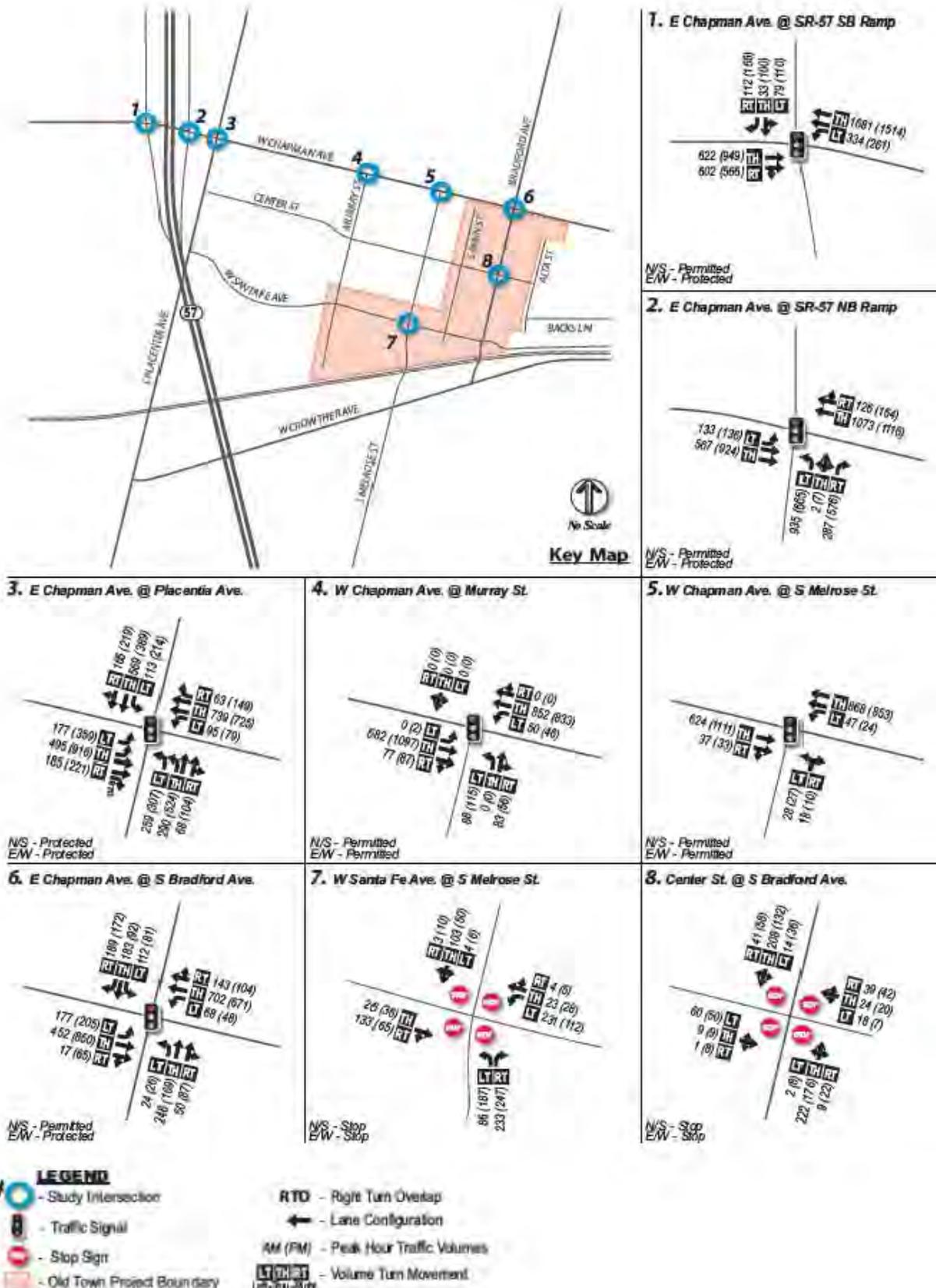


EXHIBIT 3-13
EXISTING 2017 AM/PM PEAK HOUR VOLUMES
 Source: DKS Associates

The LOS summary is shown in Table 3-9. As shown, all intersections operate at LOS C or better. LOS calculation sheets are provided in Appendix B of the DKS Traffic Study.

**Table 3-9
 Existing Levels of Service**

Intersection	AM Peak Hour		PM Peak Hour	
	V/C or Delay (sec.)	LOS	V/C or Delay (sec.)	LOS
Caltrans – Signalized				
Chapman Avenue/SR-57 Southbound Ramps	12.7	B	15.4	B
Chapman Avenue/SR-57 Northbound Ramps	26.4	C	26.2	C
City of Placentia – Signalized				
Chapman Avenue/Placentia Avenue	0.611	B	0.679	B
Chapman Avenue/Murray Street	0.352	A	0.493	A
Chapman Avenue/Melrose Street	0.332	A	0.416	A
Chapman Avenue/Bradford Avenue	0.556	A	0.521	A
City of Placentia – All-Way Stop Controlled				
Santa Fe Street/Melrose Street	11.1	B	9.9	A
Center Street/Bradford Avenue	8.6	A	8.7	A

Analysis Software: Traffix, Version 8.0. Per the Intersection Capacity Utilization methodology, overall volume to capacity ratios and levels of service are shown for intersections controlled by traffic signals. Per the Highway Capacity Manual (HCM 2000) methodology, overall average intersection delay and level of service are shown for intersections controlled by all-way stop and for intersections under the jurisdiction of Caltrans.

The Revitalization Project would add 525 residential units, 85,000 square feet of commercial use, 40,000 square feet of retail use, and a 50-room hotel. The proposed residential use will be a combination of apartments and townhomes. As a conservative approach, this analysis will assume 100% apartment use due to the greater trip generation rate compared to townhomes. Currently, the Old Town area contains a mix of residential uses and various commercial uses. The majority of properties in the area are developed. Therefore, the majority of the proposed land uses are expected to be infill development. While it can be expected that some existing land uses may be replaced, DKS is assuming the Old Town Revitalization Project consists of the proposed development plus all the existing land uses.

Based on discussions with City staff, the City has approved the use of three trip credits, transit, pass-by, and internal capture. Due to the project’s vicinity to transit facilities, including the proposed Placentia Metrolink Station, and the project’s goal to develop a walkable urban environment, DKS applied a transit trip credit. The Orange County Congestion Management Program (CMP) highlights the benefits of utilizing pass-by and internal capture credits for mixed-used developments, such as the Old Town Revitalization Project. Pass-by traffic is retail traffic that is already on the road and driving by the site. The driver will stop at the retail shop on their way to their final destination. Internal capture is the portion of trips generated by a development that both begin and end within the site. Based on techniques outlined in the Institute of Transportation Engineers’ (ITE) Trip Generation Manual, 9th Edition, pass-by and internal

capture trip credits were estimated. As shown in Table 3-10, a 34% pass-by credit can be applied to commercial retail use and a 12% internal capture trip credit can be applied to the overall development.

Trip generation estimates for the proposed project were developed using trip rates established in the ITE Trip Generation Manual, 9th Edition. As shown in Table 3-10, the combination of the proposed development and the applied trip credits results in approximately 7,361 trip-ends per day, with 306 (115 inbound, 191 outbound) trips during the AM peak hour and 510 (281 inbound, 229 outbound) trips during the PM peak hour.

**Table 3-10
 Project Trip Generation Summary**

Land Use	ITE Code	Size	Daily	AM Peak Hour			PM Peak Hour			
				In	Out	Total	In	Out	Total	
Trip Rates										
Apartment	220	per	DU	6.65	0.10	0.41	0.51	0.40	0.22	0.62
Hotel	310	per	RM	8.17	0.31	0.22	0.53	0.31	0.29	0.60
Shopping Center (Retail/Commercial)	820	per	TSF	42.70	0.60	0.36	0.96	1.78	1.93	3.71
Trip Generation										
<i>Trips</i>										
Apartment	525	DU	3,491	53	215	268	210	116	326	
Hotel	50	RM	409	16	11	27	15	15	30	
Shopping Center (Retail/Commercial)	125.00	TSF	5,338	75	45	120	223	241	464	
Gross Total Trips				9,238	144	271	415	448	372	820
Land Use	ITE Code	Size	Daily	AM Peak Hour		PM Peak Hour				
				In	Out	In	Out			
<i>Transit Reduction for Apartment (Less 25% Transit Trip)</i>				(873)	(13)	(54)	(67)	(53)	(29)	(82)
<i>Pass-by Reduction for Retail/Commercial (Less 34% Pass-by Trip)</i>				--	--	--	--	(76)	(82)	(158)
<i>Internal Capture Reduction for Development (12% Internal Trip)</i>				(1,004)	(16)	(26)	(42)	(38)	(32)	(70)
Net Project Trip Generation				7,361	115	191	306	281	229	510

Project trip distribution patterns were based on factors such as: 1) transportation facility characteristics that impact travel demand (i.e. location of urban arterials, freeways, and interchanges); 2) location of employment and commercial facilities; and 3) existing traffic patterns.

Exhibits 3-14 and 3-15 illustrate trip distribution percentages for the proposed project. Trip distribution percentages were applied to the proposed project's trip generation to calculate the traffic volumes which the project would generate at study intersections (i.e. trip assignment). The resulting AM and PM peak hour trip assignments used for the LOS analysis are shown in Exhibit 3-16.

The trips generated from the project, as shown in Exhibits 3-16, were added to the existing traffic volumes which resulted in the existing plus project traffic scenario. Exhibit 3-17 illustrates the Existing Year (2017) Plus Project traffic volumes. The existing level of service has been evaluated at the study intersections based on the ICU methodology for signalized intersections and the 2000 HCM methodology for all-way stop controlled and Caltrans intersections.

In conclusion, and as shown, all intersections operate at LOS C or better. LOS calculation sheets are provided in Appendix C of the DKS Traffic Study. Based on the threshold for significant impacts of the proposed project, the trips generated from the proposed project would not cause significant impact on any of the study intersections under Existing (2017) Plus Project traffic conditions for both the AM and PM peak periods. Therefore, no mitigation measures are required on study intersections as part of the project.

Table 3-11
Existing (2017) Plus Project Intersection Level of Service Summary

Intersection	Existing				Existing Plus Project				Difference		Project Impact
	AM Peak		PM Peak		AM Peak		PM Peak		AM Peak	PM Peak	
	V/C or Delay (sec)	LOS	V/C or Delay (sec)	LOS	V/C or Delay (sec)	LOS	V/C or Delay (sec)	LOS	V/C or Delay (sec)	LOS	
Caltrans – Signalized											
Chapman Ave/ SR-57 SB Ramp	12.7	B	15.4	B	13.5	B	17.7	B	2.7	2.3	NO
Chapman Ave/ SR-57 NB Ramp	26.4	C	26.2	C	27.3	C	27.3	C	0.9	1.1	NO
City of Placentia – Signalized											
Chapman Ave/ Placentia Ave	0.611	B	0.679	B	0.637	B	0.721	C	0.026	0.042	NO
Chapman Ave/ Murray St	0.352	A	0.493	A	0.393	A	0.558	A	0.558	0.041	NO
Chapman Ave/ Melrose St	0.332	A	0.416	A	0.367	A	0.494	A	0.494	0.035	NO
Chapman Ave/ Bradford Ave	0.556	A	0.521	A	0.566	A	0.541	A	0.541	0.010	NO
City of Placentia – All-Way Stop Controlled											
Santa Fe Ave/ Melrose St	11.1	B	9.9	A	12.2	B	11.1	B	1.1	1.2	NO
Center St/ Bradford Ave	8.6	A	8.7	A	9.1	A	9.9	A	0.5	1.2	NO

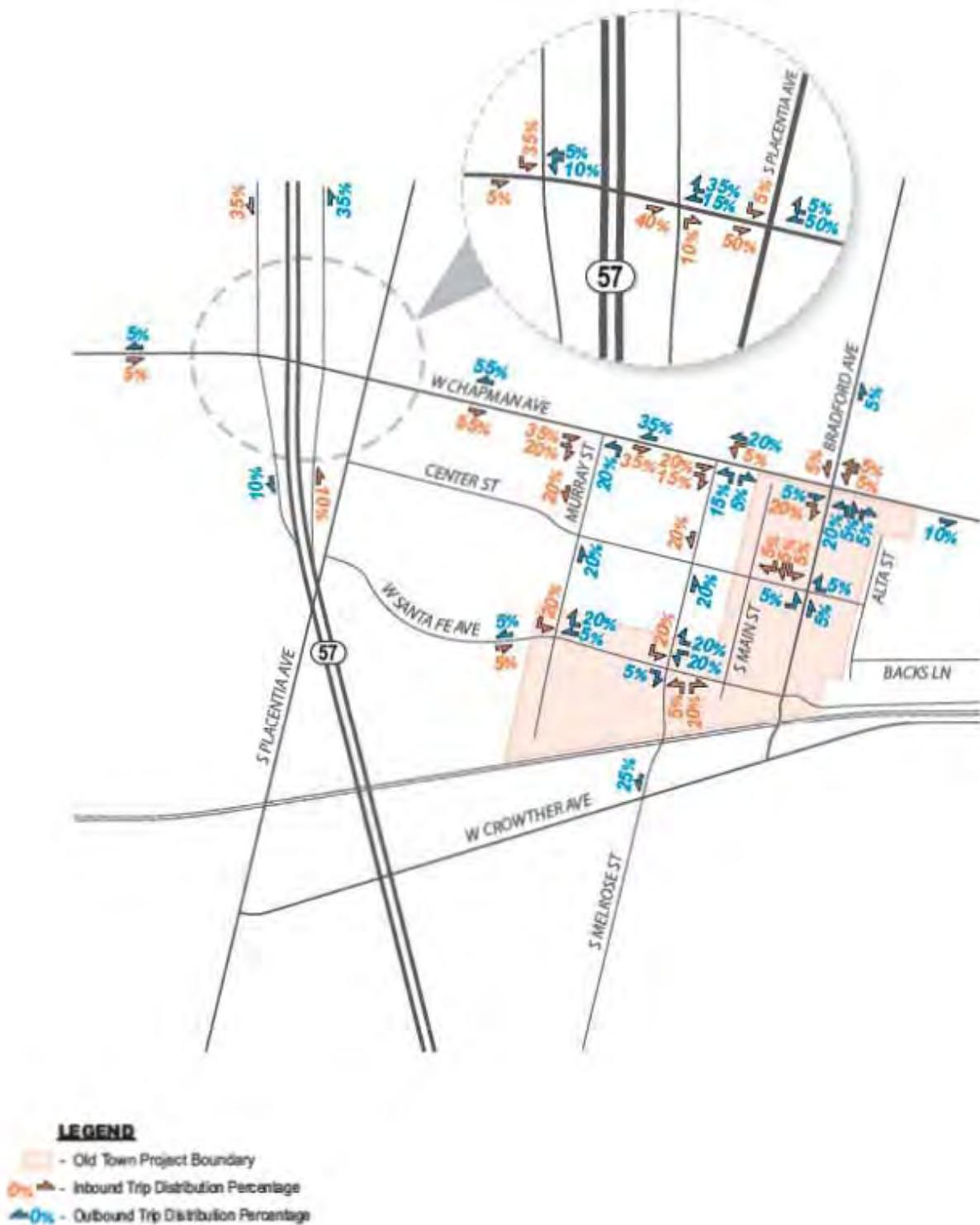


EXHIBIT 3-14
PROJECT TRIP DISTRIBUTION-RESIDENTIAL & HOTEL
 Source: DKS Associates

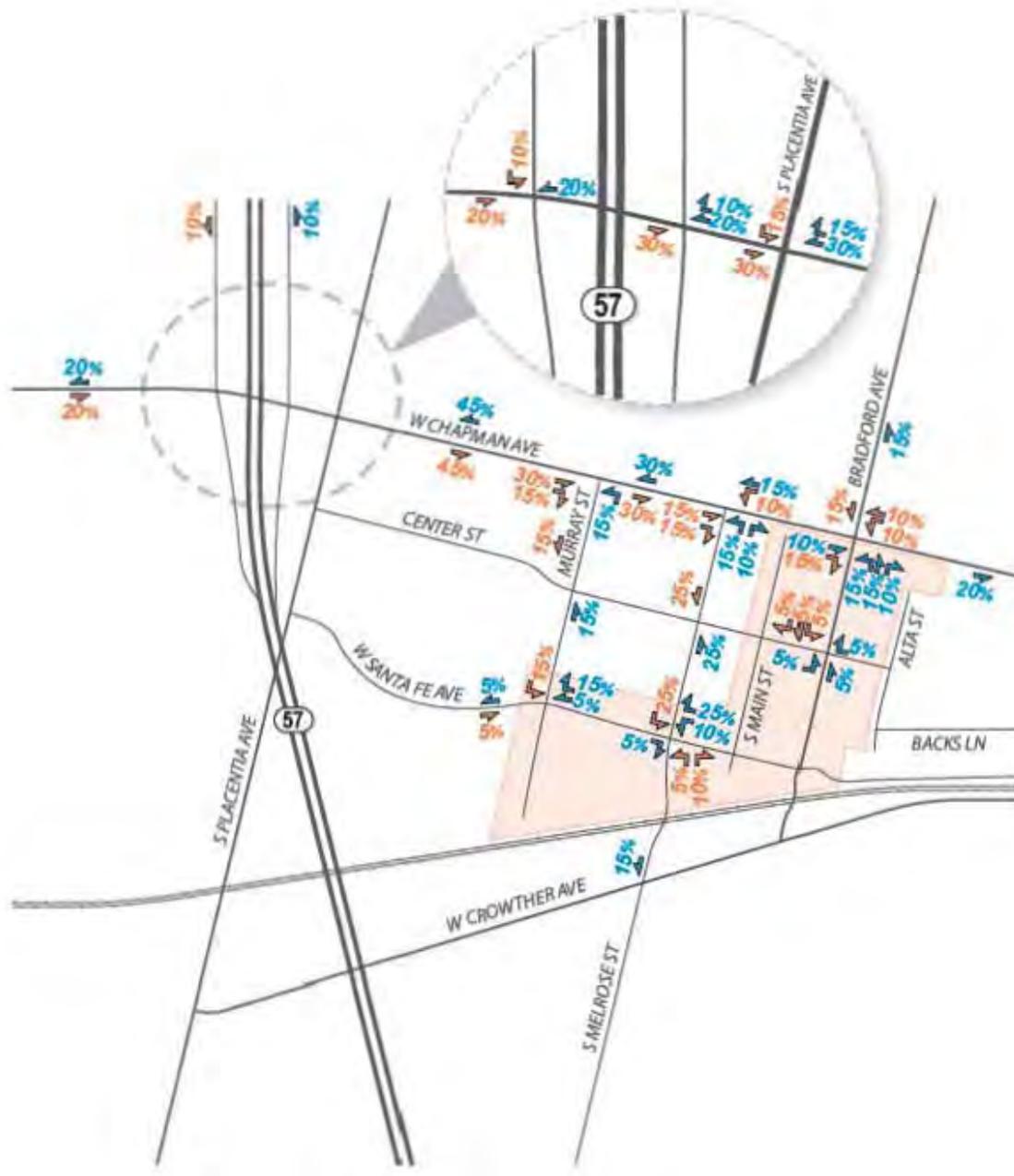


EXHIBIT 3-15
PROJECT TRIP DISTRIBUTION-COMMERCIAL & RETAIL
 Source: DKS Associates

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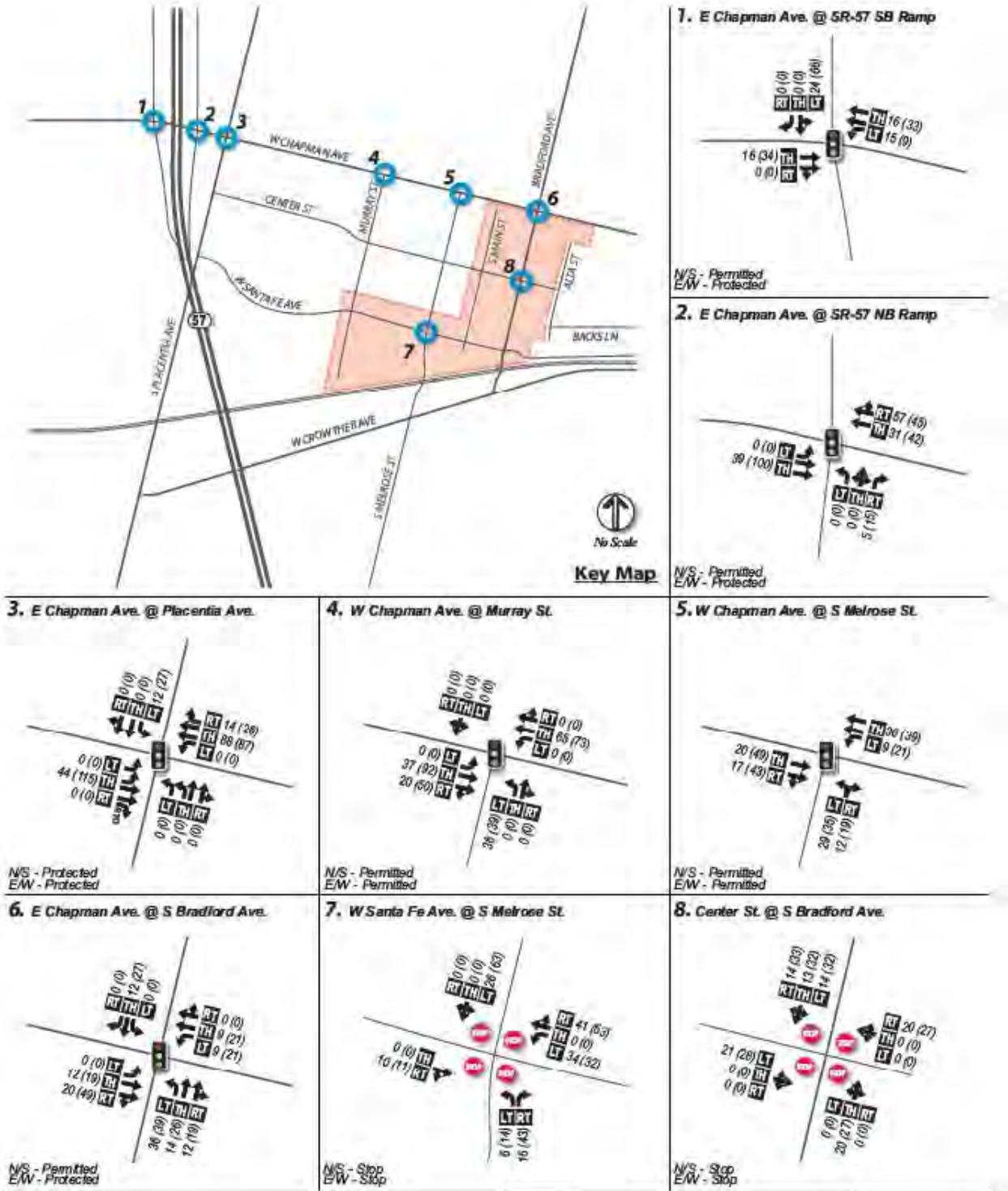


EXHIBIT 3-16
PROJECT TRIP ASSIGNMENT
 Source: DKS Associates

Future year buildout traffic forecasts were developed in order to analyze the project traffic impacts during the buildout year of the project (2037). Based on discussions with the City staff, a 1% annual growth was added to the existing vehicular traffic volumes for a period of 20 years to determine the future 2037 traffic volumes at the study intersections. In addition, the City of Placentia provided a list and locations of cumulative projects to be used for the future 2037 analysis. Figure 8 in the Traffic Report included in Appendix B illustrates the general location of these cumulative projects. The list of cumulative projects is presented in Table 3-12.

Table 3-12
List of Cumulative Projects

Cumulative Project	Description	Location
Placentia Metrolink Station	New Train Station	NEC of Melrose St/Crowther Ave
Transit Oriented Development (TOD)	752 Single-Family Residential Units	Melrose St/Crowther Ave Vicinity
General Plan Zoning Change	318,000 square feet of 'Industrial' to 'Commercial'	NEC of Placentia Ave/Crowther Ave

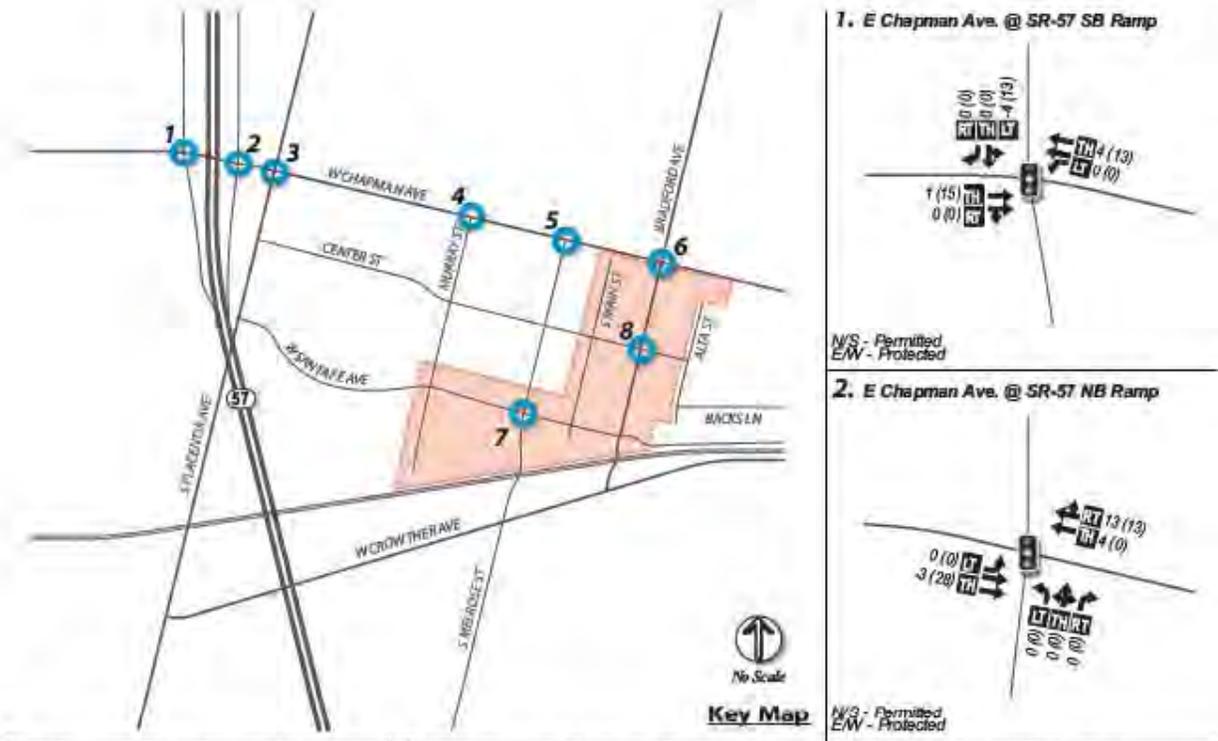
As shown in Table 3-13, the cumulative developments are projected to generate approximately 8,263 trip-ends per day, with 425 (113 inbound, 312 outbound) trips during the AM peak hour and 708 (389 inbound, 319 outbound) trips during the PM peak hour. Trip distribution details for the cumulative projects are included in Appendix E of the DKS Traffic Study.

Table 3-13
Cumulative Projects Trip Generation Summary

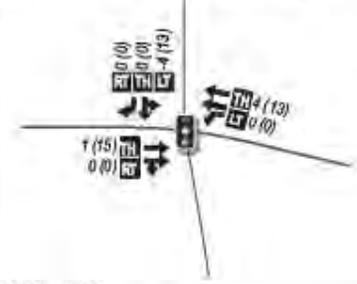
Cumulative Project	ITE Code	Size	Units	Daily	AM Peak Hour			PM Peak Hour		
					In	Out	Total	In	Out	Total
Placentia Metrolink Station	--	--	--	1,180	211	31	242	31	211	242
Transit Oriented Development (TOD)	220	752	DU	3,753	-65	259	194	261	60	321
Existing Zoning Designation – Industrial	110	108.580	TSF	-757	-88	-12	-100	-13	-92	-105
Proposed Zoning Designation – Commercial	826	92.220	TSF	4,087	55	34	89	110	140	250
Total Trips				8,263	113	312	425	389	319	708

Exhibit 3-17 shows the cumulative project volumes at study intersections. Exhibit 3-18 illustrates the Project Buildout Year (2037) Without Project AM and PM peak hour traffic volumes in the study area. Project Buildout Year (2037) traffic conditions include the existing peak hour intersection volumes plus the ambient growth rate (20 years at 1% growth per year) and cumulative traffic volumes.

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1. E Chapman Ave. @ SR-57 SB Ramp



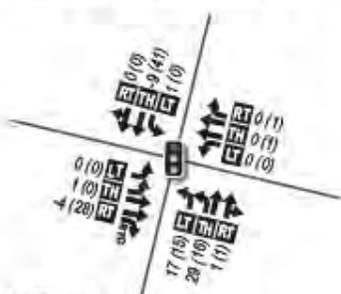
N/S - Permitted
 EW - Protected

2. E Chapman Ave. @ SR-57 NB Ramp



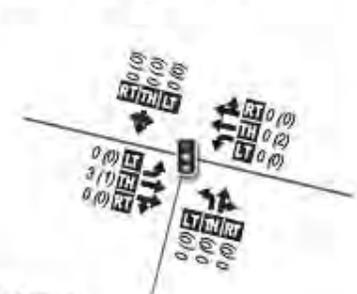
N/S - Permitted
 EW - Protected

3. E Chapman Ave. @ Placentia Ave.



N/S - Protected
 EW - Protected

4. W Chapman Ave. @ Murray St.



N/S - Permitted
 EW - Permitted

5. W Chapman Ave. @ S Melrose St.



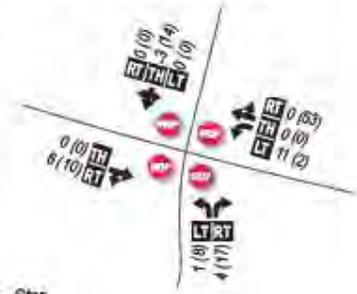
N/S - Permitted
 EW - Permitted

6. E Chapman Ave. @ S Bradford Ave.



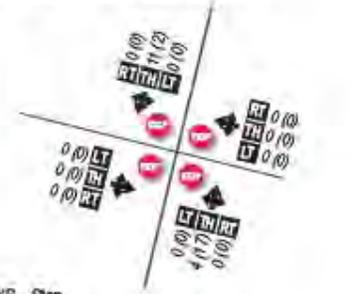
N/S - Permitted
 EW - Permitted

7. W Santa Fe Ave. @ S Melrose St.



N/S - Stop
 EW - Stop

8. Center St. @ S Bradford Ave.



N/S - Stop
 EW - Stop

LEGEND

- # - Study Intersection
- 🚦 - Traffic Signal
- 🛑 - Stop Sign
- 🏠 - Old Town Project Boundary
- RTO - Right Turn Overlap
- ← - Lane Configuration
- AM (PM) - Peak Hour Traffic Volumes
- 📊 - Volume Turn Movement

EXHIBIT 3-17
CUMULATIVE PROJECT PEAK HOUR VOLUMES
 Source: DKS Associates

The Project Buildout Year (2037) Without Project level of service has been evaluated at the study intersections based on the ICU methodology for signalized intersections and the 2000 HCM methodology for all-way stop controlled and Caltrans intersections. The LOS summary for intersections is shown in Table 3-14. As shown, all intersections operate at LOS D or better. LOS calculation sheets are provided in Appendix E of the DKS Traffic Study.

**Table 3-14
 Project Buildout Year (2037) Without Project
 Intersection Level of Service Summary**

Intersection	AM Peak Hour		PM Peak Hour	
	V/C or Delay (sec.)	LOS	V/C or Delay (sec.)	LOS
Caltrans – Signalized				
Chapman Avenue/SR-57 Southbound Ramps	15.1	B	18.3	B
Chapman Avenue/SR-57 Northbound Ramps	39.1	D	40.8	D
City of Placentia – Signalized				
Chapman Avenue/Placentia Avenue	0.726	C	0.811	D
Chapman Avenue/Murray Street	0.413	A	0.582	A
Chapman Avenue/Melrose Street	0.390	A	0.510	A
Chapman Avenue/Bradford Avenue	0.656	B	0.623	B
City of Placentia – All-Way Stop Controlled				
Santa Fe Street/Melrose Street	13.2	B	11.2	B
Center Street/Bradford Street	10.4	B	9.5	A

The projected trips from the Revitalization Project, as shown in Exhibit 3-19, were added to the Project Buildout Year (2037) Without Project traffic volumes shown in Exhibit 3-18, which results in the Project Buildout Year (2037) With Project traffic scenario. Exhibit 3-19 illustrates the Project Buildout Year (2037) With Project traffic volumes.

The Project Buildout Year (2037) With Project level of service has been evaluated at the study intersections based on the ICU methodology for signalized intersections and the 2000 HCM methodology for all-way stop controlled and Caltrans intersections. The LOS summary for intersections is shown in Table 3-15. As shown, all intersections operate at LOS D or better. LOS calculation sheets are provided in Appendix F of the DKS Traffic Study.

Based on the threshold for significant impacts of the proposed project, the trips generated from the proposed project would not cause significant impact on any of the study intersections under Project Buildout Year (2037) Plus Project traffic conditions for both the AM and PM peak periods. Therefore, no mitigation measures are required on study intersections as part of the project.

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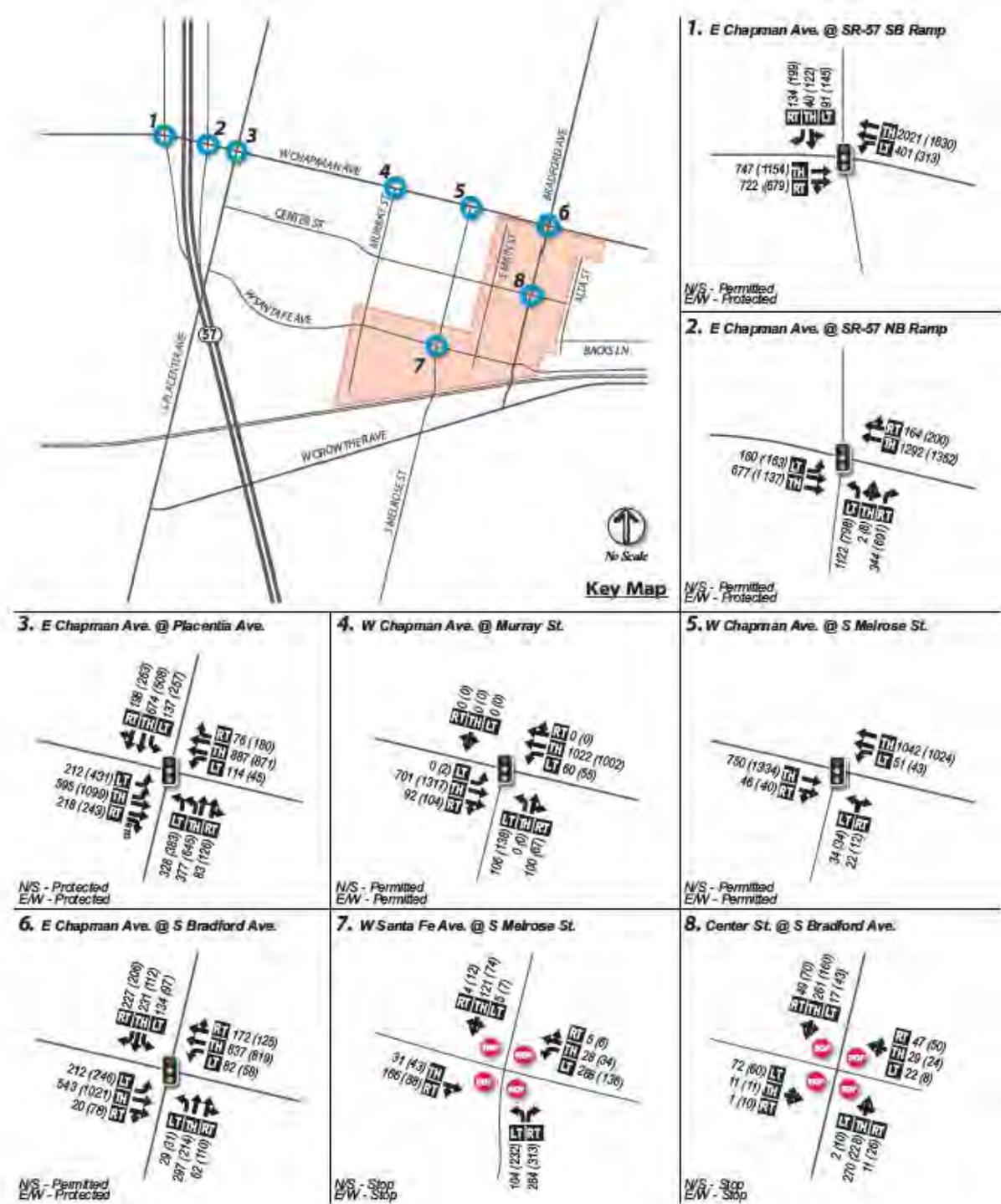
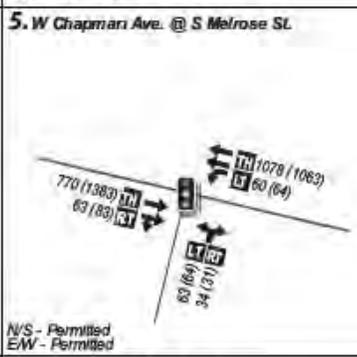
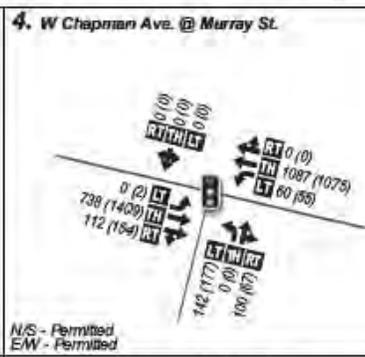


EXHIBIT 3-18
PROJECT BUILDOUT YEAR (2037) WITHOUT PROJECT TRAFFIC
 Source: DKS Associates

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- LEGEND**
- # - Study Intersection
 - 🚦 - Traffic Signal
 - 🛑 - Stop Sign
 - 📏 - Old Town Project Boundary
 - RTO - Right Turn Overlap
 - ← - Lane Configuration
 - AM (PM) - Peak Hour Traffic Volumes
 - 📊 - Volume Turn Movement

EXHIBIT 3-19
PROJECT BUILDOUT YEAR (2037) WITH PROJECT TRAFFIC
 Source: DKS Associates

Table 3-15
Level of Service Impacts from Buildout Year (2037) Plus Project

Intersection	Year 2037 without Project				Year 2037 with Project				Difference		Project Impact
	AM Peak		PM Peak		AM Peak		PM Peak		AM Peak	PM Peak	
	V/C or Delay (sec.)	LOS	V/C or Delay (sec.)	LOS	V/C or Delay (sec.)	LOS	V/C or Delay (sec.)	LOS	V/C or Delay (sec.)	LOS	
Caltrans – Signalized											
Chapman Avenue/ SR-57 Southbound Ramps	15.1	B	18.3	B	16.1	B	21.0	C	1.0	2.7	NO
Chapman Avenue/ SR-57 Northbound Ramps	39.1	D	40.8	D	43.0	D	45.3	D	3.9	4.5	NO
City of Placentia – Signalized											
Chapman Avenue/ Placentia Avenue	0.726	C	0.811	D	0.752	C	0.856	D	0.026	0.045	NO
Chapman Avenue/ Murray Street	0.413	A	0.582	A	0.453	A	0.647	B	0.040	0.065	NO
Chapman Avenue/ Melrose Street	0.390	A	0.510	A	0.425	A	0.581	A	0.035	0.071	NO
Chapman Avenue/ Bradford Avenue	0.656	B	0.623	B	0.666	B	0.642	B	0.010	0.019	NO
City of Placentia – All-Way Stop Controlled											
Santa Fe Street/ Melrose Street	13.2	B	11.2	B	15.0	B	12.9	B	1.8	1.7	NO
Center Street/ Bradford Street	10.4	B	9.5	A	11.4	B	11.0	B	1.0	1.5	NO

Based on the results of the analysis, the proposed project generated trips would not cause significant impact at any of the study intersections for both Existing (2017) Plus Project and Project Buildout (Year 2037) With Project traffic conditions during the AM and PM peak periods. Therefore, no mitigation measures are required at any study intersection as a part of the proposed project.

B. Would the project result in a conflict with an applicable congestions management program, including but not limited to, level of service standards and travel demand measures, or other standards established by the County congestion management agency for designated roads or highways? No Impact.

In June 1990, the passage of the Proposition 111 gas tax increase required California’s urbanized areas – areas with populations of 50,000 or more– to adopt a Congestion Management Program (CMP). The following year, Orange County’s local governments designated the Orange County Transportation Authority (OCTA) as the Congestion Management Agency (CMA) for the County. As a result, OCTA is responsible for the development, monitoring, and biennial updating of Orange County’s CMP.

The CMP for Orange County requires that the traffic impact of individual development projects of potentially regional significance be analyzed. Developments that will be within the Planning Area will not be projects that are classified as “Regionally Significant Projects” and therefore will not potentially affect any local CMP intersection. In order to be classified as a “Regionally Significant Project” by CEQA, a

project must be a residential development of more than 500 units; a commercial center or business employing more than 1,000 persons or having more than 500,000 square feet of floor area; an office building employing more than 1,000 persons or encompassing more than 250,000 square feet of floor area; a hotel consisting of more than 500 rooms; or an industrial use with more than 650,000 square feet of floor area or occupying more than 40 acres of land, or employing more than 1,000 persons.⁸⁹ Therefore, the adoption and subsequent implementation of the Old Town Placentia Revitalization Project will not lead to any significant impacts. As a result, no impacts will occur.

C. Would the project results in a change in air traffic patterns, including either an increase in traffic levels or a change in the location that results in substantial safety risks? No Impact.

The proposed Old Town Placentia Revitalization Project will not result in traffic air traffic patterns. As a result, no significant adverse impacts will result.

D. Would the project substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)? No Impact.

The adoption and subsequent implementation of the Old Town Placentia Revitalization Project will not lead to any significant impacts. The Revitalization Plan document lists various design options considered for one-way circulation patterns within the Planning Area. The preferred circulation design (Option #3) for the Old Town Planning Area proposes one-way traffic southbound on Bradford Avenue from Center Street to Santa Fe Avenue, and westbound on Santa Fe Avenue from Bradford Avenue to Main Street. This proposed circulation pattern will not result in a hazardous design feature for the Planning Area. Furthermore, various development standards will result in traffic calming. Traffic calming involves the incorporation of physical design and other measures to deliberately reduce traffic speeds. Physical design elements may include speed bumps, the narrowing of roads, and the installation of raised pedestrian crossings. Traffic calming measures will result in safer road conditions for both drivers and pedestrians. As a result, no additional mitigation beyond that which may be required for individual development projects is required.

E. Would the project result in inadequate emergency access? No Impact.

The future development supported by the Old Town Placentia Revitalization Project would comply with applicable regulations established by the Orange County Fire Department and the Placentia Division of Building and Safety, in addition to the standard design requirements of the Uniform Building Code. The Fire Department will review any development plan including all buildings, fences, drive gates, or other features that might affect Fire Department access. This review process, along with the proponent's compliance with the applicable regulations and standards, would ensure that adequate emergency access would be provided. Therefore, no impacts will occur.

⁸⁹ California, State of. *Title 14. California Code of Regulations. Chapter 3. Guidelines for the Implementation of the California Environmental Quality Act.* As Amended 1998 (CEQA Guidelines). § 15206 (2) (A-E).

F. Would the project result in a conflict with adopted policies, plans, or programs regarding public transit, bicycle, or pedestrian facilities, or otherwise decrease the performance or safety of such facilities? No Impact.

The adoption and subsequent implementation of the Old Town Placentia Revitalization Project will not lead to any significant impacts. The goals, policies, and implementation programs contained within the Revitalization Project will mitigate the potential impacts from new development contemplated as part of the implementation of the Revitalization Project. Thus, no impacts will occur.

3.16.3 MITIGATION MEASURES

Based on the results of the analysis, the proposed project generated trips would not cause significant impact at any of the study intersections for both Existing (2017) Plus Project and Project Buildout (Year 2037) With Project traffic conditions during the AM and PM peak periods. In addition, the goals, policies, and implementation programs contained within the Revitalization Plan document will also further reduce the potential impacts from new development contemplated as part of the implementation of the Revitalization Project. Therefore, no mitigation measures are required at any study intersection as a part of the proposed project.

3.17 TRIBAL CULTURAL RESOURCES

3.17.1 THRESHOLDS OF SIGNIFICANCE

According to the City of Placentia, acting as Lead Agency, a project may be deemed to have a significant adverse impact on tribal cultural resources if it results in any of the following:

- A substantial adverse change in the significance of a tribal cultural resources, defined in Public Resources Code section 21074 as either a site, feature, place, cultural landscape that is geographically defined in terms of the size and scope of the landscape, sacred place, or object with cultural value to a California Native American Tribe, and that is listed or eligible for listing in the California Register of Historical Resources, or in a local register of historical resources as defined in Public Resources Code section 5020.1(k); or,
- A substantial adverse change in the significance of a tribal cultural resources, defined in Public Resources Code section 21074 as either a site, feature, place, cultural landscape that is geographically defined in terms of the size and scope of the landscape, sacred place, or object with cultural value to a California Native American Tribe, and that is a resource determined by the Lead Agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Public Resources Code Section 5024.1.

3.17.2 ANALYSIS OF ENVIRONMENTAL IMPACTS

- A. *Would the project cause a substantial adverse change in the significance of a tribal cultural resource, defined in Public Resources Code section 21074 as either a site, feature, place, cultural landscape that is geographically defined in terms of the size and scope of the landscape, sacred place, or object with cultural value to a California Native American tribe, and that is listed or eligible for listing in the California Register of Historical Resources, or in a local register of historical resources as defined in Public Resources Code §5020.1(k)?* • *Less than Significant Impact.*

A Tribal Resource is defined in Public Resources Code section 21074 and includes the following:

- Sites, features, places, cultural landscapes, sacred places, and objects with cultural value to a California Native American tribe that are either of the following: included or determined to be eligible for inclusion in the California Register of Historical Resources or included in a local register of historical resources as defined in subdivision (k) of Section 5020.1.
- A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Section 5024.1. In applying the criteria set forth in subdivision (c) of Section 5024.1 for the purposes of this paragraph, the lead agency shall consider the significance of the resource to a California Native American tribe.
- A cultural landscape that meets the criteria of subdivision (a) is a tribal cultural resource to the extent that the landscape is geographically defined in terms of the size and scope of the landscape.
- A historical resource described in Section 21084.1, a unique archaeological resource as defined in subdivision (g) of Section 21083.2, or a “non-unique archaeological resource” as defined in subdivision (h) of Section 21083.2 may also be a tribal cultural resource if it conforms with the criteria of subdivision (a).

Formal Native American consultation was undertaken in accordance with AB-52 and no responses were received. In the unlikely event that remains are uncovered by construction crews, all excavation and grading activities shall be halted and the Orange County Sheriff will be contacted (the Department will then contact the County Coroner). This is a standard condition under California Health and Safety Code Section 7050.5(b), which states:

“In the event of discovery or recognition of any human remains in any location other than a dedicated cemetery, there shall be no further excavation or disturbance of the site or any nearby area reasonably suspected to overlie adjacent remains until the coroner of the county in which the human remains are discovered has determined, in accordance with Chapter 10 (commencing with (b) Section 27460) of Part 3 of Division 2 of Title 3 of the Government Code, that the remains are not subject to the provisions of Section 27491 of the Government Code or any other related provisions of law concerning investigation of the circumstances, manner and cause of any death, and the recommendations concerning the treatment and disposition of the human remains have been made to the person responsible for the excavation, or to his or her authorized representative, in the manner provided in Section 5097.98 of the Public Resources Code. The coroner shall make his or her

determination within two working days from the time the person responsible for the excavation, or his or her authorized representative, notifies the coroner of the discovery or recognition of the human remains. If the coroner determines that the remains are not subject to his or her authority and if the coroner recognizes the human remains to be those of a Native American, or has reason to believe that they are those of a Native American, he or she shall contact, by telephone within 24 hours, the Native American Heritage Commission.”

In addition, Title 14; Chapter 3; Article 5; Section 15064.5 of CEQA will apply in terms of the identification of significant archaeological resources and their salvage. Therefore, the potential impacts are considered to be less than significant.

B. Would the project cause a substantial adverse change in the significance of a tribal cultural resource, defined in Public Resources Code section 21074 as either a site, feature, place, cultural landscape that is geographically defined in terms of the size and scope of the landscape, sacred place, or object with cultural value to a California Native American tribe, and that is a resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Public Resources Code Section 5024.1? In applying the criteria set forth in subdivision (c) of Public Resource Code Section 5024.1, the lead agency shall consider the significance of the resource to a California Native American tribe. • Less than Significant Impact.

Formal Native American consultation was undertaken in accordance with AB-52. In the unlikely event that remains are uncovered by construction crews, all excavation and grading activities shall be halted and the Orange County Sheriff will be contacted (the Department will then contact the County Coroner). In addition, Title 14; Chapter 3; Article 5; Section 15064.5 of CEQA will apply in terms of the identification of significant archaeological resources and their salvage. Therefore, the potential impacts are considered to be less than significant.

3.17.3 MITIGATION MEASURES

The adoption and subsequent implementation of the Revitalization Project will not lead to any significant impacts. The goals, policies, and implementation programs contained within the Revitalization Project will also further mitigate the potential impacts from new development. As a result, no additional mitigation beyond that which may be required for individual development projects is required.

3.18 UTILITIES

3.18.1 THRESHOLDS OF SIGNIFICANCE

According to the City of Placentia, acting as Lead Agency, a project may be deemed to have a significant adverse impact on utilities if it results in any of the following:

- An exceedance of the wastewater treatment requirements of the applicable Regional Water Quality Control Board;

- The construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental impacts;
- The construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects;
- An overcapacity of the storm drain system causing area flooding;
- A determination by the wastewater treatment provider that serves or may serve the project that it has inadequate capacity to serve the project's projected demand;
- The project will be served by a landfill with insufficient permitted capacity to accommodate the project's solid waste disposal needs;
- Non-compliance with federal, state, and local statutes and regulations relative to solid waste;

3.18.2 ANALYSIS OF ENVIRONMENTAL IMPACTS

A. Would the project exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board? Less than Significant Impact.

The Orange County Sanitation District treats wastewater from the City of Placentia.⁹⁰ Local sewer lines are maintained by the City of Placentia. This system contains sewage collection piping ranging in size from eight inches to fifteen-inches. The Orange County Sanitation District (OCSD) owns, operates, and maintains the large trunk sewers of the regional wastewater conveyance system. The OCSD collects, treats, and disposes of and/or reclaims the wastewater generated by 2.5 million people living and working in central and northwestern Orange County. OCSD's service area encompasses approximately 479 square miles and its system includes approximately 580 miles of sewer lines and two treatment plants located in the cities of Fountain Valley and Huntington Beach. Through these facilities, OCSD collects, conveys, treats, and/or reclaims approximately 230 million gallons of wastewater generated daily in its service area. Approximately 80% of the wastewater comes from homes (i.e., sinks, toilets, showers, laundry, and dishwashers).

The OCSD operates two treatment facilities: Reclamation Plant No. 1, located in the City of Fountain Valley, and Reclamation Plant No. 2, located in the City of Huntington Beach. The City of Placentia is served by Reclamation Plant No. 2. The plant receives raw wastewater through five major sewers. Approximately 33 percent of the effluent receives secondary treatment through an activated sludge system, and all of the effluent is discharged into the ocean disposal system. The current capacity for Reclamation Plant No. 2 is 168 million gallons per day (mgd) of primary treated wastewater and 90 mgd of secondary treated wastewater. The current average flow is 151 mgd; thus, remaining capacity at this plant is approximately 24 mgd. Expansion plans by OCSD are ongoing and designed to address the incremental increase in sewage generation as a result of a new development. The secondary treatment capacity at this plant is currently being increased by 60 mgd for a future total capacity of 150 mgd. The capacity of

⁹⁰ Orange County Sanitation Districts. www.lacsd.org/about/serviceareamap.asp

Reclamation Plant No. 2 will also be analyzed on a per project basis as development under the project occurs. As a result, the potential impacts are considered to be less than significant.

B. Would the project require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental impacts? Less than Significant Impact.

As stated in the previous subsection, the City of Placentia is located within the service boundaries of OCS D Reclamation Plant No. 2, which is located in the City of Huntington Beach. As individual projects within the Revitalization Project are proposed, an analysis of local sewer line capacity and treatment plant capacity will be undertaken on a per project basis. Thus, the proposed impacts are anticipated to be less than significant.

Golden State Water Company has been serving the City of Placentia since 1929. Golden State Water Company provides water for 15,500 customers in Placentia and portions of Anaheim, Cowan Heights, Lemon Heights, Orange, and Yorba Linda. Three water systems serve the Placentia Customer Service Area. Water delivered to customers in the Cowan Heights, Placentia, and Yorba Linda systems is a blend of groundwater pumped from the Orange County Groundwater Basin and imported water from the Colorado River Aqueduct and State Water Project (imported and distributed by Metropolitan Water District of Southern California).

According to the 2010 Urban Water Management Plan prepared for Golden State Water Company and the City of Placentia, demand for water is projected to be 5,691 acre-feet per year by 2020 and 5,848 acre-feet per year by 2025. Supplies are anticipated to total 9,178 acre-feet per year by 2020 and 9,432 acre-feet per year in 2025. Demand through the year 2025 is anticipated to be up to 62 percent of projected supplies. The future development permitted under the Revitalization Project will contribute to an incremental increase in water consumption. Each individual project's water consumption will be analyzed on a per project basis and mitigation may be recommended if demand exceeds the capacity of the local water lines. The installation of water efficient appliances and fixtures will reduce demand for water. In addition, the planting of Xeriscape landscaping will further reduce future water consumption. As a result, the impacts are considered to be less than significant.

C. Would the project require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects? Less than Significant Impact.

The City of Placentia is served by the Orange County Flood Control District (LACFCD), which operates and maintains regional and municipal storm drainage facilities. The City works with the (LACFCD) in making local drainage plans and improvements. According to the City, any project submitted to the City of Placentia for review and approval may be subject to the requirements of the Municipal NPDES permit. Water Quality Management Plan (WQMP) compliance is a requirement within the Permit. Development and Redevelopment projects submitted for review and approval will be screened during this process to determine if a WQMP is required. The WQMP will identify both construction and operational Low Impact Development (LID) Best Management Practices (BMPs) that will reduce the volume of water discharged into the local storm drains and filter out any contaminants present in the stormwater runoff.

As individual projects within the revitalization area are proposed, the applicants will be required to implement the construction and operational Best Management Practices (BMPs) identified in the WQMPs. These construction BMPs may include, but are not limited to, the limiting of grading during rain events; planting vegetation on slopes; covering slopes susceptible to erosion; maintaining stockpiles of soil on-site; and containing runoff, spills, and equipment on-site.

Typical post-construction/operational BMPs that may also be required include biofiltration/retention basins, stormwater detention chambers, grate inlet filters, and bioswales as well as other mechanisms for reducing runoff and removing potential contaminants. The biofiltration basins facilitate proper treatment and discharge of storm water runoff by using plants to capture and biologically degrade pollutants carried by storm water runoff. Biofiltration/retention areas also reduce the volume of storm water runoff discharged into the local storm drains.⁹¹ These facilities normally consist of a grass buffer strip, sand bed, ponding area, organic layer or mulch layer, planting soil, and plants. The runoff's velocity is reduced by passing over or through a sand bed and is subsequently distributed evenly along a ponding area. Adherence to the post construction BMPs identified in the WQMPs will ensure that all potential impacts remain at levels that are less than significant.

D. Would the project have sufficient water supplies available to serve the project from existing entitlements and resources, or are new or expanded entitlements needed? Less than Significant Impact.

As indicated in subsection 3.18.2.B, Golden State Water Company presently serves the City of Placentia. Golden State Water Company provides water for 15,500 customers in Placentia and portions of Anaheim, Cowan Heights, Lemon Heights, Orange, and Yorba Linda. Three water systems serve the Placentia Customer Service Area. Water delivered to customers in the Cowan Heights, Placentia, and Yorba Linda systems is a blend of groundwater pumped from the Orange County Groundwater Basin and imported water from the Colorado River Aqueduct and State Water Project (imported and distributed by Metropolitan Water District of Southern California).

According to the 2010 Urban Water Management Plan prepared for Golden State Water Company and the City of Placentia, demand for water is projected to be 5,691 acre-feet per year by 2020 and 5,848 acre-feet per year by 2025. Supplies are anticipated to total 9,178 acre-feet per year by 2020 and 9,432 acre-feet per year in 2025. Demand through the year 2025 is anticipated to be up to 62 percent of projected supplies. The future development permitted under the Revitalization Project will contribute to an incremental increase in water consumption. Each individual project's water consumption will be analyzed on a per project basis and mitigation may be recommended if demand exceeds the capacity of the local water lines. The installation of water efficient appliances and fixtures will reduce demand for water. In addition, all new development will be required to meet the City's Water Efficiency Landscape Ordinance to reduce the burden placed on the City's water resources.⁹² As a result, the impacts are considered to be less than significant.

⁹¹ California Department of Transportation. *Biofiltration Strips*.
http://www.dot.ca.gov/hq/LandArch/16_la_design/guidance/ec_toolbox/stormwater/biofiltration_strips.htm

⁹² Placentia, City of. *Municipal Code, Chapter 23.77, Water Efficient Landscape Ordinance*.

E. Would the project result in a determination by the wastewater treatment provider that serves or may serve the project that it has inadequate capacity to serve the project's projected demand in addition to the provider's existing commitments? Less than Significant Impact.

The adoption of the Old Town Placentia Revitalization Project will not lead to a direct generation in wastewater or a direct increase in demand for water. Individual projects within the planning area will be analyzed on a per project basis. Specifically, each project's impacts to the local sewer and water lines will be analyzed and mitigation may be proposed to better accommodate the increased demand. As a result, the potential impacts will be less than significant.

F. Would the project be served by a landfill with sufficient permitted capacity to accommodate the project's solid waste disposal needs? Less than Significant Impact.

Trash collection in the City of Placentia is provided by Republic Services for disposal into the nearby landfills, primarily the Olinda Alpha landfill (the Puente Landfill is now closed). The Olinda Alpha landfill accepts up to 8,000 tons of solid waste on a daily basis and processes an average of 5,322 tons of waste per day.⁹³ In addition, the Los Angeles County Sanitation District selected the Mesquite Regional Landfill in Imperial County as the new target destination for the County's waste (as an alternative to the closed Puente Hills landfill). The Mesquite Regional Landfill in Imperial County has a 100-year capacity at 8,000 tons per day.

Additionally, the nearby Puente Hills Transfer Station/Materials Recovery Facility (MRF) is able to accept 4,440 tons per day of solid waste. Waste may also be transferred to the Downey Area Recycling and Transfer Facility, the South Gate Transfer Station, the Commerce Refuse-to-Energy Facility, and the Southeast Resource and recovery facility. The adoption of the Revitalization Project will lead to approximately 2,850 pounds per day of solid waste, which is well within the capacity of the abovementioned waste facilities. Furthermore, solid waste generation will be analyzed on a per project basis. As a result, the potential impacts are considered to be less than significant.

G. Would the project comply with federal, state, and local statutes and regulations related to solid waste? No Impact.

The future development supported by the Revitalization Project, like all other development in Placentia, will be required to adhere to City and County ordinances with respect to waste reduction and recycling. As a result, no impacts related to state and local statutes governing solid waste are anticipated.

3.18.3 MITIGATION MEASURES

The adoption and subsequent implementation of the Revitalization Project will not lead to any significant impacts. The goals, policies, and implementation programs contained within the Revitalization Project will also further mitigate the potential impacts from new development contemplated as part of the implementation of the Revitalization Project. As a result, no additional mitigation beyond that which may

⁹³ Solid Waste Association of North America (SWANA). *SWANA 2014 Landfill Management Excellence Award for Olinda Alpha Landfill.*

be required for individual development projects is required. The analysis determined that the project's potential impacts in regards to utilities were less than significant.

3.19 MANDATORY FINDINGS OF SIGNIFICANCE

The following findings can be made regarding the mandatory findings of significance set forth in Section 15065 of the CEQA Guidelines based on the results of this environmental assessment:

- The approval and subsequent implementation of the Old Town Placentia Revitalization Project *will not* have the potential to degrade the quality of the environment, with the implementation of the mitigation measures included herein.
- The approval and subsequent implementation of the Old Town Placentia Revitalization Project *will not* have the potential to achieve short-term goals to the disadvantage of long-term environmental goals, with the implementation of the mitigation measures referenced herein.
- The approval and subsequent implementation of the Old Town Placentia Revitalization Project *will not* have impacts that are individually limited, but cumulatively considerable, when considering planned or proposed development in the immediate vicinity, with the implementation of the mitigation measures contained herein.
- The approval and subsequent implementation of the Old Town Placentia Revitalization Project *will not* have environmental effects that will adversely affect humans, either directly or indirectly.

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SECTION 4 CONCLUSIONS

4.1 FINDINGS

The Initial Study determined that the proposed project is not expected to have significant adverse environmental impacts. The following findings can be made regarding the mandatory findings of significance set forth in Section 15065 of the CEQA Guidelines based on the results of this Initial Study:

- The Old Town Placentia Revitalization Project *will not* have the potential to degrade the quality of the environment, with the implementation of the mitigation measures included herein.
- The Old Town Placentia Revitalization Project *will not* have the potential to achieve short term goals to the disadvantage of long-term environmental goals, with the implementation of the mitigation measures referenced herein.
- The Old Town Placentia Revitalization Project *will not* have impacts that are individually limited, but cumulatively considerable, when considering planned or proposed development in the immediate vicinity, with the implementation of the mitigation measures contained herein.
- The Old Town Placentia Revitalization Project *will not* have environmental effects that will adversely affect humans, either directly or indirectly, with the implementation of the mitigation measures contained herein.

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SECTION 5 REFERENCES

5.1 PREPARERS

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5.2 REFERENCES

Bugliarello, et. al., *The Impact of Noise Pollution*, Chapter 127, 1975.

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APPENDICES

Appendix A – Air Quality Worksheets

Appendix B – DKS Associates Traffic Impact Analysis

Appendix C – Utilities Worksheets

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APPENDIX A
AIR QUALITY WORKSHEETS

Old Town Placentia - South Coast AQMD Air District, Summer

Old Town Placentia
South Coast AQMD Air District, Summer

1.0 Project Characteristics

1.1 Land Usage

Land Uses	Size	Metric	Lot Acreage	Floor Surface Area	Population
Hotel	50.00	Room	1.67	72,600.00	0
Apartments Mid Rise	525.00	Dwelling Unit	13.82	525,000.00	1502
Regional Shopping Center	125.00	1000sqft	2.87	125,000.00	0

1.2 Other Project Characteristics

Urbanization	Urban	Wind Speed (m/s)	2.2	Precipitation Freq (Days)	31
Climate Zone	8			Operational Year	2020
Utility Company	Southern California Edison				
CO2 Intensity (lb/MWhr)	702.44	CH4 Intensity (lb/MWhr)	0.029	N2O Intensity (lb/MWhr)	0.006

1.3 User Entered Comments & Non-Default Data

- Project Characteristics -
- Land Use - Per IS/MND
- Construction Phase - Per IS/MND
- Woodstoves - No hearths planned
- Construction Off-road Equipment Mitigation -
- Area Mitigation -
- Water Mitigation -
- Mobile Land Use Mitigation -

Old Town Placentia - South Coast AQMD Air District, Summer

Table Name	Column Name	Default Value	New Value
tblAreaMitigation	UseLowVOCPaintParkingCheck	False	True
tblConstDustMitigation	WaterUnpavedRoadVehicleSpeed	40	0
tblConstructionPhase	NumDays	20.00	260.00
tblConstructionPhase	NumDays	300.00	261.00
tblConstructionPhase	NumDays	20.00	261.00
tblConstructionPhase	NumDays	30.00	262.00
tblConstructionPhase	NumDays	20.00	260.00
tblConstructionPhase	NumDays	10.00	261.00
tblConstructionPhase	PhaseEndDate	7/12/2019	12/30/2023
tblConstructionPhase	PhaseEndDate	5/17/2019	12/31/2021
tblConstructionPhase	PhaseEndDate	1/26/2018	12/31/2018
tblConstructionPhase	PhaseEndDate	3/23/2018	12/31/2020
tblConstructionPhase	PhaseEndDate	6/14/2019	12/31/2022
tblConstructionPhase	PhaseEndDate	2/9/2018	12/31/2019
tblConstructionPhase	PhaseStartDate	6/15/2019	1/1/2023
tblConstructionPhase	PhaseStartDate	3/24/2018	1/1/2021
tblConstructionPhase	PhaseStartDate	2/10/2018	1/1/2020
tblConstructionPhase	PhaseStartDate	5/18/2019	1/1/2022
tblConstructionPhase	PhaseStartDate	1/27/2018	1/1/2019
tblGrading	AcresOfGrading	655.00	75.00
tblProjectCharacteristics	OperationalYear	2018	2020

2.0 Emissions Summary

Old Town Placentia - South Coast AQMD Air District, Summer

2.1 Overall Construction (Maximum Daily Emission)
Unmitigated Construction

Year	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio-CO2	NBio-CO2	Total CO2	CH4	N2O	CO2e
lb/day																
2018	3,7898	38,3805	23,0567	0,0407	0,1677	1,5999	2,1076	0,0445	1,8061	1,8505	0,0000	4,054,669 ₃	4,054,669 ₃	1,0730	0,0000	4,091,463 ₁
2019	4,4232	45,6341	22,8718	0,0401	18,2675	2,3919	20,6594	9,9840	2,2006	12,1846	0,0000	3,879,030 ₉	3,879,030 ₉	1,1883	0,0000	4,006,988 ₈
2020	4,5406	50,2583	32,7759	0,0643	6,5492	2,1756	8,7248	3,4023	2,0016	5,4039	0,0000	6,234,748 ₈	6,234,748 ₈	1,9490	0,0000	6,283,473 ₇
2021	4,0397	27,1470	35,4668	0,0994	5,5772	1,0126	6,5898	1,4920	0,9516	2,4436	0,0000	9,939,233 ₁	9,939,233 ₁	0,8861	0,0000	9,961,635 ₆
2022	1,1622	11,1620	15,1030	0,0244	0,1677	0,5691	0,7368	0,0445	0,5236	0,5680	0,0000	2,367,818 ₉	2,367,818 ₉	0,7180	0,0000	2,385,769 ₉
2023	20,2088	1,5044	4,7064	0,0123	1,0060	0,0778	1,0838	0,2668	0,0773	0,3441	0,0000	1,206,685 ₂	1,206,685 ₂	0,0387	0,0000	1,207,662 ₃
Maximum	20,2088	50,2583	35,4668	0,0994	18,2675	2,3919	20,6594	9,9840	2,2006	12,1846	0,0000	9,939,233₁	9,939,233₁	1,9490	0,0000	9,961,635₆

Old Town Placentia - South Coast AQMD Air District, Summer

2.1 Overall Construction (Maximum Daily Emission)
Mitigated Construction

Year	lb/day											CO ₂ e				
	ROG	NO _x	CO	SO ₂	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio-CO ₂		NBio-CO ₂	Total CO ₂	CH ₄	N ₂ O
2018	3,7988	38,3805	23,0567	0,0407	0,1677	1,9399	2,1076	0,0445	1,8061	1,8505	0,0000	4,054,666 ₃	1,0730	0,0000	4,081,483 ₀	
2018	4,4232	45,6341	22,8718	0,0401	7,2470	2,3919	9,6380	3,9263	2,2006	6,1269	0,0000	3,879,030 ₉	1,1983	0,0000	4,006,988 ₈	
2020	4,5406	50,2583	32,7759	0,0643	2,6906	2,1756	4,8662	1,3631	2,0016	3,3646	0,0000	6,234,748 ₈	1,9430	0,0000	6,283,473 ₇	
2021	4,0397	27,1470	35,4668	0,0994	5,5772	1,0126	6,5898	1,4920	0,9516	2,4436	0,0000	9,839,233 ₁	0,8881	0,0000	9,961,535 ₆	
2022	1,1622	11,1620	15,1030	0,0244	0,1677	0,5691	0,7368	0,0445	0,5236	0,5880	0,0000	2,367,818 ₉	0,7180	0,0000	2,385,769 ₉	
2023	20,2068	1,5044	4,7054	0,0123	1,0680	0,0778	1,0838	0,2668	0,0773	0,3441	0,0000	1,206,585 ₂	0,0387	0,0000	1,207,562 ₃	
Maximum	20,2068	50,2583	35,4668	0,0994	7,2470	2,3919	9,6390	3,9263	2,2006	6,1269	0,0000	9,839,233 ₁	1,9490	0,0000	9,961,535 ₆	
Percent Reduction	lb/day											CO ₂ e				
	ROG	NO _x	CO	SO ₂	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio-CO ₂		NBio-CO ₂	Total CO ₂	CH ₄	N ₂ O
0.00	0.00	0.00	0.00	0.00	46.89	0.00	37.29	53.15	0.00	35.52	0.00	0.00	0.00	0.00	0.00	0.00

Old Town Placentia - South Coast AQMD Air District, Summer

2.2 Overall Operational
Unmitigated Operational

Category	lb/day															
	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio-CO2	NBio-CO2	Total CO2	CH4	N2O	CO2e
Area	154.6033	11.3957	310.4648	0.6834		40.3429	40.3429		40.3429	40.3429	4.917532	9.528028	14.44566	14.7415	0.3338	14.91366
Energy	0.2991	2.6000	1.4123	0.0163		0.2066	0.2066		0.2066	0.2066		3.262608	3.262608	0.0625	0.0598	3.281367
Mobile	18.4712	89.0884	220.8915	0.7342	56.1706	0.7195	56.8901	15.0302	0.6754	15.7055		74.63414	74.63414	3.8313		74.72992
Total	173.3735	103.0841	532.6687	1.4339	56.1706	41.2691	97.4396	15.0302	41.2249	56.2550	4.917532	87.42475	92.34241	18.6353	0.3936	52.92568

Mitigated Operational

Category	lb/day															
	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio-CO2	NBio-CO2	Total CO2	CH4	N2O	CO2e
Area	17.0344	0.5028	43.4751	2.2800e-003		0.2390	0.2390		0.2390	0.2390	0.0000	78.0283	78.0283	0.0762	0.0000	78.9320
Energy	0.2991	2.6000	1.4123	0.0163		0.2066	0.2066		0.2066	0.2066		3.262608	3.262608	0.0625	0.0598	3.281367
Mobile	14.9194	63.9169	125.3386	0.3727	26.3558	0.3720	26.7278	7.0523	0.3488	7.4011		37.95197	37.95197	2.2540		38.00831
Total	32.2529	67.0197	170.2270	0.3913	26.3558	0.8177	27.1735	7.0523	0.7945	7.8468	0.0000	41.29260	41.29260	2.3926	0.0598	41.37024

Old Town Placentia - South Coast AQMD Air District, Summer

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio-CO2	Total CO2	CH4	N2O	CO2e
Percent Reduction	81.40	34.99	68.04	72.71	53.08	98.02	72.11	53.08	98.07	86.05	100.00	52.77	55.28	87.16	84.80	55.48

3.0 Construction Detail

Construction Phase

Phase Number	Phase Name	Phase Type	Start Date	End Date	Num Days Week	Num Days	Phase Description
1	Demolition	Demolition	1/1/2018	12/31/2018	5	261	
2	Site Preparation	Site Preparation	1/1/2019	12/31/2019	5	261	
3	Grading	Grading	1/1/2020	12/31/2020	5	262	
4	Building Construction	Building Construction	1/1/2021	12/31/2021	5	261	
5	Paving	Paving	1/1/2022	12/31/2022	5	260	
6	Architectural Coating	Architectural Coating	1/1/2023	12/30/2023	5	260	

Acres of Grading (Site Preparation Phase): 0

Acres of Grading (Grading Phase): 75

Acres of Paving: 0

Residential Indoor: 1,063,125; Residential Outdoor: 354,375; Non-Residential Indoor: 296,400; Non-Residential Outdoor: 98,800; Striped Parking Area: 0 (Architectural Coating – sqft)

OffRoad Equipment

INITIAL STUDY & NEGATIVE DECLARATION • CITY OF PLACENTIA
 OLD TOWN PLACENTIA REVITALIZATION PROJECT

Date: 5/15/2017 12:31 PM

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CalEEMod Version: CalEEMod.2016.3.1

Old Town Placentia - South Coast AQMD Air District, Summer

Phase Name	Offroad Equipment Type	Amount	Usage Hours	Horse Power	Load Factor
Demolition	Concrete/Industrial Saws	1	8.00	81	0.73
Demolition	Excavators	3	8.00	158	0.38
Demolition	Rubber Tired Dozers	2	8.00	247	0.40
Site Preparation	Rubber Tired Dozers	3	8.00	247	0.40
Site Preparation	Tractors/Loaders/Backhoes	4	8.00	97	0.37
Grading	Excavators	2	8.00	158	0.38
Grading	Graders	1	8.00	187	0.41
Grading	Rubber Tired Dozers	1	8.00	247	0.40
Grading	Scrapers	2	8.00	367	0.48
Grading	Tractors/Loaders/Backhoes	2	8.00	97	0.37
Building Construction	Cranes	1	7.00	231	0.29
Building Construction	Forklifts	3	8.00	89	0.20
Building Construction	Generator Sets	1	8.00	84	0.74
Building Construction	Tractors/Loaders/Backhoes	3	7.00	97	0.37
Building Construction	Welders	1	8.00	46	0.45
Paving	Pavers	2	8.00	130	0.42
Paving	Paving Equipment	2	8.00	132	0.36
Paving	Rollers	2	8.00	80	0.38
Architectural Coating	Air Compressors	1	6.00	78	0.48

Trips and VMT

Old Town Placentia - South Coast AQMD Air District, Summer

Phase Name	Offroad Equipment Count	Worker Trip Number	Vendor Trip Number	Hauling Trip Number	Worker Trip Length	Vendor Trip Length	Hauling Trip Length	Worker Vehicle Class	Vendor Vehicle Class	Hauling Vehicle Class
Demolition	6	15.00	0.00	0.00	14.70	6.90	20.00	LD_Mix	HDT_Mix	HHDT
Site Preparation	7	18.00	0.00	0.00	14.70	6.90	20.00	LD_Mix	HDT_Mix	HHDT
Grading	8	20.00	0.00	0.00	14.70	6.90	20.00	LD_Mix	HDT_Mix	HHDT
Building Construction	9	448.00	88.00	0.00	14.70	6.90	20.00	LD_Mix	HDT_Mix	HHDT
Paving	6	15.00	0.00	0.00	14.70	6.90	20.00	LD_Mix	HDT_Mix	HHDT
Architectural Coating	1	90.00	0.00	0.00	14.70	6.90	20.00	LD_Mix	HDT_Mix	HHDT

3.1 Mitigation Measures Construction

Water Exposed Area

3.2 Demolition - 2018

Unmitigated Construction On-Site

Category	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio-CO2	NBlc-CO2	Total CO2	CH4	N2O	CO2e
Off-Road	3.7190	38.3225	22.3040	0.0388	1.9386	1.9386	1.9386	1.8048	1.8048	1.8048	3.871766	3.871766	3.871766	1.0667	1.0667	3.898.434
Total	3.7190	38.3225	22.3040	0.0388	1.9386	1.9386	1.9386	1.8048	1.8048	1.8048	3.871766	3.871766	3.871766	1.0667	1.0667	3.898.434

3.2 Demolition - 2018
Unmitigated Construction Off-Site

Category	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio-CO2	MBio-CO2	Total CO2	CH4	N2O	CO2e
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000
Worker	0.0808	0.0580	0.7525	1.8400e-003	0.1677	1.3400e-003	0.1680	0.0445	1.2300e-003	0.0457	182.9028	182.9028	6.2400e-003	183.0587		183.0587
Total	0.0808	0.0580	0.7525	1.8400e-003	0.1677	1.3400e-003	0.1690	0.0445	1.2300e-003	0.0457	182.9028	182.9028	6.2400e-003	183.0587		183.0587

Mitigated Construction On-Site

Category	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio-CO2	MBio-CO2	Total CO2	CH4	N2O	CO2e
Off-Road	3.7190	38.3225	22.3040	0.0388		1.9386	1.9386		1.8048	1.8048	0.0000	3.8717665	3.8717665	1.0667		3.8954344
Total	3.7190	38.3225	22.3040	0.0388		1.9386	1.9386		1.8048	1.8048	0.0000	3.8717665	3.8717665	1.0667		3.8954344

Old Town Placentia - South Coast AQMD Air District, Summer

3.2 Demolition - 2018
Mitigated Construction Off-Site

Category	lb/day															
	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Biogenic CO2	NBIogenic CO2	Total CO2	CH4	N2O	CO2e
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Worker	0.0808	0.0680	0.7520	1.8400e-003	0.1677	1.3400e-003	0.1690	0.0445	1.2300e-003	0.0457	182.9028	182.9028	6.2400e-003	0.0000	0.0000	183.0587
Total	0.0808	0.0680	0.7520	1.8400e-003	0.1677	1.3400e-003	0.1690	0.0445	1.2300e-003	0.0457	182.9028	182.9028	6.2400e-003	0.0000	0.0000	183.0587

3.3 Site Preparation - 2019
Unmitigated Construction On-Site

Category	lb/day															
	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Biogenic CO2	NBIogenic CO2	Total CO2	CH4	N2O	CO2e
Fugitive Dust					18.0663	0.0000	18.0663	9.9307	0.0000	9.9307	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Off-Road	4.3350	45.5727	22.0630	0.0380	2.3604	2.3604	2.3604	2.1991	2.1991	2.1991	3,766.4529	3,766.4529	1.1917	1.1917	0.0000	3,796.2445
Total	4.3350	45.5727	22.0630	0.0380	18.0663	2.3604	20.4668	9.9307	2.1991	12.1288	3,766.4529	3,766.4529	1.1917	1.1917	0.0000	3,796.2445

Old Town Placentia - South Coast AQMD Air District, Summer

3.3 Site Preparation - 2019
Unmitigated Construction Off-Site

Category	lb/day																
	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio-CO2	NBio-CO2	Total CO2	CH4	N2O	CO2e	
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Worker	0.0882	0.0613	0.8088	2.1400e-003	0.2012	1.5700e-003	0.2028	0.0534	1.4400e-003	0.0548	212.5780	212.5780	212.5780	6.6500e-003	6.6500e-003	212.7442	212.7442
Total	0.0882	0.0613	0.8088	2.1400e-003	0.2012	1.5700e-003	0.2028	0.0534	1.4400e-003	0.0548	212.5780	212.5780	212.5780	6.6500e-003	6.6500e-003	212.7442	212.7442

Mitigated Construction On-Site

Category	lb/day																
	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio-CO2	NBio-CO2	Total CO2	CH4	N2O	CO2e	
Fugitive Dust					7.0458	0.0000	7.0458	3.8730	0.0000	3.8730			0.0000			0.0000	0.0000
Off-Road	4.3350	45.5727	22.0630	0.0380		2.3904	2.3904	2.1991	2.1991	2.1991	0.0000	3.7664529	3.7664529	1.1917		3.7962445	3.7962445
Total	4.3350	45.5727	22.0630	0.0380	7.0458	2.3904	9.4362	3.8730	2.1991	6.0721	0.0000	3.7664529	3.7664529	1.1917		3.7962445	3.7962445

Old Town Placentia - South Coast AQMD Air District, Summer

**3.3 Site Preparation - 2019
 Mitigated Construction Off-Site**

Category	lb/day											CO2e				
	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Biogenic CO2		NH3+CO2	Total CO2	CH4	N2O
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Worker	0.0882	0.0613	0.8088	2.1400e-003	0.2012	1.5700e-003	0.2028	0.0534	1.4400e-003	0.0548	212.5780	212.5780	6.8500e-003	6.8500e-003	212.7442	212.7442
Total	0.0882	0.0613	0.8088	2.1400e-003	0.2012	1.5700e-003	0.2028	0.0534	1.4400e-003	0.0548	212.5780	212.5780	6.8500e-003	6.8500e-003	212.7442	212.7442

**3.4 Grading - 2020
 Unmitigated Construction On-Site**

Category	lb/day											CO2e				
	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Biogenic CO2		NH3+CO2	Total CO2	CH4	N2O
Fugitive Dust					6.3257	0.0000	6.3257	3.3430	0.0000	3.3430			0.0000			0.0000
Off-Road	4.4501	50.1675	31.9563	0.0620		2.1739	2.1739		2.0000	2.0000	6,005.8653	6,005.8653	1.9424	1.9424		6,054.4257
Total	4.4501	50.1675	31.9563	0.0620	6.3257	2.1739	8.4996	3.3430	2.0000	5.3430	6,005.8653	6,005.8653	1.9424	1.9424		6,054.4257

3.4 Grading - 2020

Unmitigated Construction Off-Site

Category	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio-CO2	NEBio-CO2	Total CO2	CH4	N2O	CO2e
	lb/day															
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000	0.0000		0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000	0.0000		0.0000
Worker	0.0905	0.0608	0.8176	2.3000e-003	0.2236	1.7000e-003	0.2263	0.0593	1.5600e-003	0.0609		228.8835	228.8835	6.5800e-003		229.0480
Total	0.0905	0.0608	0.8176	2.3000e-003	0.2236	1.7000e-003	0.2263	0.0593	1.5600e-003	0.0609		228.8835	228.8835	6.5800e-003		229.0480

Mitigated Construction On-Site

Category	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio-CO2	NEBio-CO2	Total CO2	CH4	N2O	CO2e
	lb/day															
Fugitive Dust					2.4670	0.0000	2.4670	1.3038	0.0000	1.3038			0.0000			0.0000
Off-Road	4.4501	50.1975	31.9683	0.0620		2.1739	2.1739		2.0000	2.0000	0.0000	6.005.8653	6.005.8653	1.9424		6,054.4257
Total	4.4501	50.1975	31.9683	0.0620	2.4670	2.1739	4.6409	1.3038	2.0000	3.3038	0.0000	6,005.8653	6,005.8653	1.9424		6,054.4257

3.4 Grading - 2020

Mitigated Construction Off-Site

Category	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
	lb/day															
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000	0.0000		0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000	0.0000		0.0000
Worker	0.0905	0.0908	0.8176	2.3000e-003	0.2236	1.7000e-003	0.2253	0.0583	1.5600e-003	0.0609		228.8835	228.8835	6.5800e-003		229.0480
Total	0.0905	0.0908	0.8176	2.3000e-003	0.2236	1.7000e-003	0.2253	0.0583	1.5600e-003	0.0609		228.8835	228.8835	6.5800e-003		229.0480

3.5 Building Construction - 2021

Unmitigated Construction On-Site

Category	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
	lb/day															
On-Road	1.9009	17.4321	16.5752	0.0289		0.9586	0.9586		0.9013	0.9013		2,553.3639	2,553.3639	0.6160		2,565.7643
Total	1.9009	17.4321	16.5752	0.0289		0.9586	0.9586		0.9013	0.9013		2,553.3639	2,553.3639	0.6160		2,565.7643

Old Town Placentia - South Coast AQMD Air District, Summer

3.5 Building Construction - 2021

Unmitigated Construction Off-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000	0.0000		0.0000
Vendor	0.2477	8.4884	2.0143	0.0227	0.5696	0.0171	0.5867	0.1640	0.0164	0.1803		2,424.7024	2,424.7024	0.1467		2,428.3692
Worker	1.8911	1.2255	16.8773	0.0498	5.0076	0.0369	5.0444	1.3280	0.0340	1.3620		4,961.1669	4,961.1669	0.1334		4,964.5022
Total	2.1388	9.7149	18.8916	0.0725	5.5772	0.0540	5.6312	1.4920	0.0503	1.5423		7,385.8692	7,385.8692	0.2801		7,392.8714

Mitigated Construction On-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Off-Road	1.9009	17.4321	16.5752	0.0269		0.9586	0.9586		0.9013	0.9013	0.0000	2,553.3639	2,553.3639	0.6160		2,568.7643
Total	1.9009	17.4321	16.5752	0.0269		0.9586	0.9586		0.9013	0.9013	0.0000	2,553.3639	2,553.3639	0.6160		2,568.7643

Old Town Placentia - South Coast AQMD Air District, Summer

**3.5 Building Construction - 2021
 Mitigated Construction Off-Site**

Category	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
lb/day																
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000
Vendor	0.2477	8.4884	2.0143	0.0227	0.5686	0.0171	0.5867	0.1640	0.0164	0.1803	2,424.7024	2,424.7024	2,424.7024	0.1467		2,428.3692
Worker	1.8911	1.2365	16.8773	0.0488	5.0076	0.0369	5.0444	1.3280	0.0340	1.3620	4,961.1669	4,961.1669	4,961.1669	0.1334		4,964.5022
Total	2.1388	9.7149	18.8916	0.0725	5.5772	0.0540	5.6312	1.4920	0.0503	1.5423	7,385.8692	7,385.8692	7,385.8692	0.2801		7,392.8714

**3.6 Paving - 2022
 Unmitigated Construction On-Site**

Category	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
lb/day																
Off-Road	1.1028	11.1249	14.5805	0.0228		0.5679	0.5679		0.5225	0.5225	2,207.6603	2,207.6603	2,207.6603	0.7140		2,225.5104
Paving	0.0000					0.0000	0.0000		0.0000	0.0000			0.0000			0.0000
Total	1.1028	11.1249	14.5805	0.0228		0.5679	0.5679		0.5225	0.5225	2,207.6603	2,207.6603	2,207.6603	0.7140		2,225.5104

Old Town Placentia - South Coast AQMD Air District, Summer

3.6 Paving - 2022

Unmitigated Construction Off-Site

Category	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
lb/day																
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000	0.0000		0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000	0.0000		0.0000
Worker	0.0594	0.0371	0.5225	1.6100e-003	0.1677	1.2000e-003	0.1689	0.0445	1.1000e-003	0.0456		160.1586	160.1586	4.0400e-003		160.2595
Total	0.0594	0.0371	0.5225	1.6100e-003	0.1677	1.2000e-003	0.1689	0.0445	1.1000e-003	0.0456		160.1586	160.1586	4.0400e-003		160.2595

Mitigated Construction On-Site

Category	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
lb/day																
Off-Road	1.1028	11.1249	14.5805	0.0228		0.5679	0.5679	0.5225	0.5225	0.5225	0.0000	2,207,660	2,207,660	0.7140		2,225.510
Paving	0.0000					0.0000	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000			0.0000
Total	1.1028	11.1249	14.5805	0.0228		0.5679	0.5679	0.5225	0.5225	0.5225	0.0000	2,207,660	2,207,660	0.7140		2,225.510

Old Town Placentia - South Coast AQMD Air District, Summer

3.6 Paving - 2022

Mitigated Construction Off-Site

Category	lb/day											CO2e					
	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Biogenic CO2		NH3+CO2	Total CO2	CH4	N2O	CO2e
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Worker	0.0594	0.0371	0.5225	1.6100e-003	0.1577	1.2000e-003	0.1689	0.0445	1.1000e-003	0.0456	160.1586	160.1586	4.0400e-003	0.0000	0.0000	160.2595	160.2595
Total	0.0594	0.0371	0.5225	1.6100e-003	0.1577	1.2000e-003	0.1689	0.0445	1.1000e-003	0.0456	160.1586	160.1586	4.0400e-003	0.0000	0.0000	160.2595	160.2595

3.7 Architectural Coating - 2023

Unmitigated Construction On-Site

Category	lb/day											CO2e					
	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Biogenic CO2		NH3+CO2	Total CO2	CH4	N2O	CO2e
Archit. Coating	19.5800				0.0000	0.0000	0.0000	0.0000	0.0000	0.0000			0.0000			0.0000	0.0000
Off-Road	0.1917	1.3030	1.8111	2.9700e-003	0.0708	0.0708	0.0708	0.0708	0.0708	0.0708	281.4481	281.4481	0.0168	0.0168		281.9690	281.9690
Total	19.8717	1.3030	1.8111	2.9700e-003	0.0708	0.0708	0.0708	0.0708	0.0708	0.0708	281.4481	281.4481	0.0168	0.0168		281.9690	281.9690

Old Town Placentia - South Coast AQMD Air District, Summer

3.7 Architectural Coating - 2023

Unmitigated Construction Off-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000	0.0000		0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000	0.0000		0.0000
Worker	0.3351	0.2014	2.8953	9.2800e-003	1.0060	7.0100e-033	1.0130	0.2668	6.4500e-003	0.2732		925.1372	925.1372	0.0219		925.6833
Total	0.3351	0.2014	2.8953	9.2800e-003	1.0060	7.0100e-033	1.0130	0.2668	6.4500e-003	0.2732		925.1372	925.1372	0.0219		925.6833

Mitigated Construction On-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Archit. Coaling	19.6800					0.0000	0.0000		0.0000	0.0000			0.0000			0.0000
Off-Road	0.1917	1.3030	1.8111	2.9700e-003		0.0708	0.0708		0.0708	0.0708	0.0000	281.4481	281.4481	0.0168		281.8690
Total	19.8717	1.3030	1.8111	2.9700e-003		0.0708	0.0708		0.0708	0.0708	0.0000	281.4481	281.4481	0.0168		281.8690

Old Town Placentia - South Coast AQMD Air District, Summer

**3.7 Architectural Coating - 2023
 Mitigated Construction Off-Site**

Category	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bis- CO2	NBlr- CO2	Total CO2	CH4	N2O	CO2e
	lb/day															
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000	0.0000		0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000	0.0000		0.0000
Worker	0.3351	0.2014	2.8953	9.2800e-003	1.0060	7.0100e-003	1.0130	0.2668	6.4500e-003	0.2732		925.1372	925.1372	0.0219		925.6833
Total	0.3351	0.2014	2.8953	9.2800e-003	1.0060	7.0100e-003	1.0130	0.2668	6.4500e-003	0.2732		925.1372	925.1372	0.0219		925.6833

4.0 Operational Detail - Mobile

4.1 Mitigation Measures Mobile

- Increase Diversity
- Improve Destination Accessibility
- Increase Transit Accessibility
- Improve Pedestrian Network

Old Town Placentia - South Coast AQMD Air District, Summer

Category	lb/day															
	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio-CO2	NBio-CO2	Total CO2	CH4	N2O	CO2e
Mitigated	14,9184	63,9189	125,3396	0,3727	26,3558	0,3720	26,7278	7,0623	0,3488	7,4011	37,95197	37,95197	74,90394	2,2540	0,00	38,00831
Unmitigated	18,4712	89,0894	220,6915	0,7342	56,1706	0,7195	56,8901	15,0302	0,6754	15,7055	74,63414	74,63414	149,26828	3,8313	0,00	78,50000
											64	64	128			80

4.2 Trip Summary Information

Land Use	Average Daily Trip Rate			Unmitigated Annual VMT	Mitigated Annual VMT
	Weekday	Saturday	Sunday		
Apartment Mid Rise	3,491,25	3,354,75	3,076,50	11,661,031	5,471,472
Hotel	408,50	409,50	297,50	937,254	439,769
Regional Shopping Center	5,337,50	6,246,25	3,155,00	11,150,619	5,231,981
Total	9,237,25	10,010,50	6,528,00	23,748,904	11,143,222

4.3 Trip Type Information

Land Use	Miles				Trip %			Trip Purpose %		
	H-W or C-W	H-S or C-C	H-O or C-NW	H-W or C-W	H-S or C-C	H-O or C-NW	Primary	Diverted	Pass-by	
Apartment Mid Rise	14,70	5,90	8,70	40,20	19,20	40,60	86	11	3	
Hotel	15,60	8,40	6,90	18,40	61,00	19,00	58	38	4	
Regional Shopping Center	16,60	8,40	6,90	10,30	64,70	18,00	54	35	11	

4.4 Fleet Mix

Old Town Placentia - South Coast AQMD Air District, Summer

Land Use	LDA	LDT1	LDT2	MDV	LHD1	LHD2	MHD	HHD	OBUS	UBUS	MCY	SBUS	MH
Hotel	0.547828	0.043645	0.199892	0.122290	0.016774	0.005862	0.020637	0.032653	0.002037	0.001944	0.004777	0.000705	0.000956
Apartments Mid Rise	0.547828	0.043645	0.199892	0.122290	0.016774	0.005862	0.020637	0.032653	0.002037	0.001944	0.004777	0.000705	0.000956
Regional Shopping Center	0.547828	0.043645	0.199892	0.122290	0.016774	0.005862	0.020637	0.032653	0.002037	0.001944	0.004777	0.000705	0.000956

5.0 Energy Detail

Historical Energy Use: N

5.1 Mitigation Measures Energy

Category	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
	lb/day										lb/day					
Natural Gas Mitigated	0.2991	2.6000	1.4123	0.0163		0.2066	0.2066		0.2066	0.2066		3,262,609 3	3,262,609 3	0.0625	0.0598	3,281.997 3
Natural Gas Unmitigated	0.2991	2.6000	1.4123	0.0163		0.2066	0.2066		0.2066	0.2066		3,262,609 3	3,262,609 3	0.0625	0.0598	3,281.997 3

Old Town Placentia - South Coast AQMD Air District, Summer

5.2 Energy by Land Use - Natural Gas

Unmitigated

Land Use	lb/day											lb/day					
	Natural Gas Use (MBTU/yr)	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio-CO2	NEBio-CO2	Total CO2	CH4	N2O	CO2e
Apartments Mid Rise	20203.5	0.2179	1.8619	0.7923	0.0119		0.1505	0.1505		0.1505	0.1505		2,376,883.6	2,376,883.6	0.0456	0.0436	2,391,008.2
Hotel	8940.31	0.0738	0.6706	0.5633	4.0200e-003		0.0510	0.0510		0.0510	0.0510		804,7426	804,7426	0.0154	0.0148	809,5248
Regional Shopping Center	688,356	7.4200e-003	0.0675	0.0567	4.0000e-004		5.1300e-003	5.1300e-003		5.1300e-003	5.1300e-003		80,9831	80,9831	1.5500e-003	1.4800e-003	81,4643
Total		0.2991	2.6000	1.4123	0.0163		0.2066	0.2066		0.2066	0.2066		3,262,609.3	3,262,609.3	0.0625	0.0598	3,281,997.3

Mitigated

Land Use	lb/day											lb/day					
	Natural Gas Use (MBTU/yr)	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio-CO2	NEBio-CO2	Total CO2	CH4	N2O	CO2e
Apartments Mid Rise	20,203.5	0.2179	1.8619	0.7923	0.0119		0.1505	0.1505		0.1505	0.1505		2,376,883.6	2,376,883.6	0.0456	0.0436	2,391,008.2
Hotel	8,940.31	0.0738	0.6706	0.5633	4.0200e-003		0.0510	0.0510		0.0510	0.0510		804,7426	804,7426	0.0154	0.0148	809,5248
Regional Shopping Center	688,356	7.4200e-003	0.0675	0.0567	4.0000e-004		5.1300e-003	5.1300e-003		5.1300e-003	5.1300e-003		80,9831	80,9831	1.5500e-003	1.4800e-003	81,4643
Total		0.2991	2.6000	1.4123	0.0163		0.2066	0.2066		0.2066	0.2066		3,262,609.3	3,262,609.3	0.0625	0.0598	3,281,997.3

6.0 Area Detail

Old Town Placentia - South Coast AQMD Air District, Summer

6.1 Mitigation Measures Area

- Use Low VOC Paint - Residential Interior
- Use Low VOC Paint - Residential Exterior
- Use Low VOC Paint - Non-Residential Interior
- Use Low VOC Paint - Non-Residential Exterior
- No Hearths Installed

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio-CO2	NBio-CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Mitigated	17.0344	0.5028	43.4751	2.2900e-003		0.2390	0.2390		0.2390	0.2390	0.0000	78.0283	78.0283	0.0762	0.0000	79.9320
Unmitigated	154.6033	11.3957	310.4648	0.6834		40.3429	40.3429		40.3429	40.3429	4,917.6322	9,528.0283	14,445.6604	14.7415	0.3338	14,913.6621

Old Town Placentia - South Coast AQMD Air District, Summer

6.2 Area by SubCategory

Unmitigated

SubCategory	lb/day															
	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio-CO2	NBio-CO2	Total CO2	CH4	N2O	CO2e
Architectural Coating	1.4019					0.0000	0.0000		0.0000	0.0000			0.0000			0.0000
Consumer Products	14.3075					0.0000	0.0000		0.0000	0.0000			0.0000			0.0000
Hearth	137.5685	10.8929	266.9697	0.6811		40.1039	40.1039		40.1039	40.1039	4.9175322	9.4500000	14.3676322	14.6653	0.3338	14.8337300
Landscaping	1.3250	0.5028	43.4751	2.2500e-003		0.2390	0.2390		0.2390	0.2390		78.0283	78.0283	0.0752		79.9320
Total	154.6033	11.3957	310.4648	0.6834		40.3429	40.3429		40.3429	40.3429	4.9175322	9.5280283	14.4456604	14.7415	0.3338	14.9136621

Old Town Placentia - South Coast AQMD Air District, Summer

6.2 Area by SubCategory

Mitigated

SubCategory	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio-CO2	NBio-CO2	Total CO2	CH4	N2O	CO2e
	lb/day															
Architectural Coating	1.4019					0.0000	0.0000		0.0000	0.0000			0.0000			0.0000
Consumer Products	14.3075					0.0000	0.0000		0.0000	0.0000			0.0000			0.0000
Health	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000		0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Landscaping	1.3250	0.5028	43.4751	2.2800e-003		0.2390	0.2390		0.2390	0.2390	78.0283	78.0283	78.0283	0.0762		79.9320
Total	17.0344	0.5028	43.4751	2.2800e-003		0.2390	0.2390		0.2390	0.2390	78.0283	78.0283	78.0283	0.0762	0.0000	79.9320

7.0 Water Detail

7.1 Mitigation Measures Water

- Install Low Flow Bathroom Faucet
- Install Low Flow Kitchen Faucet
- Install Low Flow Toilet
- Install Low Flow Shower

8.0 Waste Detail

8.1 Mitigation Measures Waste

9.0 Operational Offroad

Equipment Type	Number	Hours/Day	Days/Year	Horse Power	Load Factor	Fuel Type
----------------	--------	-----------	-----------	-------------	-------------	-----------

10.0 Stationary Equipment

Fire Pumps and Emergency Generators

Equipment Type	Number	Hours/Day	Hours/Year	Horse Power	Load Factor	Fuel Type
----------------	--------	-----------	------------	-------------	-------------	-----------

Boilers

Equipment Type	Number	Heat Input/Day	Heat Input/Year	Boiler Rating	Fuel Type
----------------	--------	----------------	-----------------	---------------	-----------

User Defined Equipment

Equipment Type	Number
----------------	--------

11.0 Vegetation

APPENDIX B
DKS ASSOCIATES TRAFFIC IMPACT ANALYSIS

Old Town Revitalization Project Traffic Impact Analysis

Prepared for the
City of Placentia



Prepared by:
DKS Associates

Prepared On:
May 17th, 2017

Traffic Impact Analysis

OLD TOWN REVITALIZATION PROJECT

CITY OF PLACENTIA, CA

Prepared by



Project No. 17066-000
Submitted May 17, 2017

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APPENDICES

- Appendix A – Raw Turning Movement Counts
- Appendix B – Existing (2017) Intersection Level of Service Worksheets
- Appendix C – Existing (2017) Plus Project Intersection Level of Service Worksheets
- Appendix D – Cumulative Project Information
- Appendix E – Project Buildout Year (2037) Without Project Intersection Level of Service Worksheets
- Appendix F – Project Buildout Year (2037) With Project Intersection Level of Service Worksheets



1.0 INTRODUCTION

The following presents the Traffic Impact Analysis (TIA) prepared by DKS Associates (DKS) for the Old Town Revitalization Project in the City of Placentia, CA (City). The Old Revitalization Project is bound by Alta Street in the east, the Burlington Northern and Santa Fe Railway train tracks in the south, Chapman Avenue in the north and Murray Street in the west. The revitalization project consists of adding 525 residential units, 85,000 square feet (sf) of commercial use, 40,000 sf of retail use, and a 50-room hotel to the existing area. This TIA has been prepared consistent with the policies of the City of Placentia guidelines, discussions with the City staff, and methodologies from the Institute of Transportation Engineers (ITE) manuals.

Purpose and Objectives of the TIA

Based on discussions with the City, the purpose of this TIA is to evaluate the traffic and circulation impacts of the proposed project. The study objectives of this TIA include:

- Documentation of existing traffic conditions and future traffic conditions corresponding to the “future year” (existing plus ambient growth) of the proposed project when it would be completely built-out. The project completion year is 2037.
- Determination of additional mitigation measures needed to achieve City Level-Of-Service (LOS) requirements with implementation of the proposed project (if required).

Analysis Scenarios

Based on discussions with the City, the analysis was conducted at the study intersections for the following scenarios as part of the TIA:

- 1) Existing (2017) Conditions
- 2) Existing (2017) Plus Project Conditions
- 3) Project Buildout Year (2037) Without Project Conditions
- 4) Project Buildout Year (2037) With Project Conditions

Site Location and Study Area

The Old Town Revitalization Project is bound by Alta Street in the east, the Burlington Northern and Santa Fe Railway train tracks in the south, Chapman Avenue in the north and Murray Street in the west. Regional access is provided by the State Route 57 Freeway (SR-57).



Based on discussions with the City staff, the project's traffic related impacts will be evaluated at following eight (8) intersections:

- 1) Chapman Avenue/SR-57 Southbound Ramps
- 2) Chapman Avenue/SR-57 Northbound Ramps
- 3) Chapman Avenue/Placentia Avenue
- 4) Chapman Avenue/Murray Street
- 5) Chapman Avenue/Melrose Street
- 6) Chapman Avenue/Bradford Avenue
- 7) Santa Fe Street/Melrose Street (All-Way Stop)
- 8) Center Street/Bradford Avenue (All-Way Stop)

Figure 1 illustrates the project site location and study intersections.

Methodology

City of Placentia General Plan guidelines and the Orange County Congestion Management Program (CMP) require that the analysis of signalized intersections be performed using the Intersection Capacity Utilization (ICU) methodology. The assessment of intersection conditions addresses LOS, in terms of volume-to-capacity (V/C) ratio under the ICU analysis for signalized intersections. Unsignalized intersections and intersections under jurisdiction of Caltrans require the analysis to be performed using the Highway Capacity Manual (HCM) methodology. The assessment of intersection conditions addresses LOS in terms of control delay for HCM analysis.

The Traffix Version 8.0 software package was used to determine intersection LOS based on ICU methodology and HCM methodology for the study intersections. Brief LOS definitions along with the corresponding volume to capacity ratio for the ICU methodology are shown in Table A1. The corresponding control delays for the HCM methodology for signalized and unsignalized intersections are shown in Tables A2 and A3, respectively.

The degree of congestion at an intersection is described by the level-of-service, which ranges from LOS A to LOS F, with LOS A representing free-flow conditions with little delay and LOS F representing over-saturated traffic flow throughout the peak hour. Table B provides a description of each specific LOS grade (LOS A through LOS F).

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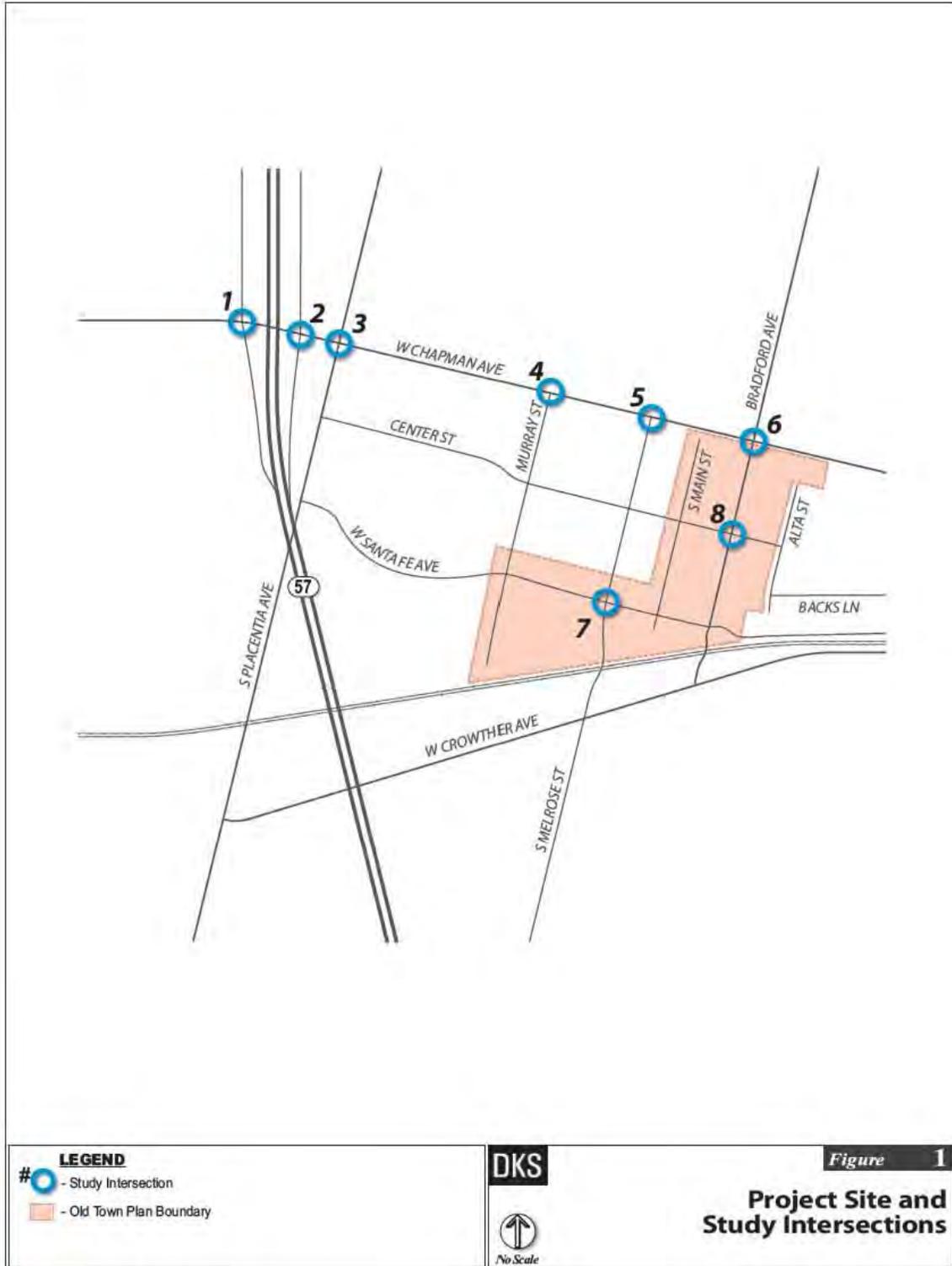




Table A1 – Level of Service Definitions for Signalized Intersections Based on ICU V/C

Level of Service	V/C Ratio
A	0.00-0.60
B	0.61-0.70
C	0.71-0.80
D	0.81-0.90
E	0.91-1.00
F	1.01 or greater

SOURCE: Orange County Congestion Management Plan, 2015

Table A2 – Level of Service Definitions for Signalized Intersections Based on HCM Delay

Level of Service	Delay per Vehicle (in seconds)
A	≤ 0 - 10
B	> 10 - 20
C	> 20 - 35
D	> 35 - 55
E	> 55 - 80
F	> 80

SOURCE: Highway Capacity Manual, Transportation Research Board, Special Report No. 209, Washington, D.C., 2000.

Table A3 – Level of Service Definitions for Unsignalized Intersections Based on HCM Delay

Level of Service	Delay per Vehicle (in seconds)
A	≤ 0 - 10
B	> 10 - 15
C	> 15 - 25
D	> 25 - 35
E	> 35 - 50
F	> 50

SOURCE: Highway Capacity Manual, Transportation Research Board, Special Report No. 209, Washington, D.C., 2000.



Table B – Level of Service Descriptions

LOS	Description
A	No approach phase is fully utilized by traffic, and no vehicle waits longer than one red indication. Typically, the approach appears quite open, turns are made easily, and nearly all drivers find freedom of operation.
B	This service level represents stable operation, where an occasional approach phase is fully utilized and a substantial number are nearing full use. Many drivers begin to feel restricted within platoons of vehicles.
C	This level still represents stable operating conditions. Occasionally drivers may have to wait through more than one red signal indication, and backups may develop behind turning vehicles. Most drivers feel somewhat restricted, but not objectionably so.
D	This level encompasses a zone of increasing restriction approaching instability at the intersection. Delays to approaching vehicles may be substantial during short peaks within the peak period; however, enough cycles with lower demand occur to permit periodic clearance of developing queues, thus preventing excessive backups.
E	Capacity occurs at the upper end of this service level. It represents the most vehicles that any particular intersection approach can accommodate. Full utilization of every signal cycle is seldom attained no matter how great the demand.
F	This level describes forced flow operations at low speeds, where volumes exceed capacity. These conditions usually result from queues of vehicles backing up from a restriction downstream. Speeds are reduced substantially, and stoppages may occur for short or long periods of time due to the congestion. In the extreme case, both speed and volume can drop to zero.

SOURCE: Highway Capacity Manual, Transportation Research Board, Special Report No. 209, Washington, D.C., 2000.

Significance Criteria

Based on the City of Placentia’s Circulation Element, the acceptable level-of-service for all study intersections is LOS D or better. Therefore, any intersection operating at a LOS E or F will be considered deficient. An intersection is considered to be significantly impacted if the project related increase in the v/c ratio equals or exceeds 0.01, if it is operating at LOS E or LOS F.

For intersections under the jurisdiction of Caltrans, the significant impact criteria is based on the ‘Caltrans Guide for the Preparation of Traffic Impact Studies’ document. Caltrans maintains a target LOS at the transition between LOS C and LOS D using the HCM methodology. The project impact on a Caltrans intersection would be significant if the project either causes an intersection operating at LOS C to deteriorate to LOS D or worse or causes an intersection already operating at LOS D or worse to deteriorate to a worse level of service.



2.0 EXISTING CONDITIONS

Key roadways in the study area are summarized in Table C along with their existing characteristics. As shown, all surrounding roadways within the study area network are under the jurisdiction of City of Placentia, with the exception of the SR-57 Freeway.

Table C – Existing Roadway Network Characteristics

Roadway	Roadway Jurisdiction	Roadway Classification	Cross-Section	Posted Speed	Pedestrian Facilities	Bicycle Facilities	Transit Facilities
Chapman Avenue	City of Placentia	Primary Arterial	4 Lanes	35/40 mph	Sidewalks	None	OCTA
Placentia Avenue	City of Placentia	Secondary Arterial	4 Lanes	40 mph	Sidewalks	None	OCTA
Murray Street	City of Placentia	Local Street	2 Lanes	25 mph	Sidewalks	None	None
Melrose Street	City of Placentia	Secondary Arterial	2 Lanes	25 mph	Sidewalks	None	None
Bradford Avenue	City of Placentia	Secondary Arterial	2 Lanes	25 mph	Sidewalks	None	OCTA
Center Street	City of Placentia	Local Street	2 Lanes	25 mph	Sidewalks	None	None
Santa Fe Avenue	City of Placentia	Local Street	2/4 Lanes	25/35 mph	Sidewalks	None	None
SR-57 Freeway	Caltrans	Freeway	10 Lanes	60 mph	None	None	None

Figure 2 illustrates the existing roadway conditions for the study area roadways. The number of through traffic lanes and the existing intersection controls are identified.

Existing (2017)

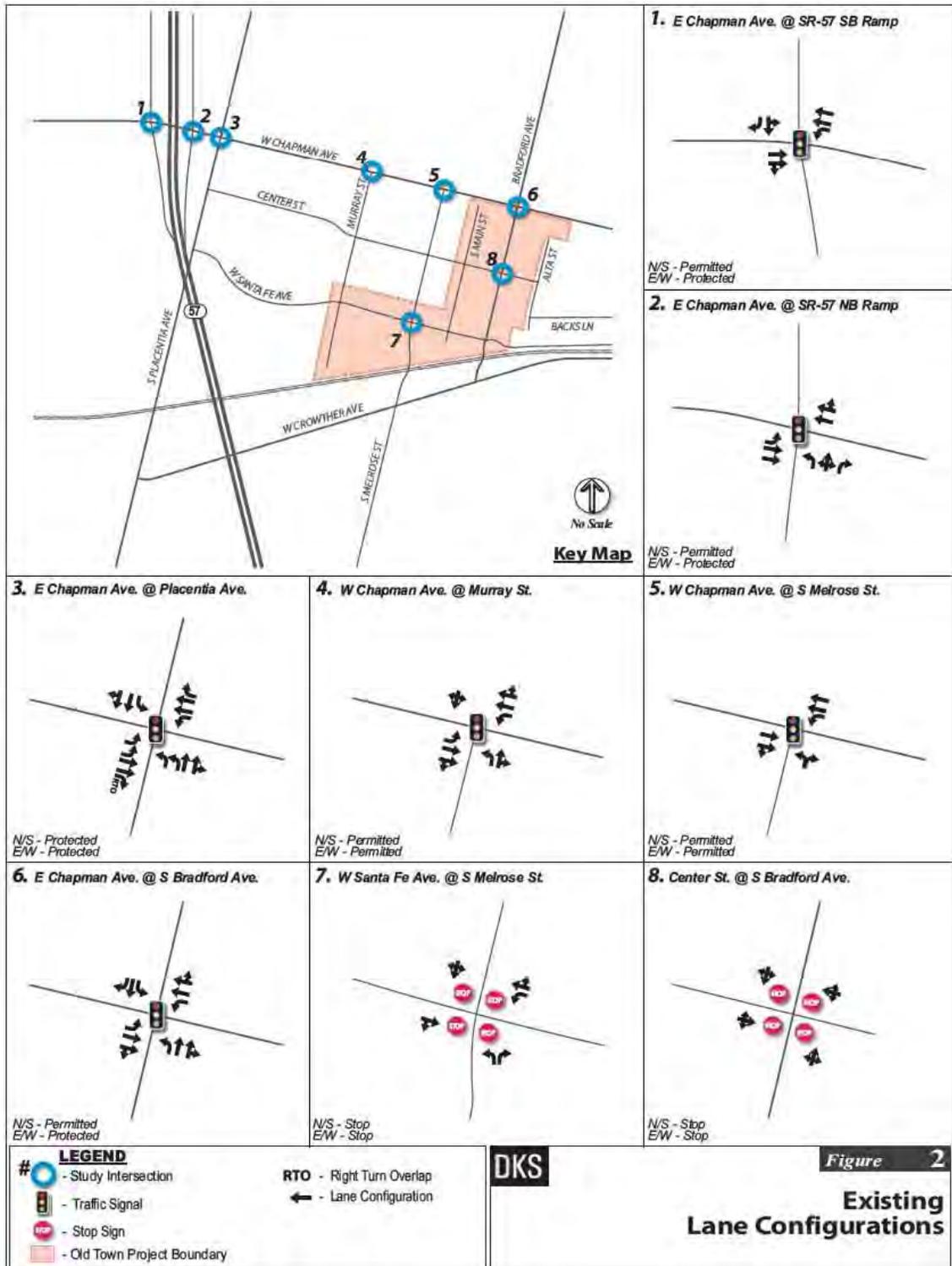
Traffic Volumes

Existing traffic volumes at all study intersections were collected on Wednesday, April 26, 2017. The peak hours were determined by combining the four highest adjacent 15 minute periods during the AM peak period (7:00-9:00 AM) and the PM peak period (4:00-6:00 PM) at the intersections. Figure 3 illustrates the existing AM and PM peak hour traffic volumes at the study intersections. The actual counts are provided in Appendix A.

Existing Level of Service

The existing level of service has been evaluated at the study intersections based on the ICU methodology for signalized intersections and the 2000 HCM methodology for all-way stop controlled and Caltrans intersections. The LOS summary is shown in Table D. As shown, all intersections operate at LOS C or better. LOS calculation sheets are provided in Appendix B.

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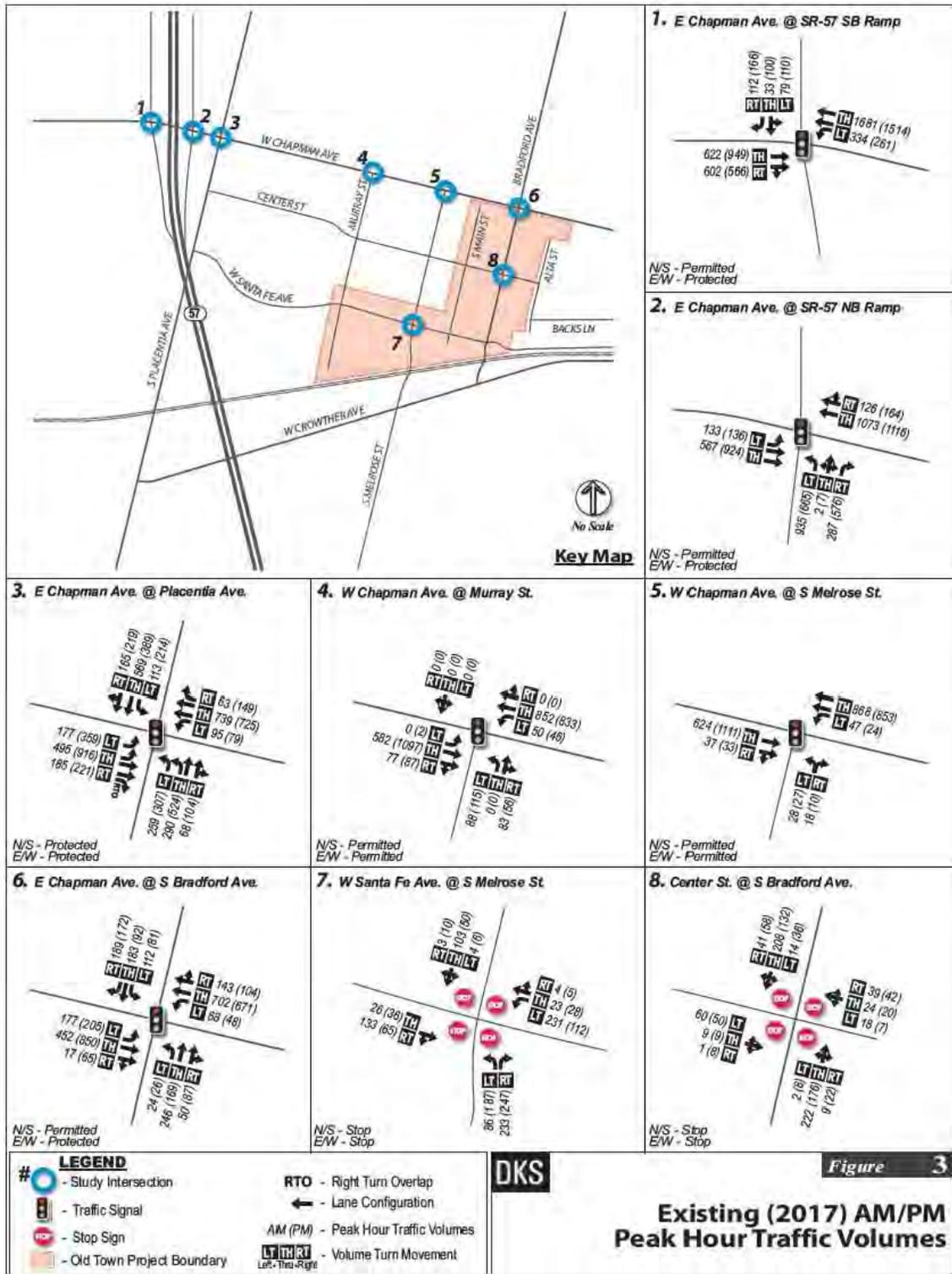




Table D: Existing (2017) Intersection Level of Service Summary¹

Intersection	AM Peak Hour		PM Peak Hour	
	V/C or Delay (sec.)	LOS	V/C or Delay (sec.)	LOS
<i>Caltrans - Signalized</i>				
Chapman Avenue/SR-57 Southbound Ramps	12.7	B	15.4	B
Chapman Avenue/SR-57 Northbound Ramps	26.4	C	26.2	C
<i>City of Placentia - Signalized</i>				
Chapman Avenue/Placentia Avenue	0.611	B	0.679	B
Chapman Avenue/Murray Street	0.352	A	0.493	A
Chapman Avenue/Melrose Street	0.332	A	0.416	A
Chapman Avenue/Bradford Avenue	0.556	A	0.521	A
<i>City of Placentia - All-Way Stop Controlled</i>				
Santa Fe Street/Melrose Street	11.1	B	9.9	A
Center Street/Bradford Avenue	8.6	A	8.7	A

¹Analysis Software: Traffix, Version 8.0. Per the Intersection Capacity Utilization methodology, overall volume to capacity ratios and levels of service are shown for intersections controlled by traffic signals. Per the Highway Capacity Manual (HCM 2000) methodology, overall average intersection delay and level of service are shown for intersections controlled by all-way stop and for intersections under the jurisdiction of Caltrans.



3.0 PROJECT DESCRIPTION

Project Size and Description

Figure 4 illustrates the Old Town Placentia District Zoning Map. The revitalization project would add 525 residential units, 85,000 sf of commercial use, 40,000 sf of retail use, and a 50-room hotel. Based on discussions with the City, the proposed residential use will be a combination of apartments and townhomes. As a conservative approach, this analysis will assume 100% apartment use due to the greater trip generation rate compared to townhomes.

Currently, the Old Town area contains a mix of single-family residential use and various commercial uses. As we understand, the majority of properties in the area are developed. Therefore, the majority of the proposed land uses are expected to be infill development. While it can be expected that some existing land uses may be replaced, DKS is assuming the Old Town Revitalization Project consists of the proposed development, with all existing land uses to remain.

Project Traffic

Trip Generation Credit

Based on discussions with City staff, the City has approved the use of three trip credits, transit, pass-by, and internal capture. Due to the project's vicinity to transit facilities, including the proposed Placentia Metrolink Station, and the project's goal to develop a walkable urban environment, DKS applied a transit trip credit. The Orange County Congestion Management Program (CMP) highlights the benefits of utilizing pass-by and internal capture credits for mixed-used developments, such as the Old Town Revitalization Project. Pass-by traffic is retail traffic that is already on the road and driving by the site. The driver will stop at the retail shop on their way to their final destination. Internal capture is the portion of trips generated by a development that both begin and end within the site. Based on techniques outlined in the Institute of Transportation Engineers' (ITE) Trip Generation Manual, 9th Edition, pass-by and internal capture trip credits were estimated. As shown in Table E, a 34% pass-by credit can be applied to shopping center use and a 12% internal capture trip credit can be applied to the overall development.

Project Trip Generation

Trip generation estimates for the proposed project were developed using trip rates established in the ITE Trip Generation Manual, 9th Edition. As shown in Table E, the combination of the proposed development and the applied trip credits results in approximately 7,361 trip-ends per day, with 306 (115 inbound, 191 outbound) trips during the AM peak hour and 510 (281 inbound, 229 outbound) trips during the PM peak hour.

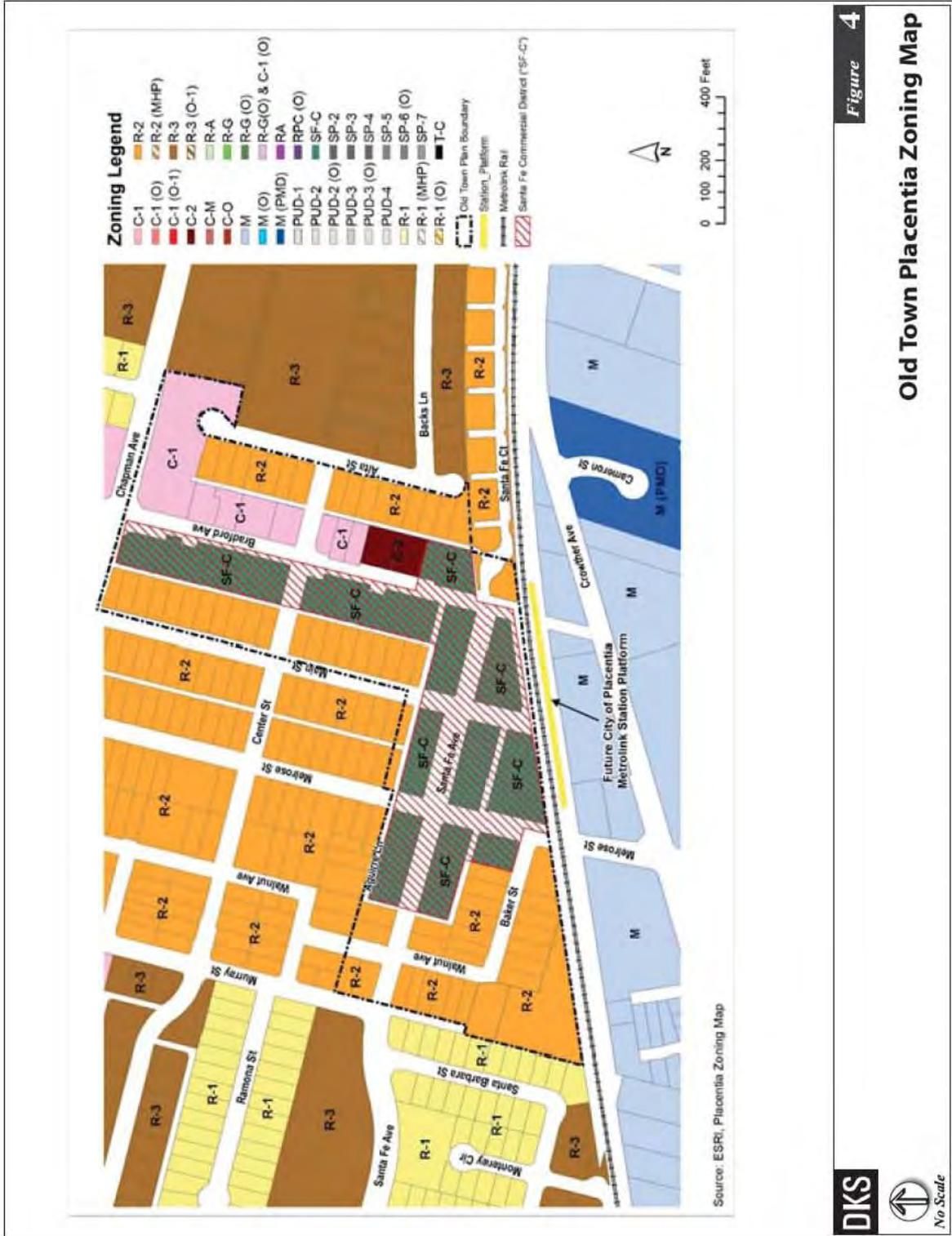


Figure 4

Old Town Placentia Zoning Map





Table E: Project Trip Generation Summary

Land Use	ITE ¹ Code	Size ²	Daily	AM Peak Hour			PM Peak Hour		
				In	Out	Total	In	Out	Total
<i>Trip Rates</i>									
Apartment	220	per DU	6.65	0.10	0.41	0.51	0.40	0.22	0.62
Hotel	310	per RM	8.17	0.31	0.22	0.53	0.31	0.29	0.60
Shopping Center (Retail/Commercial)	820	per TSF	42.70	0.60	0.36	0.96	1.78	1.93	3.71
<i>Trip Generation</i>									
<i>Trips</i>									
Apartment	525	DU	3,491	53	215	268	210	116	326
Hotel	50	RM	409	16	11	27	15	15	30
Shopping Center (Retail/Commercial)	125.00	TSF	5,338	75	45	120	223	241	464
Gross Total Trips			9,238	144	271	415	448	372	820
<i>Transit Reduction for Apartment (Less 25% Transit Trip)³</i>			(873)	(13)	(54)	(67)	(53)	(29)	(82)
<i>Pass-by Reduction for Retail/Commercial (Less 34% Pass-by Trip)⁴</i>			-	-	-	-	(76)	(82)	(158)
<i>Internal Capture Reduction for Development (12% Internal Trip)⁵</i>			(1,004)	(16)	(26)	(42)	(38)	(32)	(70)
Net Project Trip Generation			7,361	115	191	306	281	229	510

¹ITE – Institute of Transportation Engineers

²TSF = Thousand Square Feet, DU = Dwelling Unit, RM = Room

³Due to the development’s vicinity to local transit facilities, a 25% transit credit was applied.

⁴Pass-by traffic is retail commercial traffic that is already on the road and driving by the site. The driver will stop at the retail shop on their way to their final destination. A 34% pass-by reduction has been applied based on ITE Table 5.6 *Pass-by Trip for Land Use 820 – Shopping Center*. It should be noted that the trip credit can only be applied to the PM peak hour.

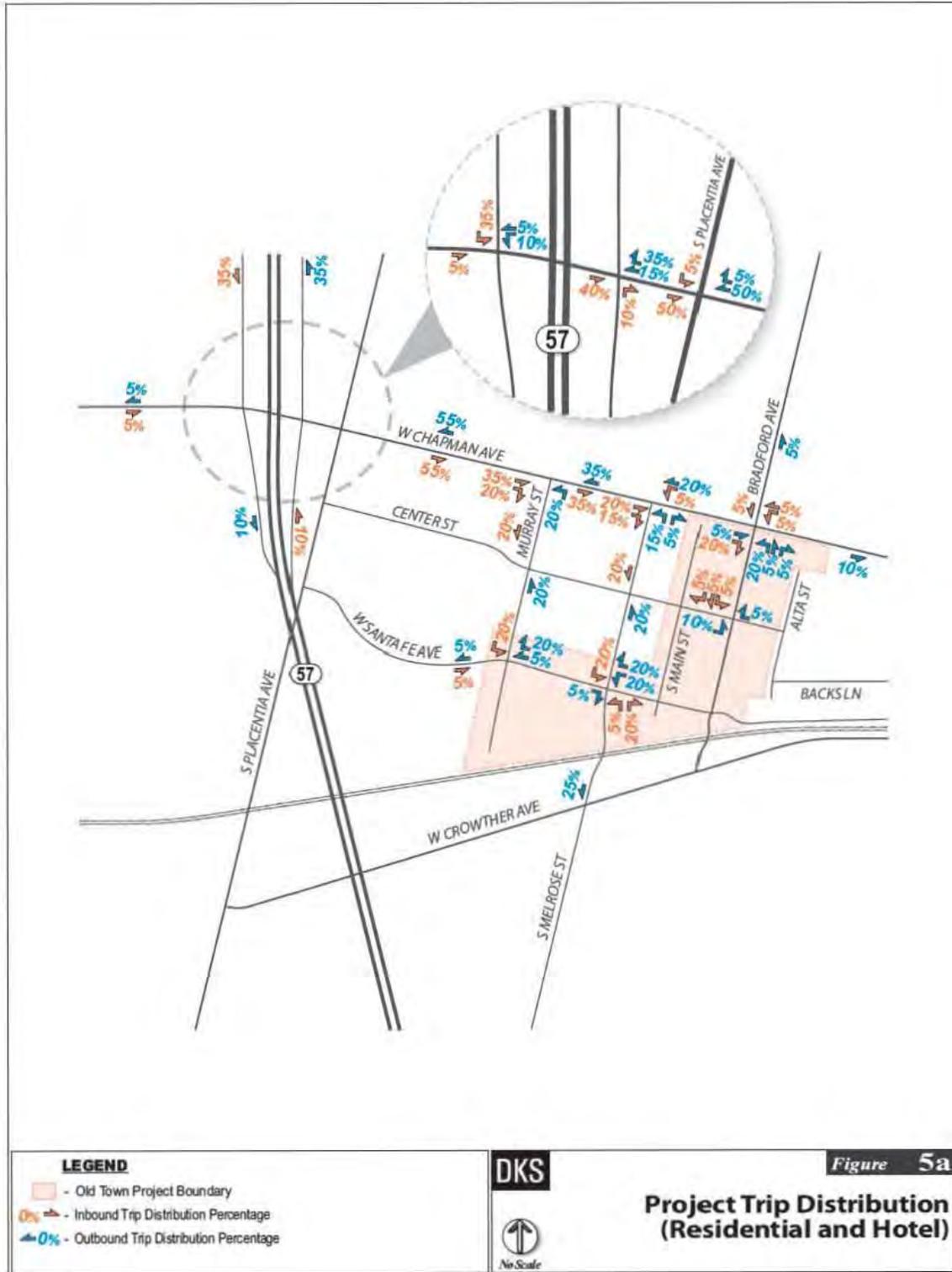
⁵A 12% internal capture reduction has been applied for the interaction of all land uses. The percentage was estimated using ITE Handbook Figure 7.2 *Multi-Use Trip Generation Calculation Table*.

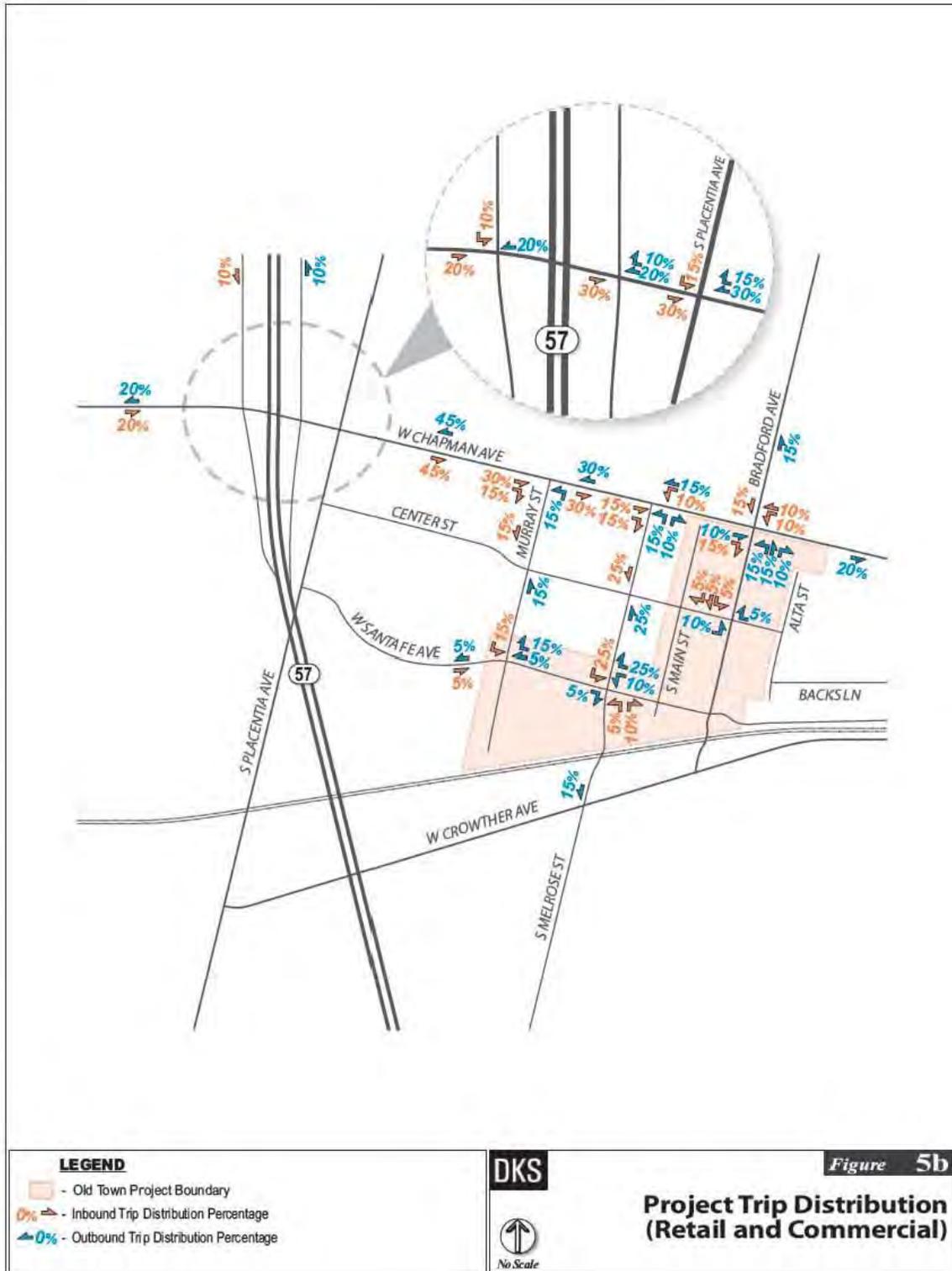
Trip Distribution and Assignment

Project trip distribution patterns were based on factors such as: 1) transportation facility characteristics that impact travel demand (i.e. location of urban arterials, freeways, and interchanges); 2) location of employment and commercial facilities; and 3) existing traffic patterns. In addition, the trip distribution patterns were based on the preferred alternative circulation design (Option #3) for the Old Town Revitalization Project. The design proposes the following circulation modifications:

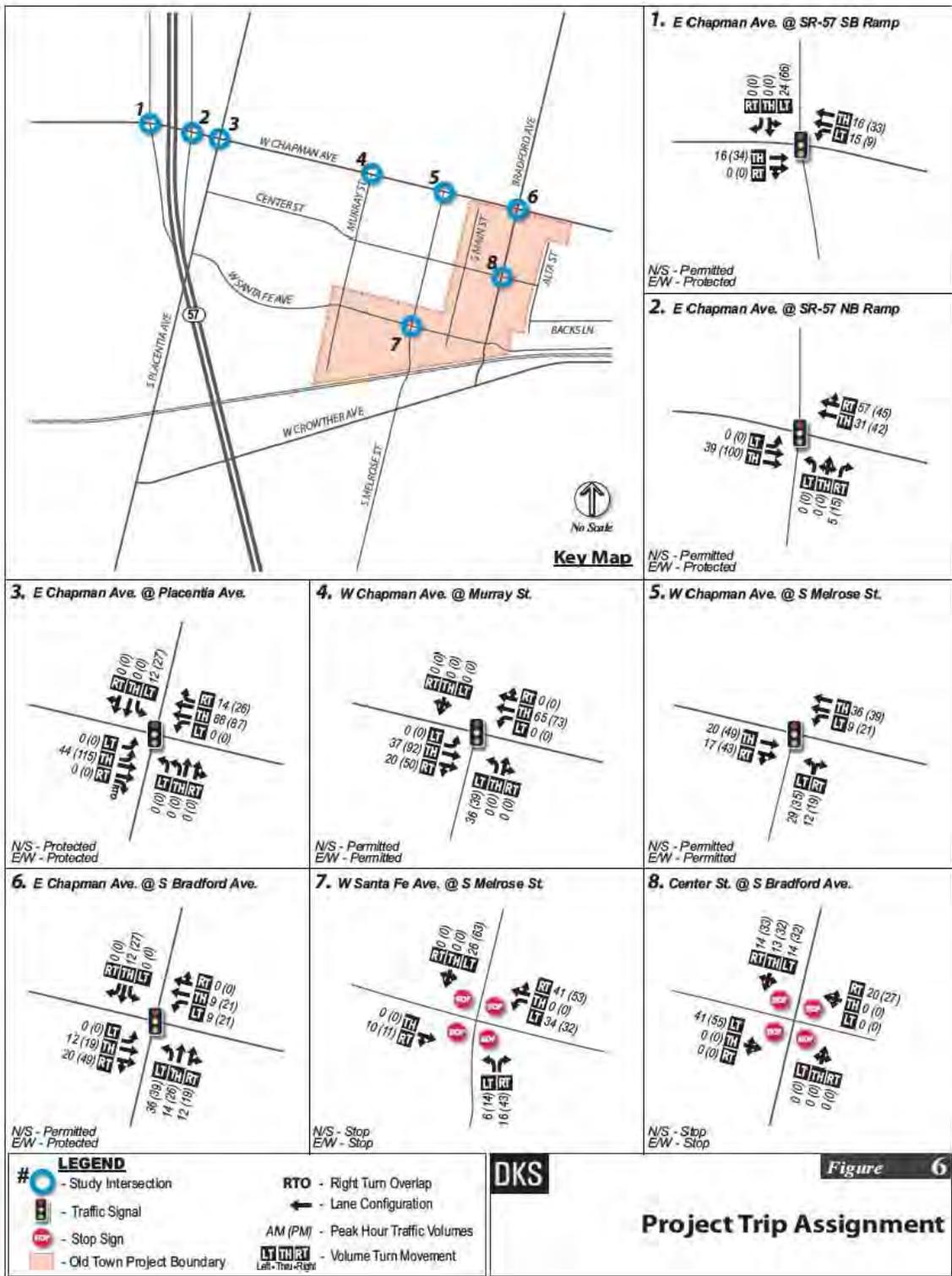
- One-Way Traffic
 - Southbound Bradford Avenue: Center Street to Santa Fe Avenue
 - Westbound Santa Fe Avenue: Bradford Avenue to Main Street

Figures 5a and 5b illustrate trip distribution percentages for the proposed project. Trip distribution percentages were applied to the proposed project’s trip generation to calculate the traffic volumes which the project would generate at study intersections (i.e. trip assignment). The resulting AM and PM peak hour trip assignments used for the LOS analysis are shown in Figure 6. The trip distribution percentages were reviewed and approved by City staff.





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Existing (2017) Plus Project

Traffic Volumes

The trips generated from the project, as shown in Figure 6, were added to the existing traffic volumes shown in Figure 3 which result in the existing plus project traffic scenario. Figure 7 illustrates the Existing Year (2017) Plus Project traffic volumes.

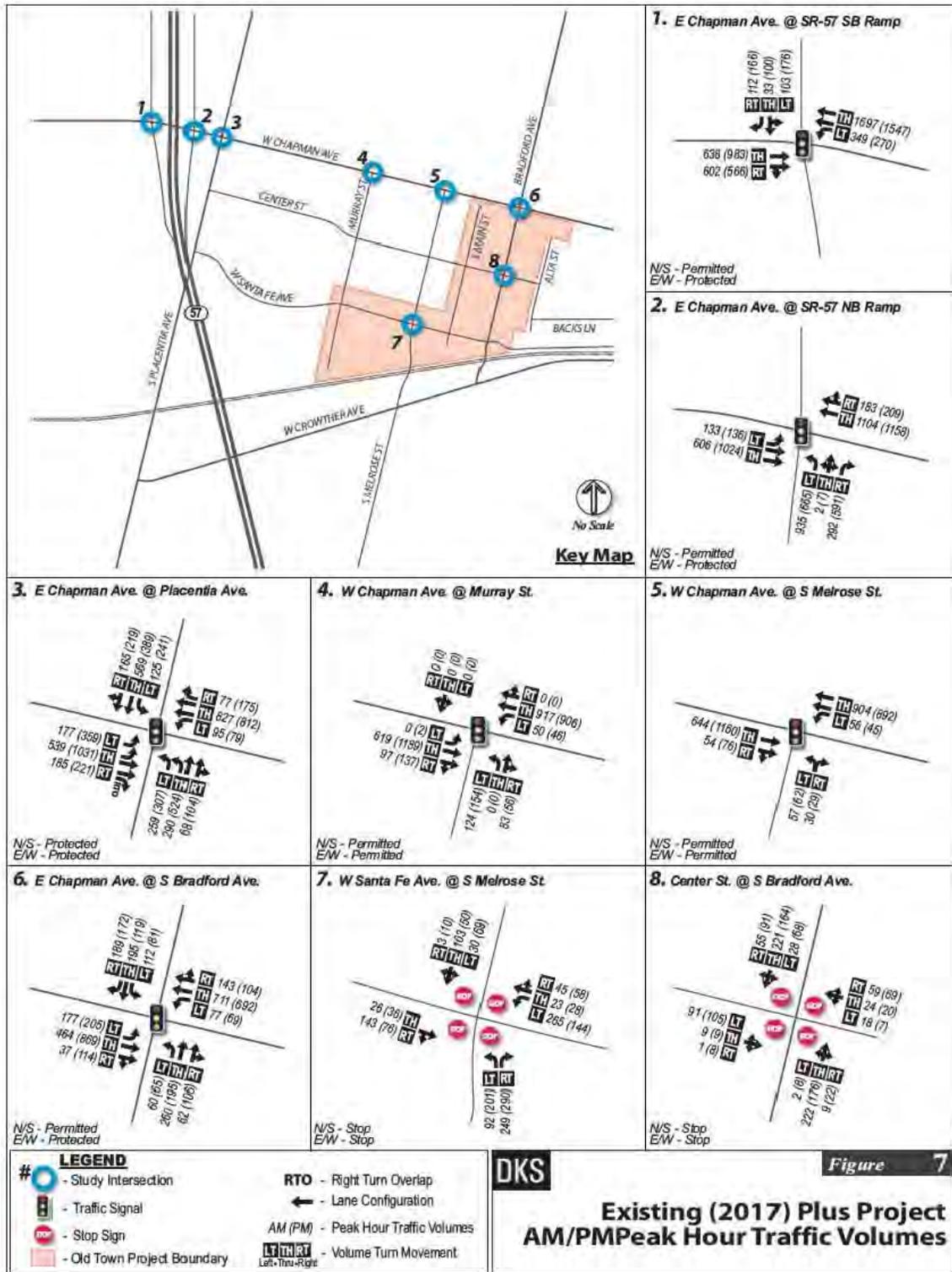
Levels of Service

The existing level of service has been evaluated at the study intersections based on the ICU methodology for signalized intersections and the 2000 HCM methodology for all-way stop controlled and Caltrans intersections. The LOS summary for intersections is shown in Table F. As shown, all intersections operate at LOS C or better. LOS calculation sheets are provided in Appendix C.

Significant Impact

Based on the threshold for significant impacts of the proposed project, the trips generated from the proposed project would not cause significant impact on any of the study intersections under Existing (2017) Plus Project traffic conditions for both the AM and PM peak periods. Therefore, no mitigation measures are required on study intersections as part of the project.

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Table F: Existing (2017) Plus Project Intersection Level of Service Summary¹

Intersection	Existing				Existing Plus Project				Difference		Project Impact
	AM Peak		PM Peak		AM Peak		PM Peak		AM Peak	PM Peak	
	V/C or Delay (sec.)	LOS	V/C or Delay (sec.)	LOS	V/C or Delay (sec.)	LOS	V/C or Delay (sec.)	LOS	V/C or Delay (sec.)	V/C or Delay (sec.)	
Caltrans - Signalized											
Chapman Ave/ SR-57 SB Ramp	12.7	B	15.4	B	13.5	B	17.7	B	2.7	2.3	NO
Chapman Ave/ SR-57 NB Ramp	26.4	C	26.2	C	27.3	C	27.3	C	0.9	1.1	NO
City of Placentia - Signalized											
Chapman Ave/ Placentia Ave	0.611	B	0.679	B	0.637	B	0.721	C	0.026	0.042	NO
Chapman Ave/ Murray St	0.352	A	0.493	A	0.393	A	0.558	A	0.041	0.065	NO
Chapman Ave/ Melrose St	0.332	A	0.416	A	0.367	A	0.494	A	0.035	0.078	NO
Chapman Ave/ Bradford Ave	0.556	A	0.521	A	0.566	A	0.541	A	0.010	0.020	NO
City of Placentia – All-Way Stop Controlled											
Santa Fe Ave/ Melrose St	11.1	B	9.9	A	12.2	B	11.1	B	1.1	1.2	NO
Center St/ Bradford Ave	8.6	A	8.7	A	9.2	A	9.9	A	0.6	1.2	NO

¹Analysis Software: Traffix, Version 8.0. Per the Intersection Capacity Utilization methodology, overall volume to capacity ratios and levels of service are shown for intersections controlled by traffic signals. Per the Highway Capacity Manual (HCM 2000) methodology, overall average intersection delay and level of service are shown for intersections controlled by all-way stop and for intersections under the jurisdiction of Caltrans.



4.0 PROJECT BUILDOUT YEAR (2037) CONDITIONS

Project Buildout Year (2037) Without Project

Traffic Volumes

Future buildout traffic forecasts were developed in order to analyze the project traffic impacts during the buildout year of the project (2037). Based on discussions with the City staff, a 1% annual growth was added to the existing vehicular traffic volumes for a period of 20 years to determine the future 2037 traffic volumes at the study intersections. In addition, the City of Placentia provided a list and locations of cumulative projects to be used for the future 2037 analysis. Figure 8 illustrates the general location of these cumulative projects. The list of cumulative projects is presented in Table G.

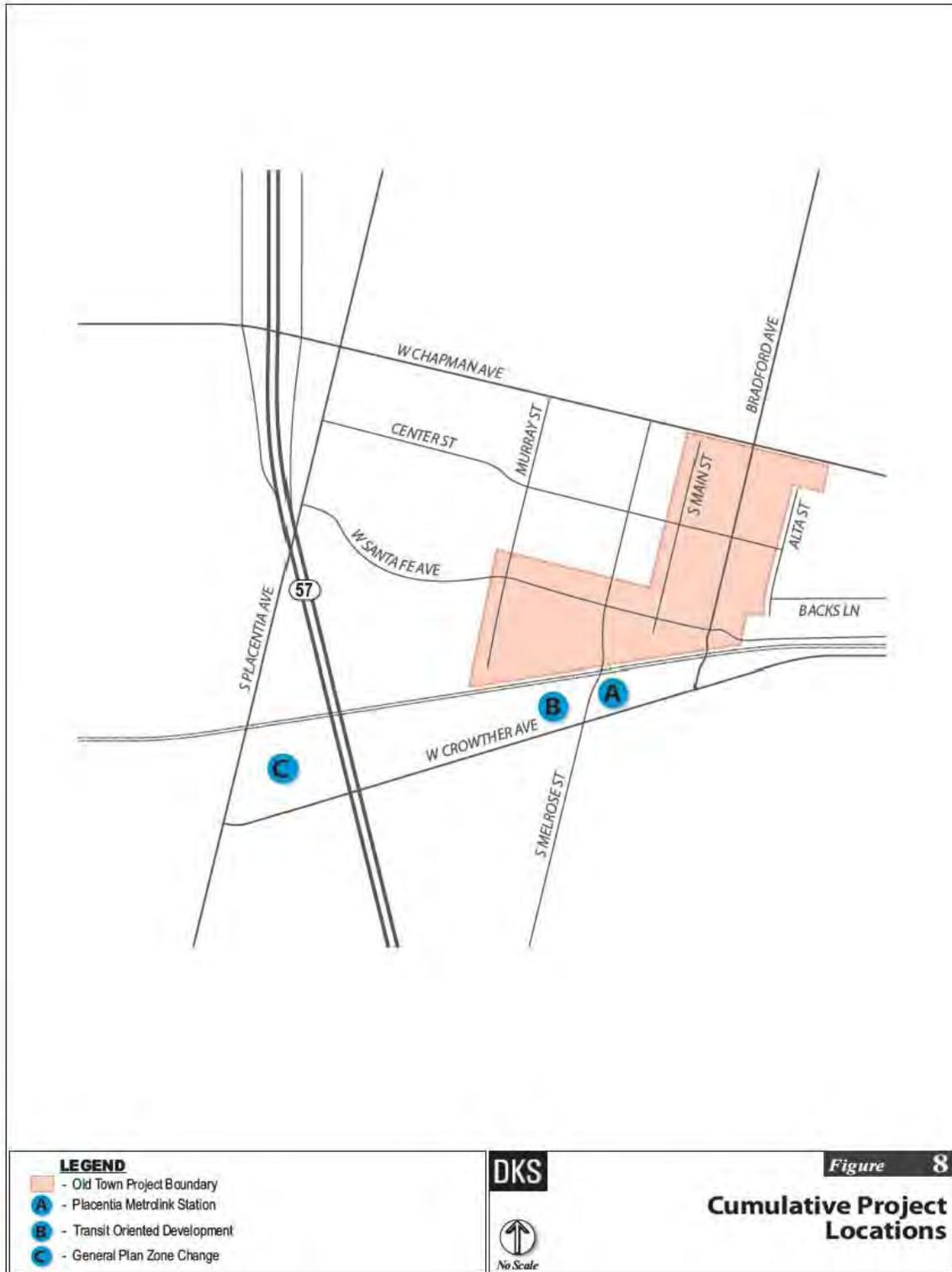
Table G: List of Cumulative Projects

Cumulative Project	Description	Location
Placentia Metrolink Station	New Train Station	NEC of Melrose St/Crowther Ave
Transit Oriented Development (TOD)	752 Single-Family Residential Units	Melrose St/Crowther Ave Vicinity
General Plan Zoning Change	318,000 square feet of 'Industrial' to 'Commercial'	NEC of Placentia Ave/Crowther Ave

Cumulative volumes were developed and distributed to the roadway network. Trip generation estimates for the Placentia Metrolink Station and the TOD project were obtained from the *Traffic Impact Study for the Proposed Packing House Area Redevelopment* developed by Albert Grover & Associates (August 2016).

For the General Plan Zoning Change project, DKS developed traffic generation estimates for the eight (8) parcels located in the triangular area bound by Placentia Avenue, Crowther Avenue, and the SR-57 Freeway. The City proposes to change the zoning designation of the site from "Industrial" to "Commercial." The site is comprised of four (4) vacant parcels (119,000 sf), four (4) occupied parcels (63,780 sf), and additional site coverage (53,200 sf) totaling approximately 318,000 square feet.

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Based on a methodology used in previous City of Placentia traffic studies, DKS estimated the net difference of trips as a result of the zoning change. In order to estimate the number of trips generated by the site assuming 100% industrial use without any vacant lots, DKS calculated the building coverage percentage of the existing four (4) occupied lots. Based on land use information provided by the City and an estimation of individual parcel square footage, DKS calculated that 46% of lot coverage is occupied by industrial building space and the remaining 54% is occupied by parking lots, landscaping, etc. As shown in Table H, the vacant lots are projected to occupy 54,740 sf of industrial use. Therefore, 118,520 sf of industrial use is estimated under the assumption of 100% occupancy.

Table H: General Plan Zoning Change – Industrial Use Estimates

Vacant Lot Address	Lot Size (sf) ¹	Average Building Coverage	Estimated Building Size (sf)
350 S. Placentia Ave.	6,000	46%	2,760
380 S. Placentia Ave.	78,000	46%	35,880
450 S. Placentia Ave.	18,000	46%	8,280
480 S. Placentia Ave.	17,000	46%	7,820
Projected Vacant Lot Occupancy			54,740
Total Existing Operation Use			63,780
Total Industrial Land Use			118,520

¹Lot size estimations were based on Google Earth software aeriels.

Similarly, DKS estimated the total square footage for 100% commercial use by surveying four (4) retail centers within a one (1) mile radius of the site. DKS chose these retail centers due to the variety of on-site land uses and due to their vicinity to project site. DKS considers this approach to be a reasonable basis on estimating building coverage for the proposed “Commercial” zoning designation. Based on lot coverage estimations of each individual retail center, DKS calculated that 29% of lot coverage is occupied by operational commercial building use. As shown in Table I, DKS assumed 92,220 sf of commercial use under the proposed “Commercial” zoning designation.

Table I: Commercial Use Building Coverage Estimates

Retail Center	Lot Size (sf) ¹	Building Coverage
Placentia Plaza (616 Chapman Avenue)	204,000	32%
Meadowbrook Plaza (629 Placentia Avenue)	97,000	29%
501 State College Boulevard Retail Center	83,700	25%
516 State College Boulevard Retail Center	367,000	30%
Average Building Coverage	-	29%
Project Site - 100% Commercial Use	318,000 sf	92,220 sf (29%)

¹Lot size estimations were based on Google Earth software aeriels.



As shown in Table J, the cumulative developments are projected to generate approximately 8,194 trip-ends per day, with 416 (105 inbound, 311 outbound) trips during the AM peak hour and 698 (388 inbound, 310 outbound) trips during the PM peak hour. Trip distribution details for the cumulative projects are included in Appendix E.

Table J: Cumulative Projects Trip Generation Summary

Cumulative Project	ITE Code	Size	Units ¹	Daily	AM Peak Hour			PM Peak Hour		
					In	Out	Total	In	Out	Total
Placentia Metrolink Station	-	-	-	1,180	211	31	242	31	211	242
Transit Oriented Development (TOD)	220	752	DU	3,753	-65	259	194	261	60	321
Existing Zoning Designation – Industrial	110	118.520	TSF	-826	-96	-13	-109	-14	-101	-115
Proposed Zoning Designation – Commercial	826	92.220	TSF	4,087	55	34	89	110	140	250
Total Trips				8,194	105	311	416	388	310	698

ITE – Institute of Transportation Engineers

¹ DU = Dwelling Unit

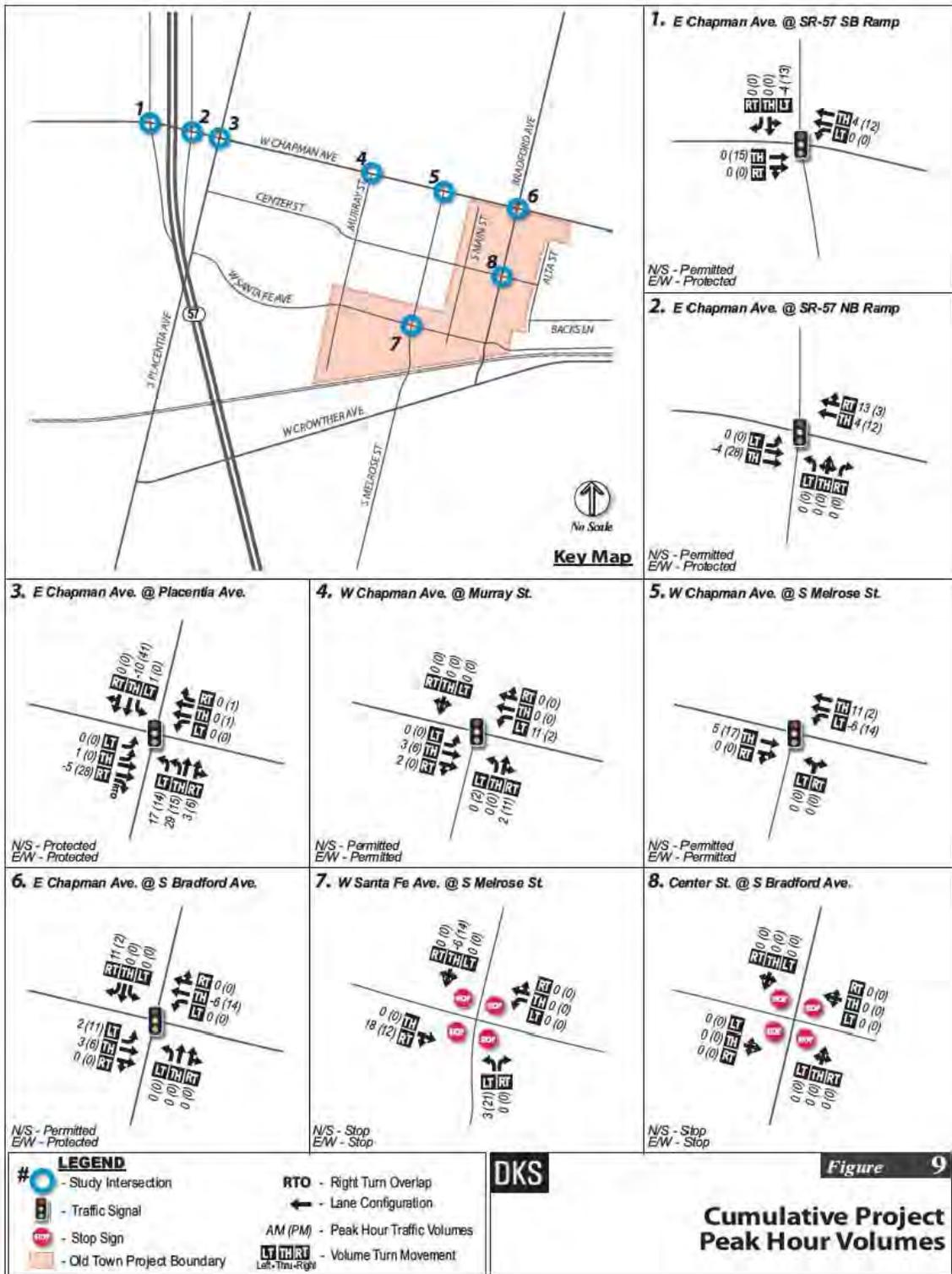
TSF = Thousand Square Feet

Figure 9 shows the cumulative project volumes at study intersections. Figure 10 illustrates the Project Buildout Year (2037) Without Project AM and PM peak hour traffic volumes in the study area. Project Buildout Year (2037) traffic conditions include the existing peak hour intersection volumes plus the ambient growth rate (20 years at 1% growth per year) and cumulative traffic volumes.

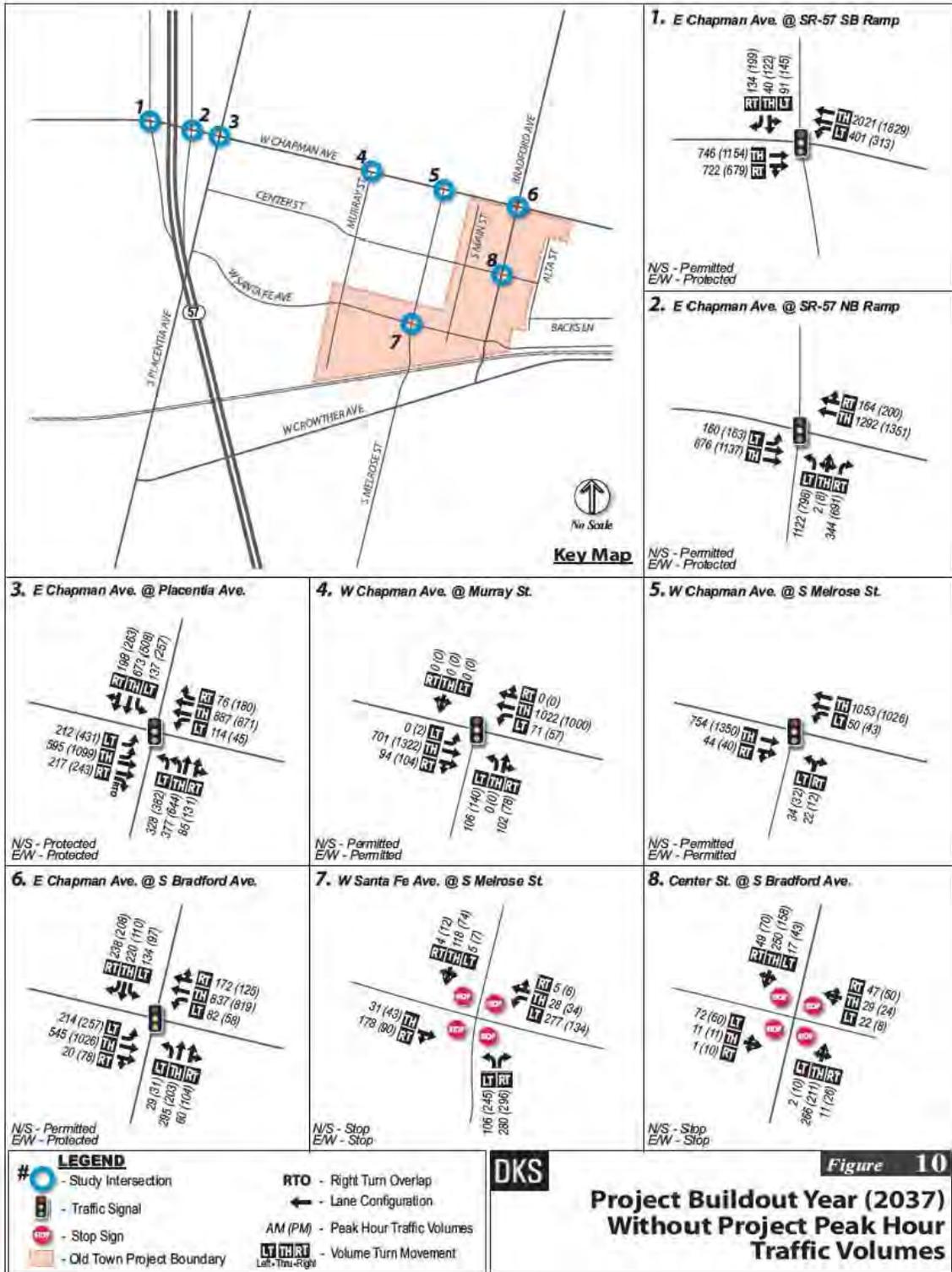
Level of Service

The Project Buildout Year (2037) Without Project level of service has been evaluated at the study intersections based on the ICU methodology for signalized intersections and the 2000 HCM methodology for all-way stop controlled and Caltrans intersections. The LOS summary for intersections is shown in Table K. As shown, all intersections operate at LOS D or better. LOS calculation sheets are provided in Appendix E.

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**Table K: Project Buildout Year (2037) Without Project
 Intersection Level of Service Summary**

Intersection	AM Peak Hour		PM Peak Hour	
	V/C or Delay (sec.)	LOS	V/C or Delay (sec.)	LOS
<i>Caltrans - Signalized</i>				
Chapman Avenue/SR-57 Southbound Ramps	15.1	B	18.3	B
Chapman Avenue/SR-57 Northbound Ramps	39.1	D	40.8	D
<i>City of Placentia - Signalized</i>				
Chapman Avenue/Placentia Avenue	0.726	C	0.812	D
Chapman Avenue/Murray Street	0.413	A	0.586	A
Chapman Avenue/Melrose Street	0.392	A	0.510	A
Chapman Avenue/Bradford Avenue	0.656	B	0.626	B
<i>City of Placentia – All-Way Stop Controlled</i>				
Santa Fe Street/Melrose Street	13.1	B	11.3	B
Center Street/Bradford Avenue	10.3	B	9.4	A

¹Analysis Software: Traffix, Version 8.0. Per the Intersection Capacity Utilization methodology, overall volume to capacity ratios and levels of service are shown for intersections controlled by traffic signals. Per the Highway Capacity Manual (HCM 2000) methodology, overall average intersection delay and level of service are shown for intersections controlled by all-way stop and for intersections under the jurisdiction of Caltrans.



Project Buildout Year (2037) With Project

Traffic Volumes

The trips generated from the project, as shown in Figure 6, were added to the Project Buildout Year (2037) Without Project traffic volumes shown in Figure 10, which results in the Project Buildout Year (2037) With Project traffic scenario. Figure 11 illustrates the Project Buildout Year (2037) With Project traffic volumes.

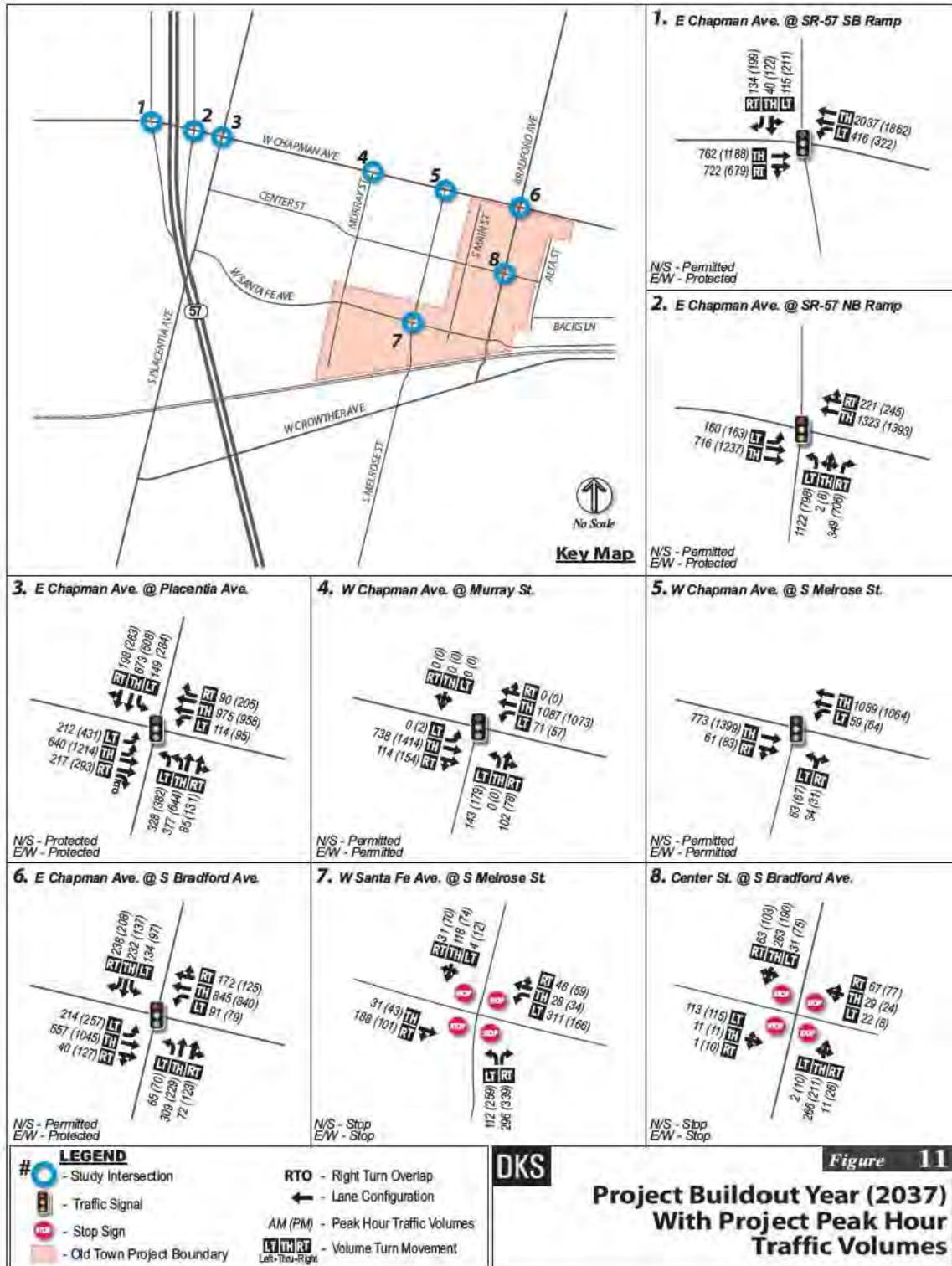
Level of Service

The Project Buildout Year (2037) With Project level of service has been evaluated at the study intersections based on the ICU methodology for signalized intersections and the 2000 HCM methodology for all-way stop controlled and Caltrans intersections. The LOS summary for intersections is shown in Table L. As shown, all intersections operate at LOS D or better. LOS calculation sheets are provided in Appendix F.

Significant Impact

Based on the threshold for significant impacts of the proposed project, the trips generated from the proposed project would not cause significant impact on any of the study intersections under Project Buildout Year (2037) Plus Project traffic conditions for both the AM and PM peak periods. Therefore, no mitigation measures are required on study intersections as part of the project.

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**Table L: Project Buildout Year (2037) With Project Intersection
 Level of Service Summary**

Intersection	Year 2037 Without Project				Year 2037 With Project				Difference		Project Impact
	AM Peak		PM Peak		AM Peak		PM Peak		AM Peak	PM Peak	
	V/C or Delay (sec.)	LOS	V/C or Delay (sec.)	LOS	V/C or Delay (sec.)	LOS	V/C or Delay (sec.)	LOS	V/C or Delay (sec.)	V/C or Delay (sec.)	
<i>Caltrans - Signalized</i>											
Chapman Ave/ SR-57 SB Ramp	15.1	B	18.3	B	16.1	B	21.0	C	1.0	2.7	NO
Chapman Ave/ SR-57 NB Ramp	39.1	D	40.8	D	43.1	D	45.2	D	4.0	4.4	NO
<i>City of Placentia - Signalized</i>											
Chapman Ave/ Placentia Ave	0.726	C	0.812	D	0.752	C	0.858	D	0.026	0.046	NO
Chapman Ave/ Murray St	0.413	A	0.586	A	0.454	A	0.650	B	0.041	0.064	NO
Chapman Ave/ Melrose St	0.392	A	0.510	A	0.427	A	0.581	A	0.035	0.071	NO
Chapman Ave/ Bradford Ave	0.656	B	0.626	B	0.666	B	0.646	B	0.010	0.020	NO
<i>City of Placentia - All-Way Stop Controlled</i>											
Santa Fe Ave/ Melrose St	13.1	B	11.3	B	14.7	B	12.9	B	1.6	1.6	NO
Center St/ Bradford Ave	10.3	B	9.4	A	11.2	B	11.0	B	0.9	1.6	NO

Analysis Software: Traffix, Version 8.0. Per the Intersection Capacity Utilization methodology, overall volume to capacity ratios and levels of service are shown for intersections controlled by traffic signals. Per the Highway Capacity Manual (HCM 2000) methodology, overall average intersection delay and level of service are shown for intersections controlled by all-way stop and for intersections under the jurisdiction of Caltrans.



5.0 CONCLUSION

Based on the results of the analysis, the proposed project generated trips would not cause significant impact at any of the study intersections for both Existing (2017) Plus Project and Project Buildout (Year 2037) With Project traffic conditions during the AM and PM peak periods. Therefore, no mitigation measures are required at any study intersection as a part of the proposed project.

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APPENDIX C
UTILITIES WORKSHEETS

INTRODUCTION TO UTILITY SCREENING TABLES

The following worksheets are used to evaluate the potential impacts of a project.

Table 1 Definition of Project

This Table is used to establish the proposed development parameters that are used in the calculation of utilities usage. The independent variable to be entered is identified by shading. For residential development, the number of housing units should be entered in the shaded area. For non-residential development, the total floor area of development should be entered in the shaded area.

Table 2 Summary of Project Impacts

Consumption/Generation Rates. This table indicates the development's projected electrical consumption, natural gas consumption, water consumption, effluent generation, and solid waste generation. No modifications should be made to this table.

Tables 3 through 7 Calculation of Project Impacts

Tables 3 through 7 indicate the results of the analysis.

Table 3 Electrical Consumption - This Table calculates the projected electrical consumption for new development. Default generation rates provided in the shaded areas may be changed.

Table 4 Natural Gas Consumption - This Table calculates the projected natural gas usage for new development. Default generation rates provided in the shaded areas may be changed.

Table 5 Water Consumption - This Table calculates the projected water consumption rates for new development. Default generation rates provided in the shaded areas may be changed.

Table 6 Sewage Generation - This Table calculates the projected effluent generation rates for new development. Default generation rates provided in the shaded areas may be changed.

Table 7 Solid Waste Generation - This Table calculates the projected waste generation for new development. Default generation rates provided in the shaded areas may be changed.

Table 1 Project Name: Old Town Placentia Revitalization

Definition of Project Parameters - Enter independent variable (no. of units or floor area) in the shaded area. The independent variable to be entered is the number of units (for residential development) or the gross floor area (for non-residential development).

Land Use	Independent Variable	Factor
Residential Uses		
Single-Family Residential	No. of Units	0
Medium Density Residential	No. of Units	0
Multiple-Family Residential	No. of Units	525
Mobile Home	No. of Units	0
Office Uses		
Office	Sq. Ft.	0
Medical Office Building	Sq. Ft.	0
Office Park	Sq. Ft.	0
Bank Financial Services	Sq. Ft.	0
Commercial Uses		
Specialty Retail Commercial	Sq. Ft.	0
Convenience Store	Sq. Ft.	0
Movie Theater	Sq. Ft.	0
Shopping Center	Sq. Ft.	125,000
Sit-Down Restaurant	Sq. Ft.	0
Fast-Food Restaurant	Sq. Ft.	0
Hotel	Rooms	50
Manufacturing Uses		
Industrial Park	Sq. Ft.	0
Manufacturing	Sq. Ft.	0
General Light Industry	Sq. Ft.	0
Warehouse	Sq. Ft.	0
Public Institutional		
Public Institutional	Sq. Ft.	0
Open Space	Sq. Ft.	0

Table 2: Projected Utility Consumption and Generation		
<i>Summary of Project Impacts - Results of analysis identified below. No modifications should be made to this Table.</i>		
Utilities Consumption and Generation	Factor	Rates
Electrical Consumption	kWh/day	21,612
Natural Gas Consumption	cubic feet/day	6,763
Water Consumption	gallons/day	124,125
Sewage Generation	gallons/day	78,350
Solid Waste Generation	pounds/day	2,850

Table 3: Electrical Consumption				
Project Component	Units of Measure	Consumption Factor		Projected Consumption
Residential Uses				
	No. of Units	kWh	Variable	kWh/Unit/Day
Single-Family Residential	0	5,625.00	kWh/Unit/Year	0.0
Medium Density Residential	0	5,625.00	kWh/Unit/Year	0.0
Multiple-Family Residential	525	5,625.00	kWh/Unit/Year	8,090.8
Mobile Home	0	4,644.00	kWh/Unit/Year	0.0
Office Uses				
	Sq. Ft.	kWh	Variable	kWh/Sq. Ft./Day
Office	0	20.80	kWh/Sq. Ft./Year	0.0
Medical Office Building	0	14.20	kWh/Sq. Ft./Year	0.0
Office Park	0	20.80	kWh/Sq. Ft./Year	0.0
Bank/Financial Services	0	20.80	kWh/Sq. Ft./Year	0.0
Commercial Uses				
	Sq. Ft./Rooms	kWh	Variable	kWh/Sq. Ft./Day
Specialty Retail Commercial	0	16.00	kWh/Sq. Ft./Year	0.0
Convenience Store	0	16.00	kWh/Sq. Ft./Year	0.0
Movie Theater	0	16.00	kWh/Sq. Ft./Year	0.0
Shopping Center	125,000	35.90	kWh/Sq. Ft./Year	12,295
Sit-Down Restaurant	0	49.10	kWh/Sq. Ft./Year	0.0
Fast-Food Restaurant	0	49.10	kWh/Sq. Ft./Year	0.0
Hotel	50	8,955.00	kWh/Sq. Ft./Year	1,226.7
Manufacturing Uses				
	Sq. Ft.	kWh	Variable	kWh/Sq. Ft./Day
Industrial Park	0	4.80	kWh/Sq. Ft./Year	0.0
Manufacturing	0	4.80	kWh/Sq. Ft./Year	0.0
General Light Industry	0	4.80	kWh/Sq. Ft./Year	0.0
Warehouse	0	4.80	kWh/Sq. Ft./Year	0.0
Public/Institutional				
	Sq. Ft.	kWh	Variable	kWh/Sq. Ft./Day
Public/Institutional	0	4.80	kWh/Sq. Ft./Year	0.0
Open Space	0	0.00	kWh/Sq. Ft./Year	0.0
Total Daily Electrical Consumption (kWh/day)				21,612.0
Sources:				
Residential rates were derived from the SCAQMD's CEQA Air Quality Handbook (April 1993).				
All other rates are from Common Forecasting Methodology VII Demand Forms, 1989				

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Table 4: Natural Gas Consumption				
Project Component	Units of Measure	Consumption Factor		Projected Consumption
Residential Uses				
	No. of Units	Cu. Ft. of Nat. Gas	Variable	Cu. Ft./Day
Single-Family Residential	0	6,665.00	Cu. Ft./Mo./Unit	0.0
Medium Density Residential	0	4,011.50	Cu. Ft./Mo./Unit	0.0
Multiple-Family Residential	525	4,011.50	Cu. Ft./Mo./Unit	5,770.0
Mobile Home	0	4,011.50	Cu. Ft./Mo./Unit	0.0
Office Uses				
	Sq. Ft.	Cu. Ft. of Nat. Gas	Variable	Cu. Ft./Day
Office	0	2.00	Cu. Ft./Mo./Sq. Ft.	0.0
Medical Office Building	0	2.00	Cu. Ft./Mo./Sq. Ft.	0.0
Office Park	0	2.00	Cu. Ft./Mo./Sq. Ft.	0.0
Bank/Financial Services	0	2.00	Cu. Ft./Mo./Sq. Ft.	0.0
Commercial Uses				
	Sq. Ft./Rooms	Cu. Ft. of Nat. Gas	Variable	Cu. Ft./Day
Specialty Retail Commercial	0	2.90	Cu. Ft./Mo./Sq. Ft.	0.0
Convenience Store	0	2.90	Cu. Ft./Mo./Sq. Ft.	0.0
Movie Theater	0	2.90	Cu. Ft./Mo./Sq. Ft.	0.0
Shopping Center	125,000	2.90	Cu. Ft./Mo./Sq. Ft.	993.2
Sit-Down Restaurant	0	2.90	Cu. Ft./Mo./Sq. Ft.	0.0
Fast-Food Restaurant	0	2.90	Cu. Ft./Mo./Sq. Ft.	0.0
Hotel	50		Cu. Ft./Mo./Room	0.0
Manufacturing Uses				
	Sq. Ft.	Cu. Ft. of Nat. Gas	Variable	Cu. Ft./Day
Industrial Park	0	4.70	Cu. Ft./Mo./Sq. Ft.	0.0
Manufacturing	0	4.70	Cu. Ft./Mo./Sq. Ft.	0.0
General Light Industry	0	4.70	Cu. Ft./Mo./Sq. Ft.	0.0
Warehouse	0	4.70	Cu. Ft./Mo./Sq. Ft.	0.0
Public/Institutional Use				
	Sq. Ft.	Cu. Ft. of Nat. Gas	Variable	Cu. Ft./Day
Public/Institutional	0	2.90	Cu. Ft./Mo./Sq. Ft.	0.0
Open Space	0	2.90	Cu. Ft./Mo./Sq. Ft.	0.0
Total Daily Natural Gas Consumption (cubic feet/day)				6,763.1
Sources:				
South Coast Air Quality Management District, CEQA Air Quality Handbook, April 1993				

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Table 5: Water Consumption				
Project Component	Units of Measure	Consumption Factor		Projected Consumption
Residential Uses				
	No. of Units	Gals. of Water	Variable	Gals./Day
Single-Family Residential	0	250.00	Gals./Day/Unit	0.0
Medium Density Residential	0	200.00	Gals./Day/Unit	0.0
Multiple-Family Residential	525	200.00	Gals./Day/Unit	105,000.0
Mobile Home	0	200.00	Gals./Day/Unit	0.0
Office Uses				
	Sq. Ft.	Gals. of Water	Variable	Gals./Day
Office	0	0.14	Gals./Day/Sq. Ft.	0.0
Medical Office Building	0	0.14	Gals./Day/Sq. Ft.	0.0
Office Park	0	0.14	Gals./Day/Sq. Ft.	0.0
Bank/Financial Services	0	0.14	Gals./Day/Sq. Ft.	0.0
Commercial Uses				
	Sq. Ft./Room	Gals. of Water	Variable	Gals./Day
Specialty Retail Commercial	0	0.10	Gals./Day/Sq. Ft.	0.0
Convenience Store	0	0.10	Gals./Day/Sq. Ft.	0.0
Movie Theater	0	0.10	Gals./Day/Sq. Ft.	0.0
Shopping Center	125,000	0.10	Gals./Day/Sq. Ft.	12,625.0
Sit-Down Restaurant	0	0.40	Gals./Day/Sq. Ft.	0.0
Fast-Food Restaurant	0	0.11	Gals./Day/Sq. Ft.	0.0
Hotel	50	130.00	Gals./Day/Room.	6,500.0
Manufacturing Uses				
	Sq. Ft.	Gals. of Water	Variable	Gals./Day
Industrial Park	0	0.14	Gals./Day/Sq. Ft.	0.0
Manufacturing	0	0.14	Gals./Day/Sq. Ft.	0.0
General Light Industry	0	0.14	Gals./Day/Sq. Ft.	0.0
Warehouse	0	0.01	Gals./Day/Sq. Ft.	0.0
Public/Institutional Use				
	Sq. Ft.	Gals. of Water	Variable	Gals./Day
Public/Institutional	0	0.10	Gals./Day/Sq. Ft.	0.0
Open Space	0	0.10	Gals./Day/Sq. Ft.	0.0
Total Daily Water Consumption (gallons/day)				124,125.0
Sources:				
Source: Derived from Orange County Sanitation District rates (150% of effluent generation).				

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Table 6: Sewage Generation				
Project Component	Units of Measure	Generation Factor		Projected Consumption
Residential Uses	No. of Units	Gals. of Effluent	Variable	Gals./Day
Single-Family Residential	0	230.00	Gals./Day/Unit	0.0
Medium Density Residential	0	200.00	Gals./Day/Unit	0.0
Multiple-Family Residential	525	120.00	Gals./Day/Unit	63,000.0
Mobile Home	0	180.00	Gals./Day/Unit	0.0
Office Uses	Sq. Ft.	Gals. of Effluent	Variable	Gals./Day
Office	0	0.11	Gals./Day/Sq. Ft.	0.0
Medical Office Building	0	0.11	Gals./Day/Sq. Ft.	0.0
Office Park	0	0.11	Gals./Day/Sq. Ft.	0.0
Bank/Financial Services	0	0.11	Gals./Day/Sq. Ft.	0.0
Commercial Uses	Sq. Ft./Rooms	Gals. of Effluent	Variable	Gals./Day
Specialty Retail Commercial	0	0.08	Gals./Day/Sq. Ft.	0.0
Convenience Store	0	0.08	Gals./Day/Sq. Ft.	0.0
Movie Theater	0	0.08	Gals./Day/Sq. Ft.	0.0
Shopping Center	125,000	0.08	Gals./Day/Sq. Ft.	10,100.0
Sit-Down Restaurant	0	0.30	Gals./Day/Sq. Ft.	0.0
Fast-Food Restaurant	0	0.08	Gals./Day/Sq. Ft.	0.0
Hotel	50	105	Gals./Day/Room.	5,250.0
Manufacturing Uses	Sq. Ft.	Gals. of Effluent	Variable	Gals./Day
Industrial Park	0	0.11	Gals./Day/Sq. Ft.	0.0
Manufacturing	0	0.11	Gals./Day/Sq. Ft.	0.0
General Light Industry	0	0.11	Gals./Day/Sq. Ft.	0.0
Warehouse	0	0.01	Gals./Day/Sq. Ft.	0.0
Public/Institutional Use	Sq. Ft.	Gals. of Effluent	Variable	Gals./Day
Public/Institutional	0	0.08	Gals./Day/Sq. Ft.	0.0
Open Space	0	0.08	Gals./Day/Sq. Ft.	0.0
Total Daily Sewage Generation (gallons/day)				78,350.0
Source: Orange County Sanitation Districts.				

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Table 7: Solid Waste Generation				
Project Component	Units of Measure	Generation Factor		Projected Generation
Residential Uses				
	No. of Units	Lbs. of Waste	Variable	Lbs./Day
Single-Family Residential	0	4.00	Lbs./Day/Unit	0.0
Medium Density Residential	0	4.00	Lbs./Day/Unit	0.0
Multiple-Family Residential	525	4.00	Lbs./Day/Unit	2,100.0
Mobile Home	0	4.00	Lbs./Day/Unit	0.0
Office Uses				
	Sq. Ft.	Lbs. of Waste	Variable	Lbs./Day
Office	0	6.00	Lbs./Day/1,000 Sq. Ft.	0.0
Medical Office Building	0	6.00	Lbs./Day/1,000 Sq. Ft.	0.0
Office Park	0	6.00	Lbs./Day/1,000 Sq. Ft.	0.0
Bank/Financial Services	0	6.00	Lbs./Day/1,000 Sq. Ft.	0.0
Commercial Uses				
	Sq. Ft./Rooms	Lbs. of Waste	Variable	Lbs./Day
Specialty Retail Commercial	0	42.00	Lbs./Day/1,000 Sq. Ft.	0.0
Convenience Store	0	42.00	Lbs./Day/1,000 Sq. Ft.	0.0
Movie Theater	0	6.00	Lbs./Day/1,000 Sq. Ft.	0.0
Shopping Center	125,000	6.00	Lbs./Day/1,000 Sq. Ft.	750.0
Sit-Down Restaurant	0	6.00	Lbs./Day/1,000 Sq. Ft.	0.0
Fast-Food Restaurant	0	42.00	Lbs./Day/1,000 Sq. Ft.	0.0
Hotel	50		Lbs./Day/Room	0.0
Manufacturing Uses				
	Sq. Ft.	Lbs. of Waste	Variable	Lbs./Day
Industrial Park	0	6.00	Lbs./Day/1,000 Sq. Ft.	0.0
Manufacturing	0	6.00	Lbs./Day/1,000 Sq. Ft.	0.0
General Light Industry	0	6.00	Lbs./Day/1,000 Sq. Ft.	0.0
Warehouse	0	6.00	Lbs./Day/1,000 Sq. Ft.	0.0
Public/Institutional Use				
	Sq. Ft.	Lbs. of Waste	Variable	Lbs./Day
Public/Institutional	0	4.00	Lbs./Day/1,000 Sq. Ft.	0.0
Open Space	0	3.00	Lbs./Day/1,000 Sq. Ft.	0.0
Total Daily Solid Waste Generation				2,850.0
Source: City of Los Angeles Average Solid Waste Generation Rates, April 1981				